



Texas Department of Transportation

DEWITT C. GREER STATE HIGHWAY BLDG. • 125 E. 11TH STREET • AUSTIN, TEXAS 78701-2483 • (512) 463-8585

May 24, 2004

NH ()
Environmental Assessment
Johnson and Tarrant Counties
CSJ 0504-04-001; 0504-05-001

SH 121: From FM 1187 to US 67

Ms. Denise Francis
State Single Point of Contact
Governor's Office of Budget and Planning
P.O. Box 12428
Austin, Texas 78711

Post-It® Fax Note	7671	Date	6-25-04	# of pages	2
To	James Thomas	From	Robert Hall		
Co./Dept.		Co.	TxDOT		
Phone #	972-960-4400	Phone #	817-370-6755		
Fax #	972-960-4471	Fax #			

Dear Ms. Francis:

A finding of no significant impact has been issued for the subject project. It has been determined that this project will not significantly affect the quality of the human environment.

Sincerely,

Ann M. Irwin
TRACS Coordinator

MMS: pat
Attachment
bcc: Fort Worth District
FS-A ERG
Reference: ENV 850

NOTE TO DISTRICT: Attached is one copy of the Finding of No Significant Impact (FONSI) signed by the FHWA. This completes the public hearing requirement. As indicated in the Environmental Manual, the news media should be notified by press release that approval has been received. Also, please notify the State intergovernmental review contact of the availability of the FONSI. Please note, coordination with the USACE for Individual and Nationwide Permits is required. Final environmental clearance will be granted once the permits are received. These permits must be received prior to the Letter of Authority date.

FEDERAL HIGHWAY ADMINISTRATION

FINDING OF NO SIGNIFICANT IMPACT

FOR

NH ()

Environmental Assessment
Johnson and Tarrant Counties
CSJ 0504-04-001; 0504-05-001

SH 121 South: From FM 1187 to US 67

The FHWA has determined that this project will not have any significant impact on the human environment. This finding of no significant impact is based on the attached environmental assessment which has been independently evaluated by the FHWA and determined to adequately and accurately discuss the environmental issues and impacts of the proposed project. It provides sufficient evidence and analysis for determining that an environmental impact statement is not required.

5/20/04
DATE

Sandra D. Deegan
FEDERAL HIGHWAY ADMINISTRATION



LAKE BENBROOK

SH 121

McPHERSON (Proposed)

FM 731

FM 1187

US 71

FM 1187

BLUE ALTERNATIVE

BURLESON

RED ALTERNATIVE

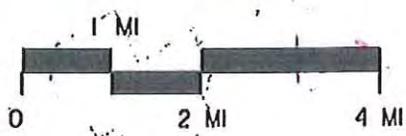
GREEN ALTERNATIVE

SH 174

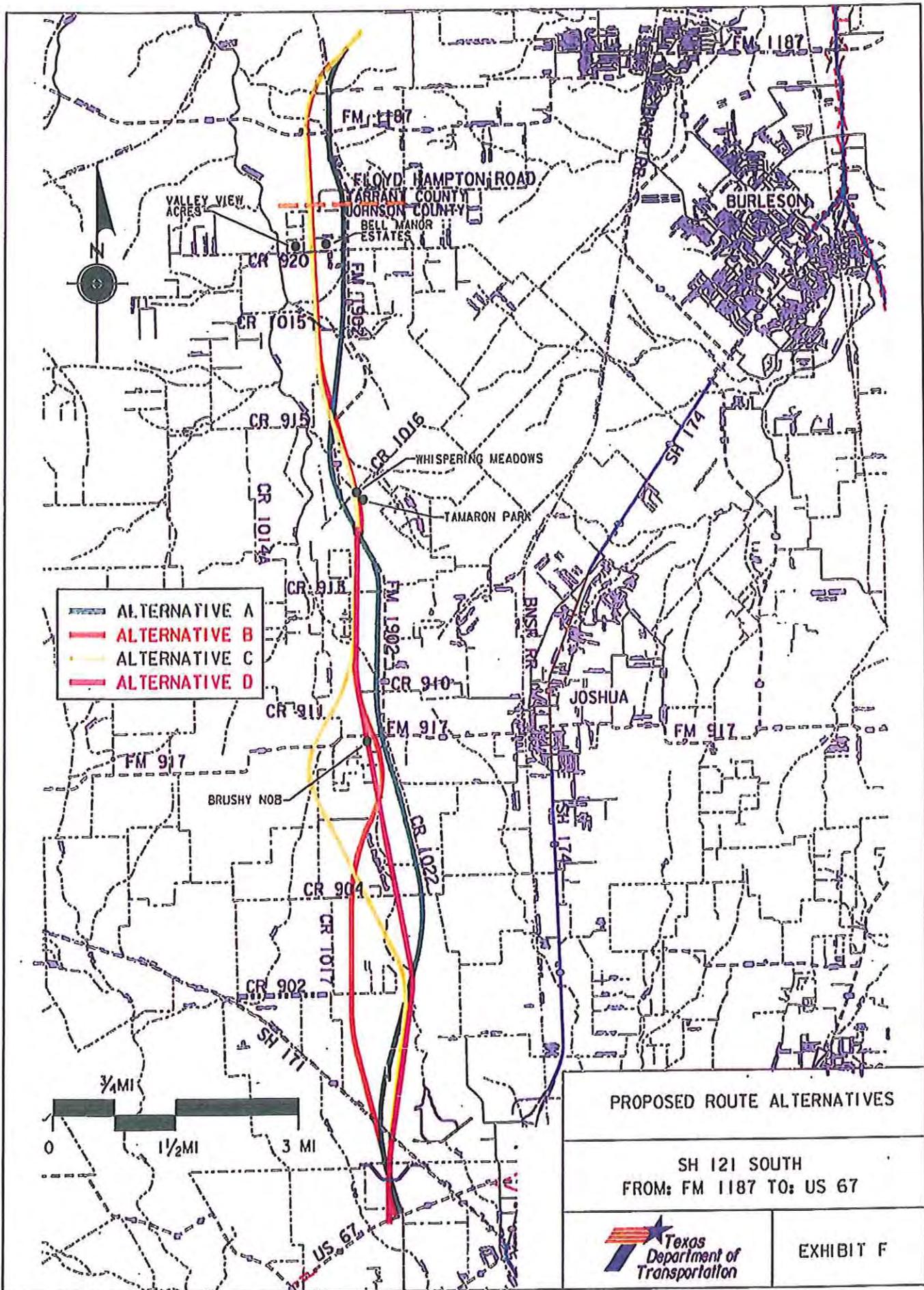
FM 917

FM 1902

YELLOW ALTERNATIVE



ALTERNATIVE ALIGNMENTS FOR "SOUTH SECTION" OCTOBER 1989	
SH 121 SOUTH FROM: FM 1187 TO: US 67	
	EXHIBIT C

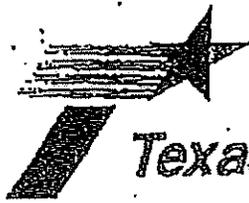


PROPOSED ROUTE ALTERNATIVES

SH 121 SOUTH
FROM: FM 1187 TO: US 67



EXHIBIT F



Texas Department of Transportation

DEWITT C. GREER STATE HIGHWAY BLDG. • 125 E. 11TH STREET • AUSTIN, TEXAS 78701-2483 • (512) 463-4585

cc: CRM

RECEIVED

JUN 04 2002

21 May 2002

SECTION 108: Determination of NRHP Eligibility
Johnson County, FTW
CSJ 2118-01-008

MAY 22 2002

SH 121 from FM 1187 to US 67

Bob Brinkman
History Programs Division
Texas Historical Commission
Austin, Texas 78711

RECEIVED

MAY 22 2002

TEXAS HISTORICAL COMMISSION

Dear Mr. Brinkman:

In accordance with the provisions of our Statewide Programmatic Agreement for Cultural Resources, we are initiating coordination with your agency regarding National Register eligibility of one property located within the project's area of potential effect (APE). This federally funded project will widen an existing transportation facility and extend its alignment in northern Johnson County. The project would acquire additional right-of-way. A map and photos are included.

Organized in 1867, Johnson County sustained a largely agricultural economy throughout the subsequent decades. Completion of the Gulf, Colorado and Santa Fe line through the county in 1881 spurred the value of agricultural production to \$1,654,960 by 1890. Cotton production led this increase, with 18,826 bales ginned in the county in that year. Nearly half of the approximately 3,000 farms in the county were involved in sharecropping cotton by the turn of the century. The county's population remained nearly 80% rural throughout this period, reaching a peak of 37,286 in 1920. Declining agricultural revenues and the effects of the Great Depression prompted a steady decline in subsequent decades.

As detailed below, field survey efforts identified only one pre-1955 property within the APE, which ranged from 500' to 1300' based on project parameters. Severe alterations and the loss of its historically associated outbuildings preclude eligibility for this modest architectural resource, however. Despite its probable role in the region's agrarian economy, this property is therefore *not eligible* for listing in the National Register of Historic Places.

ID #	LOCATION	PROPERTY TYPE	SUBTYPE	STYLISTIC INFLUENCE	DATE	INTEGRITY ISSUES	NR Elig.
1	FM 917, west of FM 1902	Domestic	Ctr. hall plan farmhouse	NA	c.1890	porch infill, additions, fenestration changes	No

07/16/2002 10:57
07/16/2002 09:22 FAX

TXDOT FT WORTH → 82145264433

NJ, 130 1/04
004

SH 121 from FM 1187 to US 67, 21 May 2002, page two

We request your written concurrence with this determination of eligibility within 30 days of receiving this letter. If you have any questions or comments concerning this project, please contact me at 416-2657.

Sincerely,

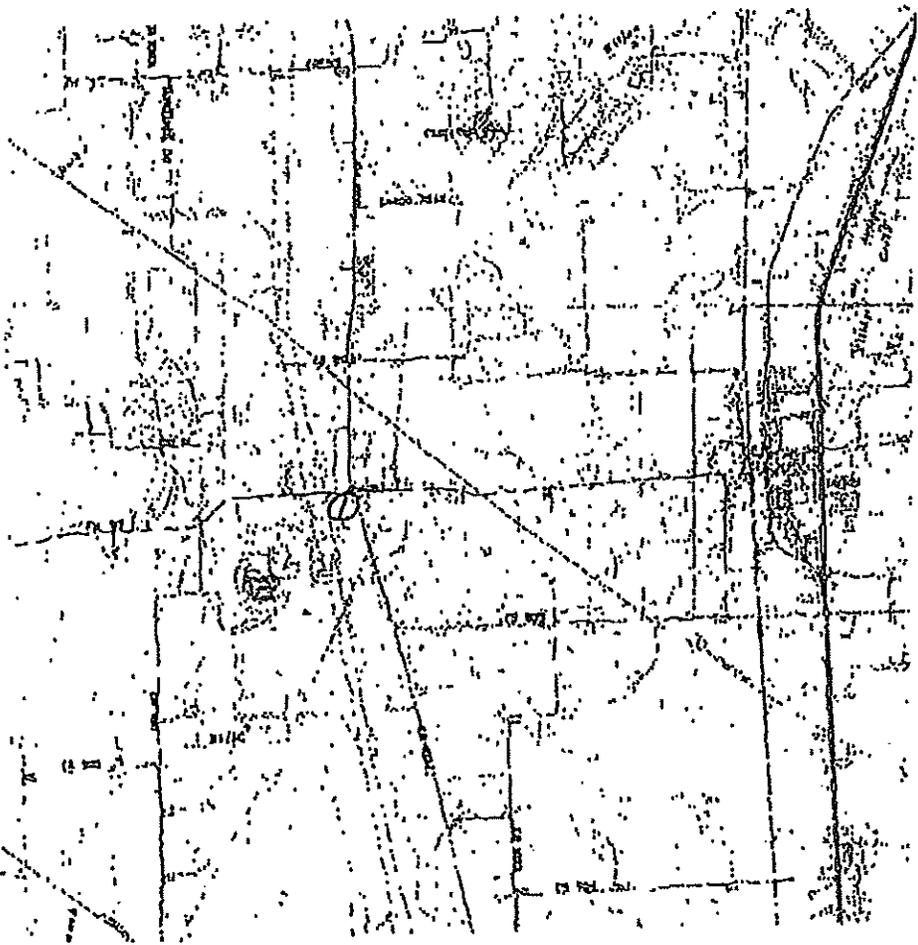


Bruce Jensen
Architectural Historian
Environmental Affairs Division

attachments

NOT ELIGIBLE
for listing in the
National Register of Historic Places
PROJECT MAY PROCEED
by R. H. B. O.
for F. L. Luperón O'Neil
State Historic Preservation Officer
Date 30 MAY 2002

SITE LOCATION MAP



07/16/2002 10:57
07/16/2002 09:21 FAX

TXDOT FT WORTH + 62145264433

NJ. 130 402
49002

pg: 1 of 1

ETS

ARCHEOLOGICAL COORDINATION
Impact Evaluatons, No Further Work Recommended

RECEIVED
Date: 05/31/2002
To: 05/31/2002

JUN 03 2002

(Section 106 and ANTIQUITIES CODE OF TEXAS)

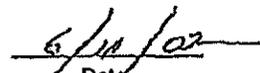
Date: 05/31/2002

TEXAS HISTORICAL COMMISSION

COUNTY	DISTRICT	PROJECT	CSJ	*F30/T20 Concur, no further work	*F10/T10 Unable to Concur
El Paso	El Paso	Loop 376	2552-01-021	✓	
Hansford	Amarillo	F.M. 520	1621-01-013	✓	
Hardeman	Childress	FM 1166	1312-01-013	✓	
Johnson	Fort Worth	SH 121	2118-01-008	✓	
Wise	Fort Worth	FM 2123	1606-02-012	✓	

Number of Projects: 5


F. Lawrence Oaks
State Historic Preservation Officer and Executive Director


Date



Texas Department of Transportation

P.O. BOX 6868 • FORT WORTH, TEXAS 76115-0868 • (817) 370-6500

P.I
2118-02-008

May 31, 2002

Mr. James Greenwade
Natural Resources Conservation Service
W.R. Poage Federal Building
101 South Main Street
Temple, Texas 76501-7682

Re: Proposed SH 121 in Johnson County
From: FM 1187
To: US 67
CSJ: 2118-02-008

Dear Mr. Greenwade:

The Texas Department of Transportation (TxDOT) is currently preparing an Environmental Assessment for the proposed SH 121 project in Johnson County. At this time we are requesting a farmland conversion impact rating for this proposed project from you.

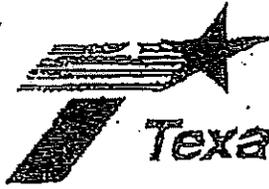
Enclosed is a copy of form AD-1006, a location map and a copy of the Johnson County Soil Survey Maps with the alignment annotated in blue. The project can be found on United States Geological Survey Quadrangles for Joshua, Primrose and Cleburne West Texas.

Please send your response to me at the address above. We previously sent this information to the Cleburne Field Office on July 9, 2001, therefore, and expedited response will be appreciated. If you have any questions or need additional information, please call me at (817) 370-6755. Thank you for your assistance with this matter.

Sincerely,

Robert Hall
Environmental Coordinator

Enclosures



Texas Department of Transportation

DEWITT C. GREER STATE HIGHWAY BLDG. • 125 E. 11TH STREET • AUSTIN, TEXAS 78701-2483 • (512) 452-8585

June 6, 2002

Section 106 Consultation
Tarrant County, Fort Worth District
C/SJ 0504-02-008 SH 121 T
Re: Proposed Alternative Shift - Alternative "C"

RECEIVED

JUN 17 2002

Dr. James E. Bruseth
Division of Archeology
Texas Historical Commission
P.O. Box 12276
Austin, Texas 78711

CONCERN

TEXAS HISTORICAL COMMISSION

by [Signature]
for [Signature] Law and
State Historical Commission Officer

Date [Signature]

Dear Dr. Bruseth:

The proposed road widening project would be undertaken with federal funds. In accord with the Programmatic Agreement (PA) among the Advisory Council on Historic Preservation, the Federal Highway Administration, the Texas Historical Commission (THC), and TxDOT, and the Memorandum of Understanding (MOU) between TxDOT and THC, we hereby continue consultation under Section 106 of the National Historic Preservation Act and the Antiquities Code of Texas.

The proposed project would construct a segment of State Highway 121 between IH 20 in Fort Worth and FM 1187 and is part of a larger project that would eventually extend to US 67 in Cleburne, Johnson County. This segment of SH 121 is located entirely within Tarrant county and has been designated SH 121 T. Recently an alignment shift has been proposed at the southern end of SH 121 T near Cleburne Crowley Road, where this segment joins the next segment of SH 121, designated SH 121 South. The proposed alignment shift would utilize Alternative "C" on the attached maps. Several archeological studies have already been conducted in association with this project.

In 1994, TxDOT conducted an archeological survey of almost the entire proposed SH 121 T and SH 121 South alignments. The survey extended from 0.8 miles north of IH 820 in Fort Worth, Tarrant County to US 67 in Cleburne, Johnson County. The survey included shovel testing of a segment, designated Alternative "A" and "B" on the attached maps, that is located approximately 1.500 ft east of the proposed alignment shift, designated Alternative "C" on the attached maps. Despite shovel testing, no archeological sites were identified within the project area. One site, 41TR137, a surface lithic scatter, was observed 30m east of the project area. Please note that site 41TR137 is located over 1.5 miles northeast of the proposed alignment shift. Furthermore, the site is located east of the area surveyed in 1994 (designated as Alternative "A" and "B" on the attached maps) and Alignment "C", the proposed alignment shift, is located west of the area surveyed in 1994.

June 6, 2002

In 1999, Hicks and Company conducted an archeological survey of the northern portion of SH 121 T, where the proposed alignment crosses the West Fork of the Trinity River. One prehistoric archeological site, 41TR170, was identified during the survey. On March 28, 2000 TxDOT recommended that site 41TR170 be tested and that no further work was required within the remainder of the SH 121 T project area. On April 24, 2000, your office concurred. Right of entry in the site was denied by the property owner and currently testing is on hold pending ROW acquisition. This area is over 6 miles north of the proposed alignment shift designated Alternative "C" and is located in an entirely different environmental setting.

In May of 2002 Geo-Marine, Inc. performed an impact evaluation of the segment of SH 121, located south of SH 121 T. This segment has been designated SH 121 South (CSJ: 2118-02-008). The impact evaluation covered the entire length of the proposed SH 121 South project and extended from the southern terminus of the SH 121 T project (600 ft northeast of Cleburne Crowley Road) to US 67. No archeological sites and no settings with reasonable potential to contain archeological historic properties or SAL's were observed. The impact evaluation report dated May 22, 2002 noted that the entire project area is located in an upland setting and that because the upland setting lacks a permanent water source, archeological sites are unlikely to occur within the project area. Furthermore, the report concluded that the soils within the project area are too shallow to be conducive to retaining archeological deposits.

The proposed SH 121 T alignment shift would be located in a setting very similar to that described in the Geo-Marine impact evaluation report. Alternative "C" is located in an upland setting with no permanent source of water. The Geologic Atlas of Texas, Dallas Sheet (Bureau of Economic Geology: 1972) indicates that Alternative "C" is located in an area mapped as Lower Cretaceous Pawpaw Formation, Lower Cretaceous Waco Limestone, and Lower Cretaceous Grayson Marl and Main Street Limestone undivided. There are no alluvial settings mapped within Alignment "C". According to the Soil survey of Tarrant County [Map Sheets 54 and 61] Alternative "C" crosses shallow upland soils. Furthermore, these shallow soils have been previously disturbed by agricultural activities. These soils are considered too shallow and too disturbed to be conducive to retaining archeological deposits.

A check of the Texas Archeological Sites Atlas revealed no recorded archeological sites within or adjacent to Alternative "C". Because Alternative "C" is located in an area of ancient geologic deposits in an upland setting that is devoid of a permanent water source and consists of previously disturbed shallow soils it is concluded that the area does not include settings with reasonable potential to contain archeological historic properties or SAL's. Recent archeological work in the vicinity of the proposed alignment shift, described above, supports this conclusion.

We request your concurrence that the proposed alignment shift labeled Alternative "C" does not contain settings with reasonable potential to contain archeological Historic Properties (36 CFR 800.16.(1)) or SAL's (13 TAC §26.12) and that no further archeological work is required within the limits of Alternative "C". In the unlikely event that archeological materials are discovered during construction, work in the area of discovery will cease and accidental discovery procedures will be implemented in accordance with the provisions of the Programmatic Agreement (PA) between TxDOT and the THC.

Dr. James E. Bruseth

-3-

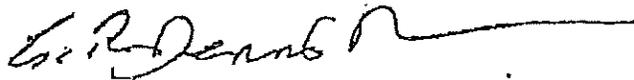
June 6, 2002

If you have any questions or need more information, please contact Mike Jordan at 512/416-2635.

Sincerely,



Michael Jordan, Staff Archeologist
Archeological Studies Program
Environmental Affairs Division



G. R. Dennis Price
Environmental Specialist
Environmental Affairs Division

Attachments



United States
Department of
Agriculture

Natural
Resources
Conservation
Service

101 South Main
Temple, Texas
76801-7602

DOT OR FT. WORTH
TXDOT MAILROOM

JUN 10 2002

Subject: LNU-Farmland Protection-
SH 121 Highway Proposed
Johnson County, Texas

June 7, 2002

Texas Department of Transportation
P.O. Box 6868
Fort Worth, Texas
76115-0686

Attention: Robert Hall, Environmental Coordinator

We have reviewed the information provided concerning proposed S. H. 121 in Johnson County, Texas. This is part of an Environmental Evaluation for the above-referenced highway being prepared for the TxDOT and FHWA. We have evaluated the soils for this project as required by the Farmland Protection Policy Act (FPPA).

The proposed project does contain Prime and Statewide Important Farmland soils as defined by the FPPA. Several map units identified in the Soil Survey of Johnson County are classified as Prime Farmland and Statewide Important Farmland. Approximately 431.4 acres of land will be acquired of which about 298.4 acres is classified as Important Farmland by the FPPA. These soils had a composite score of 74 and the Total Points on Part VII of the AD-1006 is 142. This site will require no additional consideration since the rating score is less than 160. The FPPA states, "Sites receiving a total score of less than 160 need not be given further consideration for protection and no additional sites need to be evaluated", 7CFR Part 658.4 (c) 2.

Attached is the completed AD-1006 (Farmland Conversion Impact Rating) form for this project indicating the exemption status of this proposed project.

Thanks for the quality resource materials you submitted to evaluate this project. If you have any questions please call James Greenwade at (254)-742-9960 or Sam Brown at (254)-742-9854, Fax (254)-742-9859.

Thanks,

James M. Greenwade
Soil Scientist
Soil Survey Section
USDA-NRCS, Temple, Texas



GEO-MARINE, INC.

550 East 15th Street
Plano, Texas 75074

phone: 972.423.5480
email: gmi@geo-marine.com

fax: 972.422.2736
website: geo-marine.com

22 May 2002

Nancy A. Kenmotsu, Ph.D., Supervisor
Archeological Studies Program
Environmental Affairs Division
Texas Department of Transportation
125 E. 11th Street
Austin, Texas 78701-2483

Impact Evaluation WA 57022PD002—Tarrant and Johnson Counties *2118-01-008 and*
SH 121 from 600 ft northeast of Cleburne-Crowley Road to US 67 (CSJ 2118-02-008)

Dear Dr. Kenmotsu:

Geo-Marine, Inc., performed an archeological impact evaluation for the above-referenced project in Johnson and Tarrant counties, Texas, on 2 May and 9 May 2002. Melissa Green, Michelle Wurtz, Denise Hough, and Duane Peter completed the impact evaluation. This report presents the findings and recommendations for the project areas.

This project proposes to construct a four-lane, divided highway on a new alignment from US 67 (the Cleburne Bypass) northward to approximately 183 m (600 ft) northeast of Cleburne-Crowley Road (FM 1902). The total project corridor is approximately 22.5 km (14 mi) long, and the right-of-way width varies from 73 m (240 ft) to 122 m (400 ft) at the interchanges, covering a total area of approximately 240 ha (594 acres; Figures 1, 2, and 3). Additionally, a new county road is to be constructed north from FM 917 and just west of the new SH 121 alignment (see Figure 2).

The purpose of the impact evaluation was to provide the following information:

1. Identifying areas where existing ROW is too disturbed or otherwise unlikely to contain intact archeological deposits;
2. Identifying areas/depths where existing ROW is likely to be too disturbed or otherwise unlikely to contain intact archeological deposits;
3. Identifying areas/depths where intact deposits may be present within existing ROW;
4. Identifying areas where survey should and should not be performed, including identifying locations where mechanical trenching and/or shovel testing should be performed. If trenching is recommended, the report shall indicate obstacles (if any) to machine access.

Prior to fieldwork, site files at the Texas Archeological Research Laboratory (TARL) were searched for information on any known sites within 1.6 km (1 mi) of the project corridor. No sites were identified within that perimeter. Geologic, soils, and historic maps were also consulted.

The majority of the project area is underlain by Lower Cretaceous geological formations. The northernmost one-fourth consists of undivided middle shale and lower limestone units of Weno Limestone, Denton Clay, Fort Worth Limestone, and Duck Creek Formation. Most of the southern three-fourths of the project area is composed of Grayson Marl and Main Street Limestone undivided. In addition, small areas of the Upper Cretaceous Woodbine Formation and Holocene alluvium occur within the project area (Bureau of Economic Geology 1972).

Soil surveys of Tarrant (Ressel 1981) and Johnson counties (Coburn 1985) indicate 28 soil map units within the project area. These units include the following:

- Sanger clay, 1 to 3 percent slopes; Sanger clay, 3 to 5 percent slopes
- Bolar clay loam, 1 to 3 percent slopes; Bolar clay loam, 3 to 8 percent slopes
- Frio silty clay, occasionally flooded
- Aledo-Bolar complex, 2 to 8 percent slopes; Aledo-Bolar complex, 5 to 20 percent slopes
- Purves clay, 0 to 3 percent slopes
- Lindale clay loam, 1 to 3 percent slopes
- Heiden clay, 1 to 3 percent slopes
- Ponder clay loam, 1 to 3 percent slopes; Ponder clay loam, 3 to 5 percent slopes
- Wilson silty clay loam, 0 to 1 percent slopes; Wilson silty clay loam, 1 to 3 percent slopes
- Lott silty clay, 1 to 3 percent slopes
- Culp clay loam, 0 to 3 percent slopes
- Pursley clay loam, frequently flooded
- Hassee fine sandy loam, 0 to 1 percent slopes
- Crosstell fine sandy loam, 3 to 8 percent slopes
- Rader fine sandy loam, 0 to 3 percent slopes
- Coving loamy fine sand, 0 to 3 percent slopes
- Birome-Rayex complex, 5 to 20 percent slopes
- Slidell clay, 0 to 1 percent slopes; Slidell clay, 1 to 3 percent slopes
- Lewisville silty clay, 1 to 3 percent slopes; Lewisville silty clay, 3 to 5 percent slopes
- Burleson clay, 0 to 1 percent slopes, and Burleson clay, 1 to 3 percent slopes

The entire project area is situated in an upland setting. The area is gently rolling or flat, and small tributaries to West Buffalo and Rock creeks cross the project corridor. Much of the corridor is in pasture or plowed fields, some of which have been allowed to go fallow. Wildflowers were in full bloom in many of the fields at the time of the survey.

Fieldwork consisted of visual inspection of the project area. Three crewmembers were present for each day of fieldwork; two archeologists walked the corridor while a third moved the vehicle ahead. Disturbances were noted, and drainage profiles and cleared or eroded areas were examined for cultural materials and contextual integrity potential. No shovel tests were excavated. Photographs were taken of disturbances, drainage crossings, project area overviews, and representative sections of the roadway. The project corridor investigations consisted of a pedestrian walk-over that started at the southern end and ended at the northern terminus. Approximately 14.5 km (9 mi) of the corridor were examined on the first day, and the remaining 8 km (5 mi) covered on the later date.

The walk-over began along the southern segment of the corridor at CR 1216 (see Figure 1). Between CR 1216 and Industrial Boulevard are a recently plowed field and a pasture (Plate 1). A waterline that runs north-south in the fields has a large release valve at its intersection with CR 1216. The pasture has been terraced at one time. Some small cobbles were noted across the surface of both fields. Telephone and cable lines run along the north side of Industrial Boulevard. Between Industrial Boulevard and Highway 171 are a pasture and a small field of oats. Underground fiber optics, telephone, and cable lines were observed between, and following, the rights-of-way of Highway 171 and the BNSF railroad.

The section between Highway 171 and Vaughn Road consists primarily of pasture and fallow fields (see Figure 1). A maximum of approximately 100 m (328 ft) has been added to widen this segment for an alternative alignment. A new water line in the field starts at the corner of the fence and follows the proposed highway right-of-way for slightly more than 0.8 km (0.5 mi) to the Cleburne city limits and then turns west. A high voltage power line also extends east-west at the city limits. This area is currently in pasture. Two West Buffalo Creek tributaries—small, ephemeral, upland drainages that are little more than ditches carrying very little water, and thus with no potential for archeological remains—cross this segment (Plates 2 and 3). The property from the proposed Sparks Road to Vaughn Road consists of a fallow field. New industrial complexes are east and west of the project corridor just north of a lake (appearing on the aerial photographs, but not on the earlier 1960s topographic quadrangle). The right-of-way for the proposed Sparks Road was apparent by the cleared and leveled ground and by a drainage ditch and overhead telephone or electric lines.

Between Vaughn Road and CR 904 are several tracts, particularly along West Buffalo Creek, to which access was not granted (see Figure 1). Based on the lack of soil change and depth of cutbanks noted along West Buffalo Creek in the previous segment, the potential for archeological deposits along this drainage is slight. In areas to which access had been granted, the landscape is predominantly in pasture, some with stands of mesquite. Two underground natural gas pipelines cross the project right-of-way in this segment: one trending northwest-southeast approximately 1.3 km (0.8 mi) north of Vaughn Road, and the other in a northeast-southwest direction approximately 0.7 km (0.4 mi) farther north. An east-west, overhead, high-voltage power line crosses this segment approximately 0.45 km (0.28 mi) south of CR 904. The two small tributaries that cross this segment are also no more than ephemeral, upland drainages with no likelihood for archeological deposits.

Along the middle segment of the project corridor (see Figure 2), several new manufactured homes, with overhead power lines and underground telephone and cable lines, have been built on the north side of CR 904 (Plate 4). From CR 904 to FM 917, the slightly more rolling landscape is primarily in pasture, some with stands of mesquite. Much of this segment is used for grazing horses, and many electric fences were observed. The small drainages that the right-of-way crosses are nothing more than low-lying areas that act as runoff channels from surrounding fields. Since pedestrian access was also denied at the northern end near FM 917—an area of several new homes, as well as a subdivision just below Brushy Knob—a windshield survey was conducted for the section from CR 909 to FM 917 by way of the subdivision roads. Overhead and underground utilities have been installed for these new homes.

Much of the project corridor section between FM 917 and CR 910 was not accessible for pedestrian examination (see Figure 2). Approximately 225 m (738 ft) west of the proposed SH 121 right-of-way, however, a new, north-south-oriented county road is proposed on the northern

side of FM 917. Approximately 300 m (984 ft) of this proposed county road is currently a well-maintained dirt road leading to a trailer home. The remainder of the proposed road is in pasture, although some hardwood trees are scattered about.

The corridor section that begins at CR 910 and extends to CR 1016 consists primarily of pasture land, although a sparsely wooded area of mesquite was encountered approximately 250 m (820 ft) north of CR 910 (see Figure 2). A new livestock tank is located at the northern end of this wooded area. Several shallow swales feed the tank with runoff water from the surrounding fields. A large, contoured field on the western side of the right-of-way is adjacent to the northern end of the wooded area. All the tributaries through this segment that run to Rock Creek are primarily shallow swales or drainages with no standing or flowing water. Bedrock slabs and cobbles occur at the surface in some portions of this segment as well. At CR 913, more residential areas appear along and adjacent to the project corridor, though there is still open pasture in the corridor. A Brazos Electric Coop power station is located at the intersection of the project corridor and FM 1902. The stretch of this segment between FM 1902 and CR 1016 is made up entirely of pasture (Plate 5).

The northernmost segment of the project corridor extends from CR 1016 to its northern terminus just northeast of FM 1902 (see Figure 3). The property just north of CR 1016 consists of a recently plowed field. Small, fist-sized cobbles were observed in the newly turned dirt. The next property northward, as well as the one extending to the southernmost intersection of the project corridor with FM 1902 at CR 915, was covered in blooming wildflowers at the time of the survey. The tributary to Rock Creek in this area has incised the rolling topography in varying degrees, from nearly level with the surrounding ground surface to at least 1.2 m (4 ft) in depth. The tributary section that crosses the corridor revealed a small cutbank filled with cobble lenses (Plate 6), apparently the result of high-energy deposition.

The section from CR 915 northward to CR 1015 crosses a large upland pasture that is still used for cattle grazing and was filled with wildflowers (Plate 7; see Figure 3). At the southern end of this section, a buried water line and an aerial power line were observed along CR 915. Two large swales for delivering runoff to a large tank/pond just west of the project corridor were crossed approximately 0.5 km (0.3 mi) north of CR 915. An aerial power line crosses the corridor just north of this pond and extends to a house with a windmill and outbuildings located on CR 1015A at the road's right-angle turn, which is 0.8 km (0.5 mi) north of its intersection with CR 915. Moving northward, the project corridor closely follows the now-abandoned, north-south-trending portion of CR 1015A. A natural gas pipeline crosses the corridor at a northwest-southeast direction approximately 150 m (492 ft) south of CR 1015. This pipeline is mounded and marked with orange posts and signs at fence lines. The walk-over continued northward at the west-turning elbow of CR 1015 at an unnamed tributary to Rock Creek. Some flowing water was seen in the shallow, rock-bottomed channel. The remaining portion of the segment northward to CR 920 is in pasture, covered with wildflowers and some scattered mesquite thickets. Another unnamed tributary in this segment, similar to the tributary just north of CR 1016, has incised the landscape, although no evidence of archeological remains was discovered.

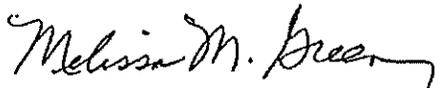
Between CR 920 and CR 1014A (just south of the Tarrant/Johnson county line), a windshield viewing of some of the area that was partially inaccessible was achieved by way of Sparks Lane (see Figure 3). Sparks Lane ends in a cul-de-sac that is in the center of the proposed alignment. Several built and manufactured homes and outbuildings will be impacted. These residences are recent (built after 1976 when the Prinrose 7.5-minute quad was last photoinspected). Pasture

runs from the north side of the cul-de-sac to CR 1014A, and from just north of CR 1014A to FM 1187. A wooden slurry(?) tank/pump was encountered approximately 100 m (328 ft) north of CR 1014A (Plate 8). The quad shows an abandoned outbuilding, though there is no extant evidence other than an electrical pole in approximately the same location. About 100 m (328 ft) north of this slurry tank/pump are two large rectangular ditches that may have been used as earthen silos(?), each about 1.8 m (6 ft) deep (Plate 9). Several outbuildings across Floyd Hampton Road may have been part of this farming complex. The remaining 1.1 km (0.7 mi) of this section to FM 1187 are in open upland pasturage. An east-west overhead power/telephone line crosses the right-of-way 200 m (656 ft) south of FM 1187.

The section between FM 1187 and its intersection with FM 1902 (Cleburne-Crowley Road; see Figure 3) is composed of pasture, with some trees close to FM 1187. A Rock Creek tributary just north of the FM 1187/corridor right-of-way intersection is slightly larger than other drainages seen along the project area and has incised the landscape, based on the rolling topography. Although the cutbanks are from 0.6–1.5 m (2–5 ft) deep, there is no evidence of archeological deposits. On the north side of this tributary the landscape gradually rises and somewhat levels off. Just south of the northernmost right-of-way intersection with FM 1902 is another tributary. This stream also varies in depth due to the topography. At the right-of-way, the cutbanks are only about 0.6 m (2 ft) deep (Plate 10), but just 300 m (984 ft) downstream, the southern cutbank is approximately 3 m (10 ft) in depth; yet no evidence of archeological remains was found. At this point, the right-of-way turns northeasterly and crosses FM 1902 onto yet more upland pasture.

In conclusion, the proposed SH 121 project corridor falls completely in an upland setting. Because the upland lacks a permanent water source and because the soils are too shallow to be conducive to retaining archeological deposits, prehistoric cultural resources are unlikely to occur and no evidence of early historic pioneer farmsteads or ranches was found. Although most of the roads in the general project area have been there for quite some time, occupation along these roads is still, for the most part, sparse. No archeological remains were discovered during the current investigation, and it is thus highly unlikely that any significant archeological resources will be impacted during construction of this portion of SH 121. Therefore, no further archeological investigations are recommended for this project corridor.

Sincerely,



Melissa M. Green
Project Archeologist

Ref: 32020.00.22

REFERENCES

Bureau of Economic Geology

1972 *Geologic Atlas of Texas, Dallas Sheet*. Revised 1988. The University of Texas at Austin.

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1985 *Soil Survey of Johnson County, Texas*. U.S. Department of Agriculture, Soil Conservation Service, Washington, D.C.

Ressel, D. D.

1981 *Soil Survey of Tarrant County, Texas*. U.S. Department of Agriculture, Soil Conservation Service, Washington, D.C.

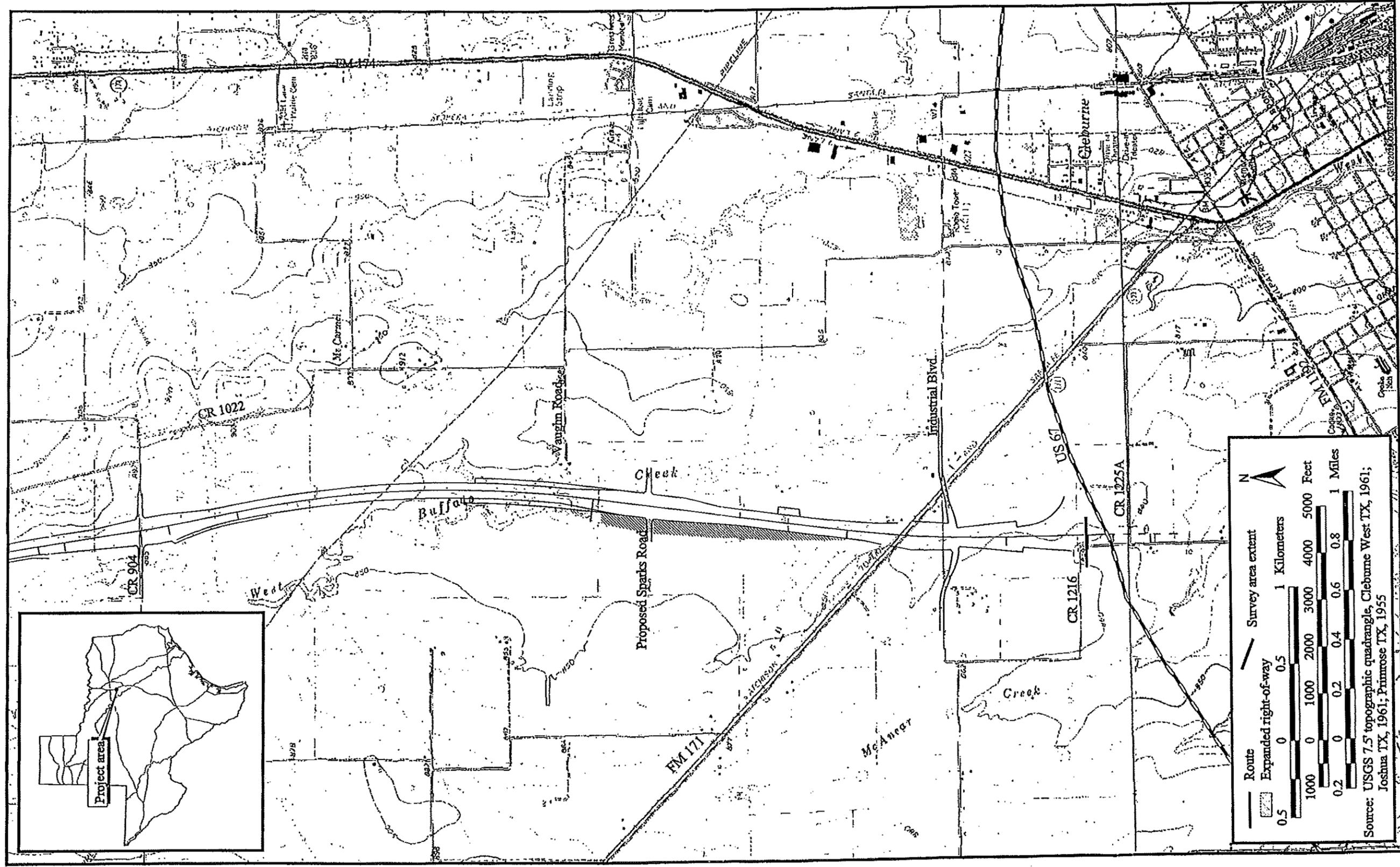
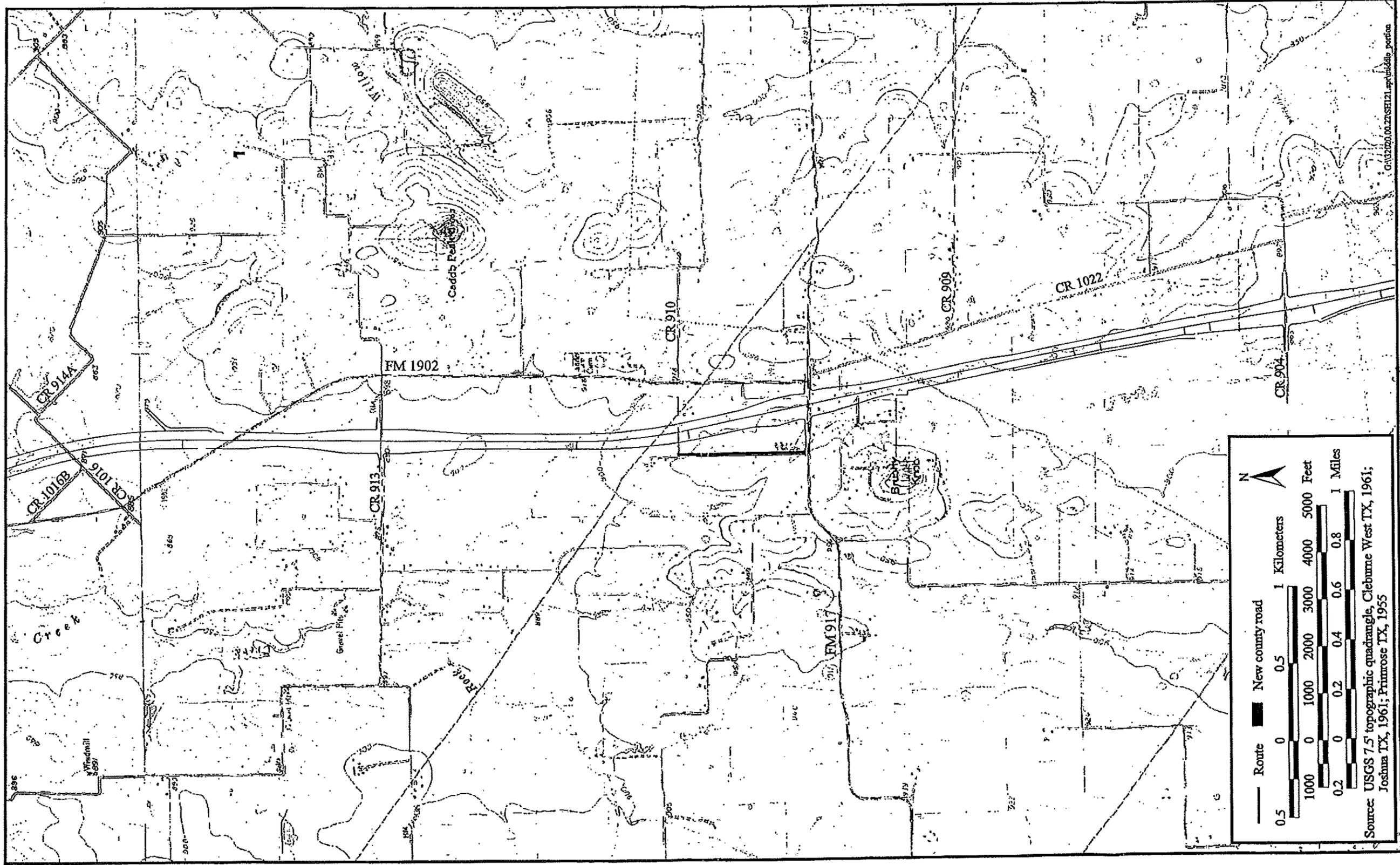


Figure 1. Detail of southern portion of the project area (CSJ 2118-02-008).



Source: USGS 7.5 topographic quadrangle, Cleburne West TX, 1961;
 Joshua TX, 1961; Prinnose TX, 1955

Figure 2. Detail of middle portion of the project area (CSI 2118-02-008).

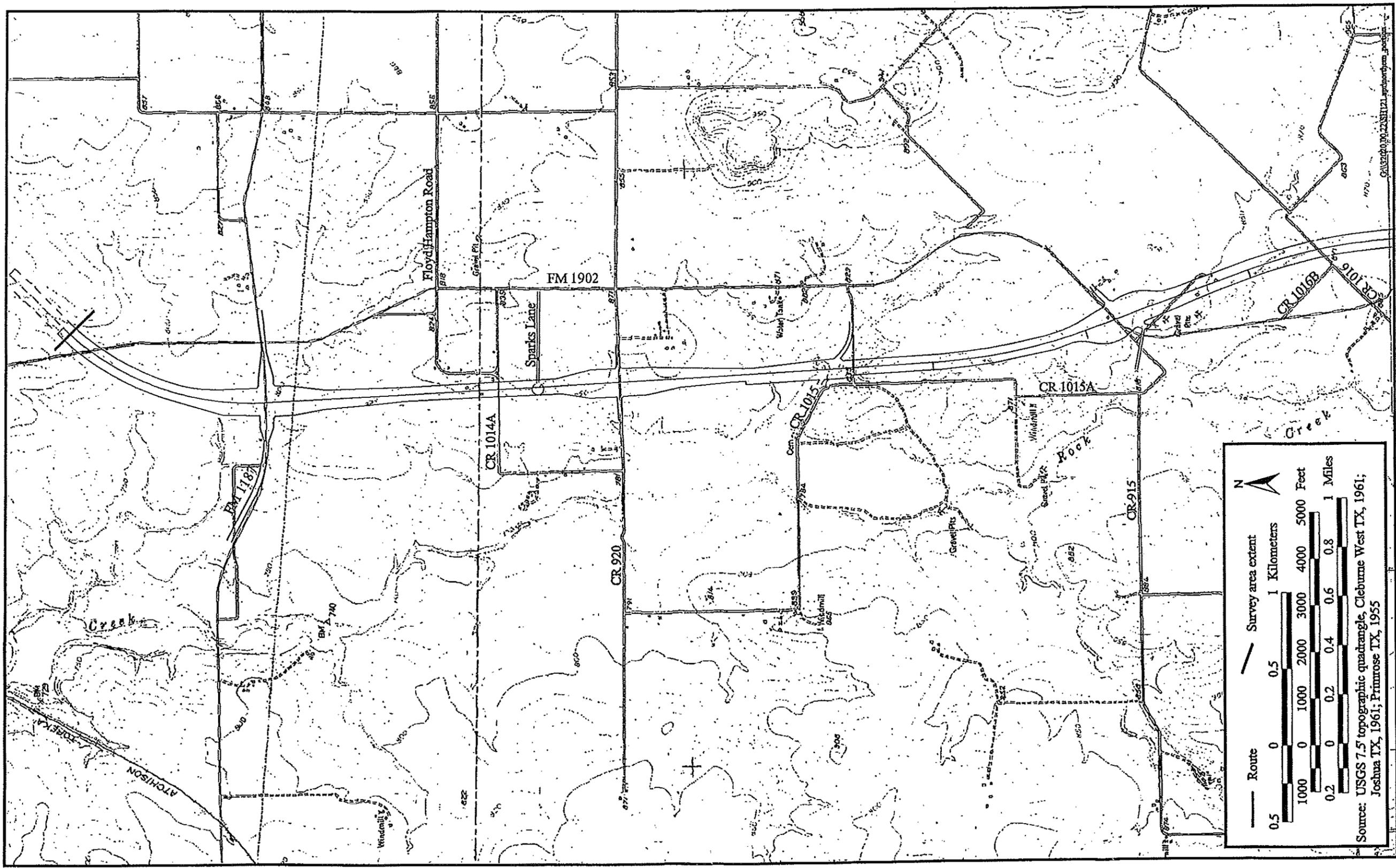


Figure 3. Detail of northern portion of the project area (CSI 2118-02-008).

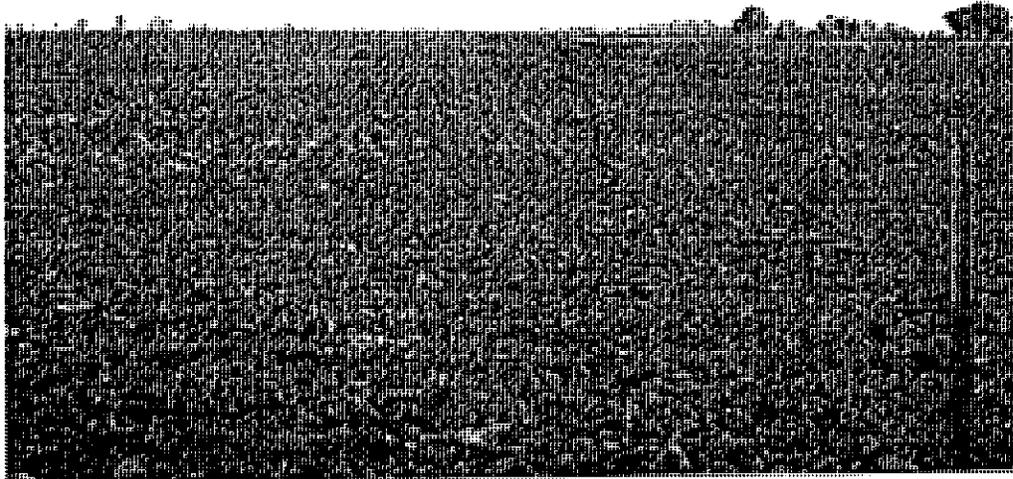


Plate 1. View north. Flooded field north of CR 1244.



Plate 2. View southeast. Tributary channel of West Buffalo Creek.



Plate 3. View east, tributary drainage of West Buffalo Creek.

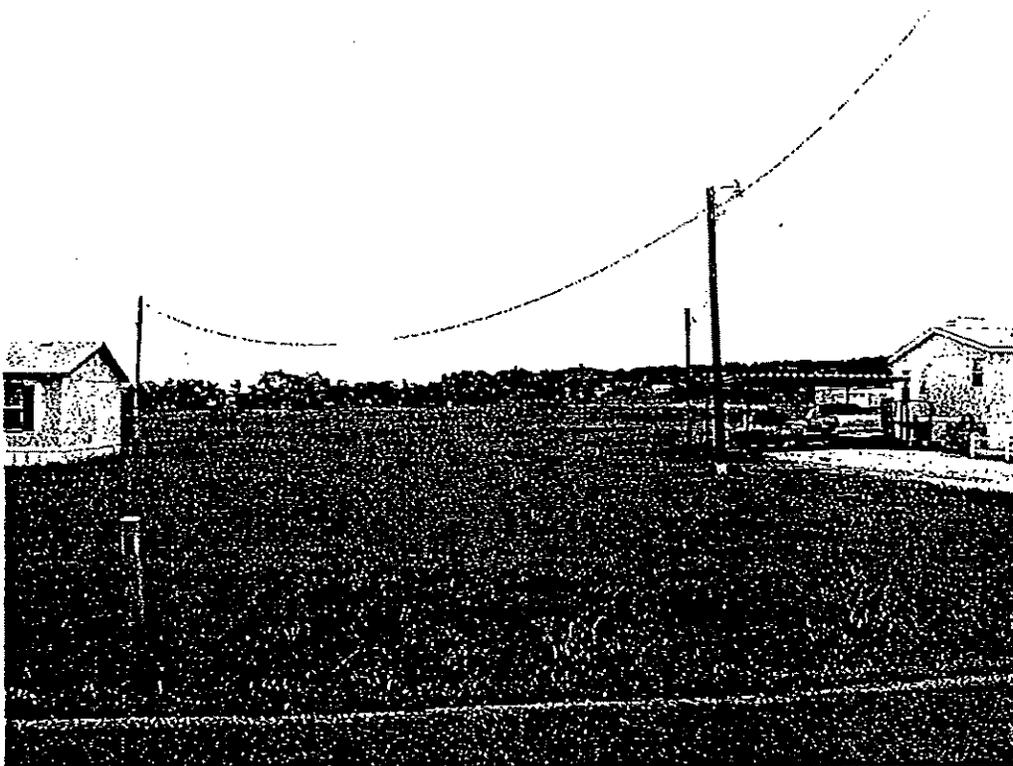


Plate 4. View north, intersection with CR 904.



Plate 5. View north, southern intersection with FM 1902.

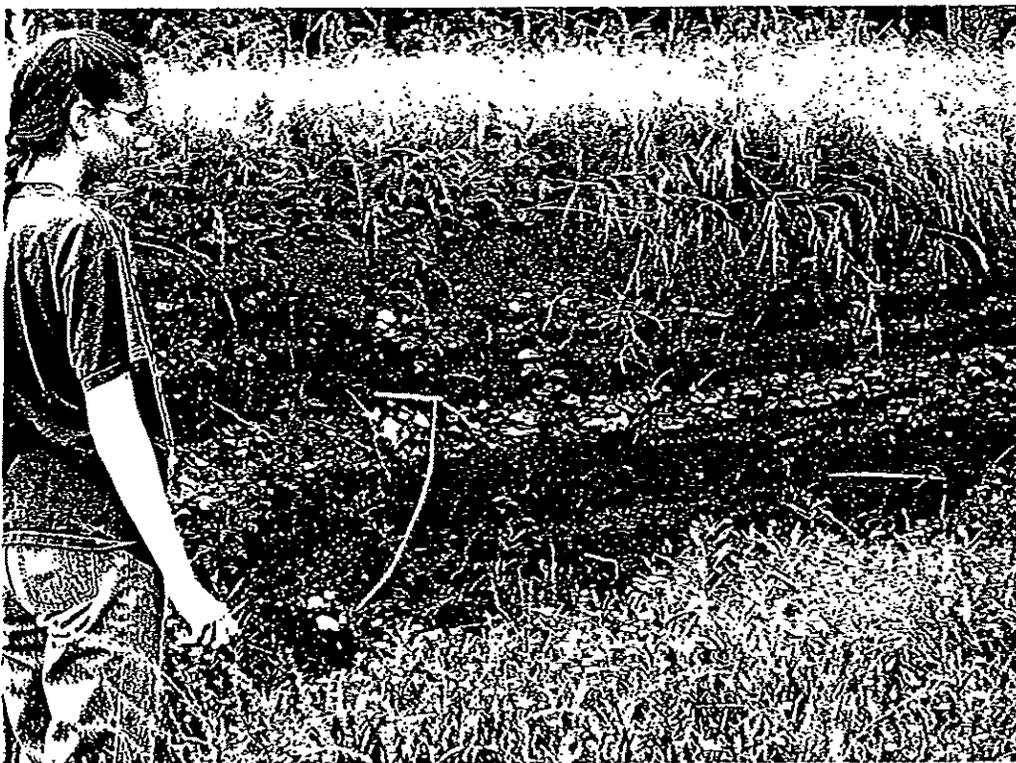


Plate 6. View southeast, creek cutbank, looking toward creek.



Photo 7. View north. Corridor to CR 1015 intersection.



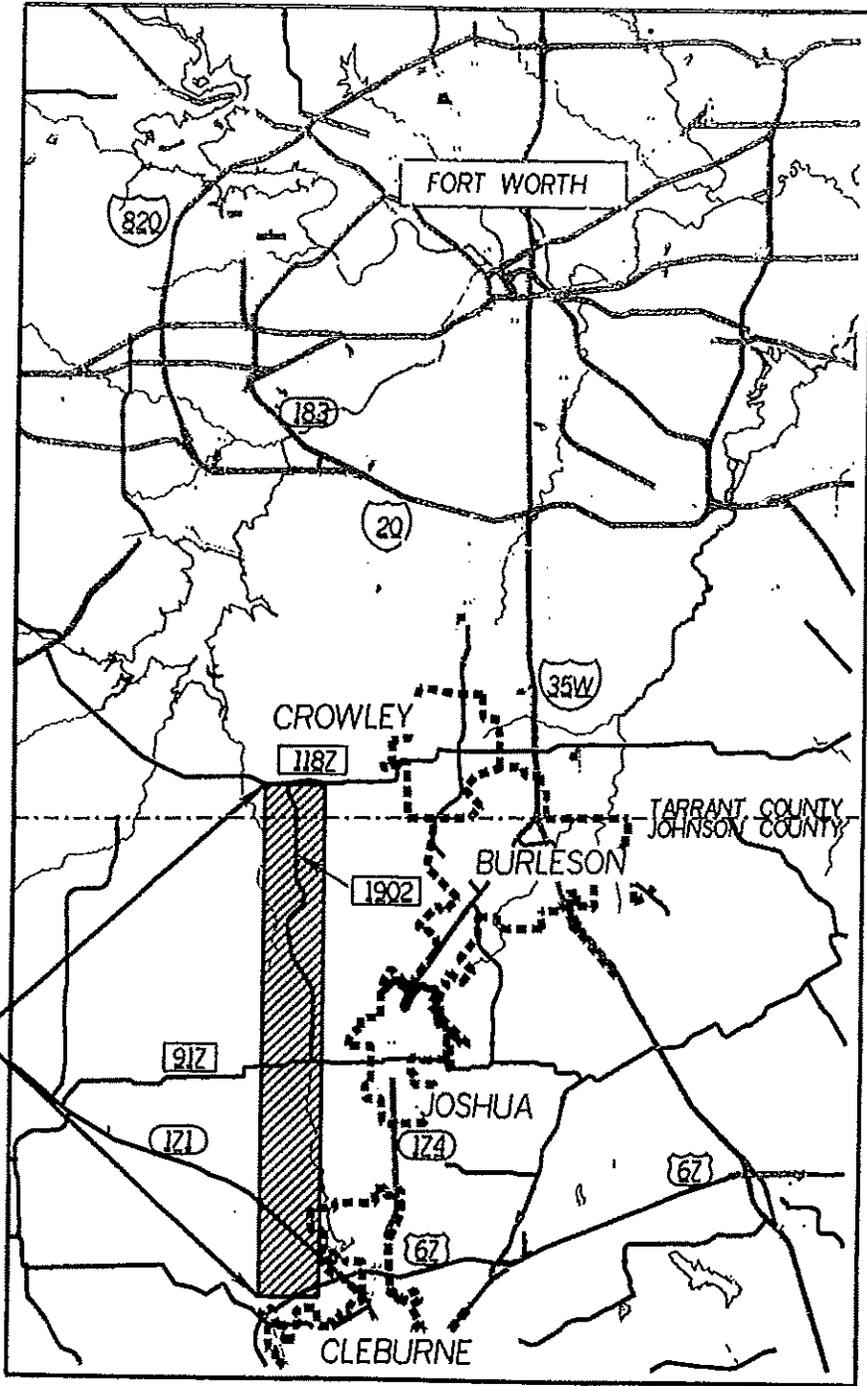
Plate 8. View east. Pump/tank in pasture off CR 1014A.



Plate 9. View west. Circled area (possible trench area) from Floyd Hampton Road.



Plate 10. View southwest. Creek cutback (see south of FMS 1903).



STUDY LIMITS

FROM
FM 1187
TO
US 67

SH 121

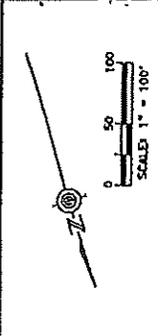
TARRANT AND
JOHNSON COUNTIES

CSJ: 2118-01-008
PROJECT LOCATION MAP



SH 121 SOUTH
FROM : FM 1187
TO : US 67

EXHIBIT A



NOTES:
 1. ALL DIMENSIONS ARE RELATIVE TO THE E.O.P. UNLESS OTHERWISE NOTED. REFER TO HORIZONTAL ALIGNMENT DATA SHEETS FOR ADDITIONAL INFORMATION.
 2.

LEGEND

	EXISTING R.O.W.
	PROPOSED R.O.W.
	DIRECTION OF TRAVEL
	CURVE NAME
	CONCRETE PAVEMENT
	ASPHALT PAVEMENT

SHEET TOTALS

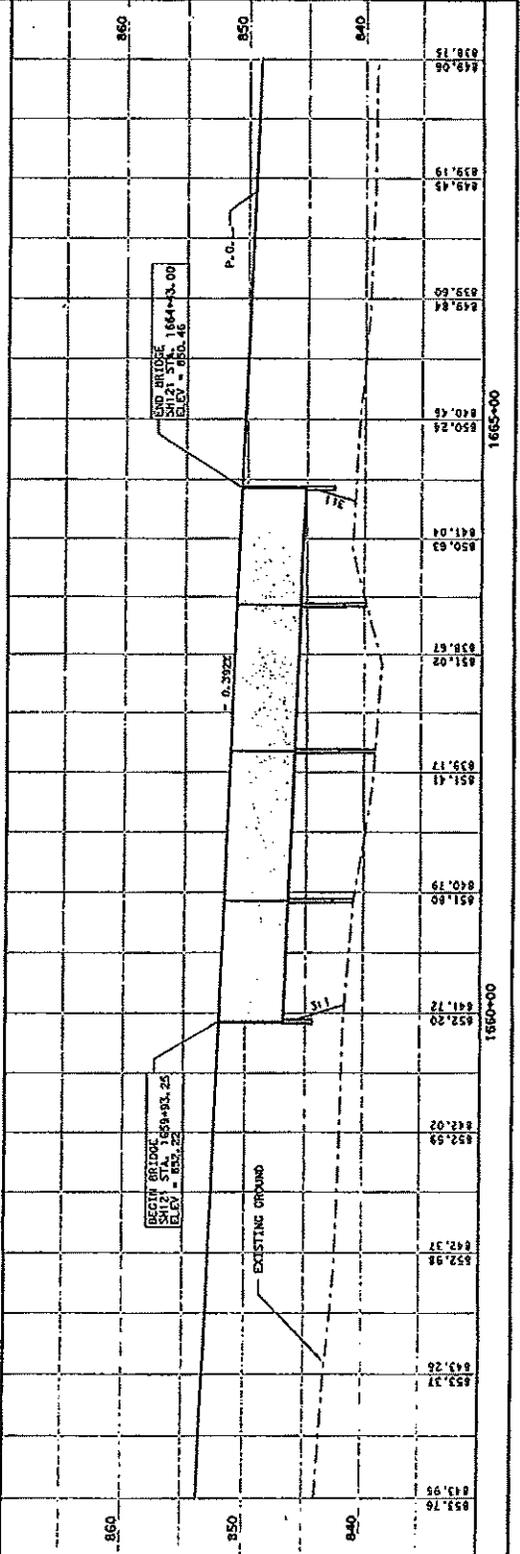
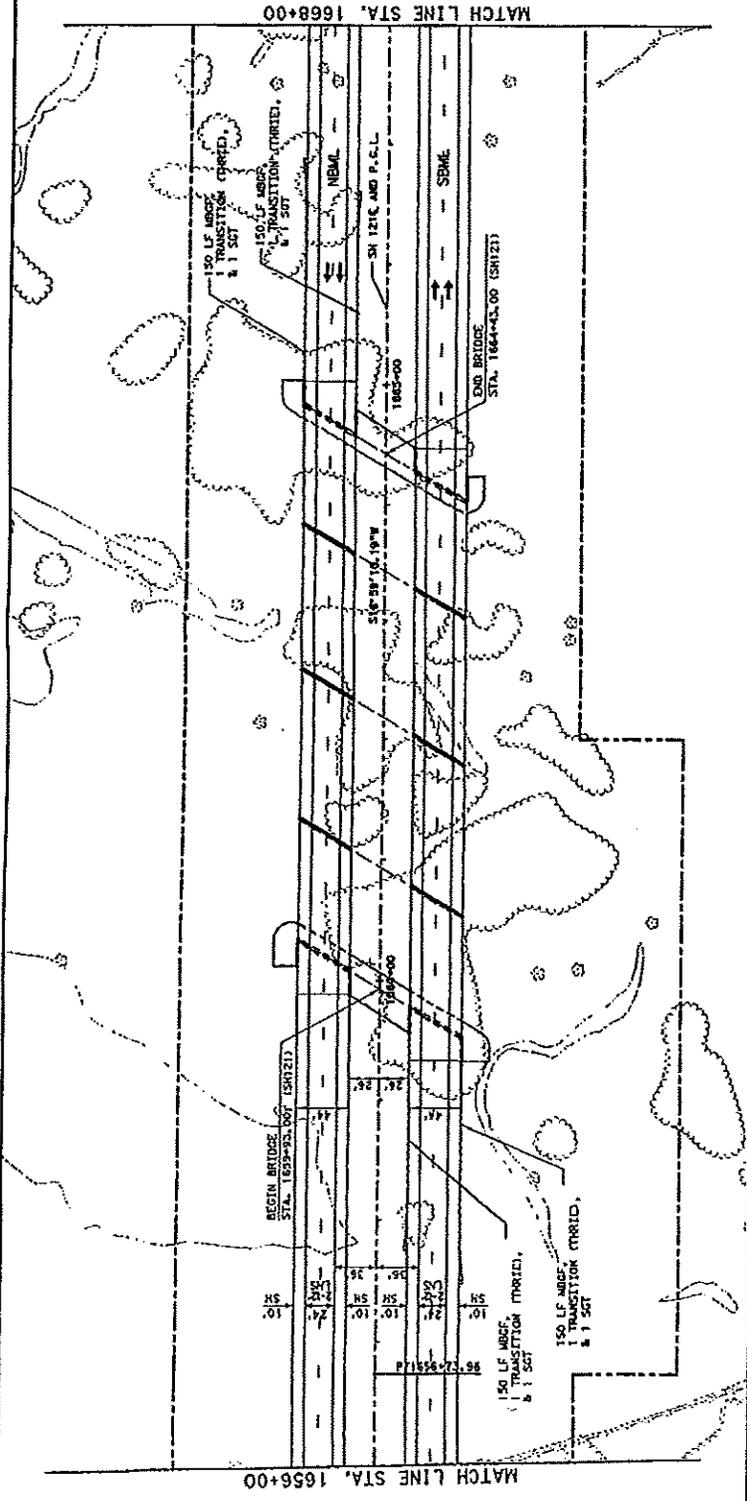
EST.	FINAL	UNIT	DESCRIPTION

INSURE EVERY
 THE STATE OF TEXAS HAS THE HONOR OF BEING THE FIRST TO INSURE EVERY
 STATE OF TEXAS, THE STATE OF TEXAS, THE STATE OF TEXAS, THE STATE OF TEXAS
 IT IS NOT TO BE USED FOR CONSTRUCTION PURPOSES

Texas Department of Transportation
 RODRIGUEZ TRANSPORTATION GROUP
 CONSULTING ENGINEERS

SH 121
PLAN AND PROFILE
 STA. 1656+00 TO STA. 1668+00
 SCALE: PLAN 1"=100' HORIZ.
 SHEET 4B OF 61

DATE	NO.	BY	REVISION





NOTES:
 1. ALL DIMENSIONS ARE RELATIVE TO THE E.O.P. UNLESS OTHERWISE NOTED.
 2. REFER TO HORIZONTAL ALIGNMENT INFORMATION FOR ADDITIONAL INFORMATION.

LEGEND	
---	PLANNED METRICS
---	EXISTING R.O.W.
---	PROPOSED R.O.W.
---	DIRECTION OF TRAVEL
---	CURVE NAME
---	CONCRETE PAVEMENT
---	ASPHALT PAVEMENT

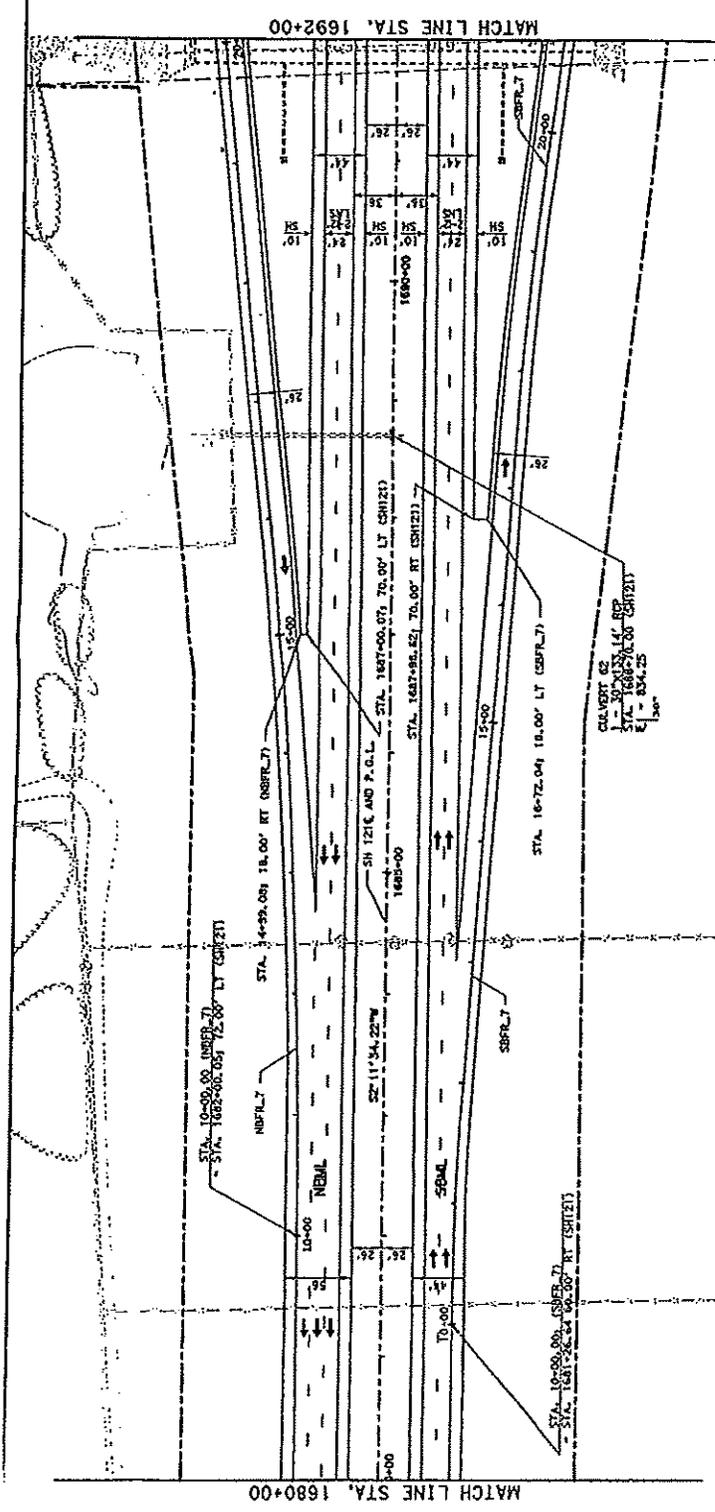
SHEET TOTALS	
EST.	FINAL UNIT DESCRIPTION



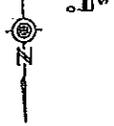
Texas Department of Transportation
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ROCKWELL TRANSPORTATION GROUP
 CONSULTING ENGINEERS

SH 121	
PLAN AND PROFILE	
STA. 1680+00 TO STA. 1692+00	
SCALE: PLAN 1"=100' VERT. 1"=10' HORIZ.	
SHEET NO. 08 OF 91	
PROJECT NO. 030-232 PROJECT No. 030-232	
DATE: 08/01/03	
DRAWN BY: J. BRIDGES	
CHECKED BY: J. BRIDGES	
APPROVED BY: J. BRIDGES	
DATE: 08/01/03	
SCALE: 1"=100' VERT. 1"=10' HORIZ.	
SHEET NO. 08 OF 91	
PROJECT NO. 030-232 PROJECT No. 030-232	
DATE: 08/01/03	
DRAWN BY: J. BRIDGES	
CHECKED BY: J. BRIDGES	
APPROVED BY: J. BRIDGES	
DATE: 08/01/03	



STATION	ELEVATION	STATION	ELEVATION
1680+00	844.28	1685+00	838.19
1680+10	843.85	1685+10	837.76
1680+20	843.42	1685+20	837.33
1680+30	842.99	1685+30	836.90
1680+40	842.56	1685+40	836.47
1680+50	842.13	1685+50	836.04
1680+60	841.70	1685+60	835.61
1680+70	841.27	1685+70	835.18
1680+80	840.84	1685+80	834.75
1680+90	840.41	1685+90	834.32
1681+00	839.98	1686+00	833.89
1681+10	839.55	1686+10	833.46
1681+20	839.12	1686+20	833.03
1681+30	838.69	1686+30	832.60
1681+40	838.26	1686+40	832.17
1681+50	837.83	1686+50	831.74
1681+60	837.40	1686+60	831.31
1681+70	836.97	1686+70	830.88
1681+80	836.54	1686+80	830.45
1681+90	836.11	1686+90	830.02
1682+00	835.68	1687+00	829.59
1682+10	835.25	1687+10	829.16
1682+20	834.82	1687+20	828.73
1682+30	834.39	1687+30	828.30
1682+40	833.96	1687+40	827.87
1682+50	833.53	1687+50	827.44
1682+60	833.10	1687+60	827.01
1682+70	832.67	1687+70	826.58
1682+80	832.24	1687+80	826.15
1682+90	831.81	1687+90	825.72
1683+00	831.38	1688+00	825.29
1683+10	830.95	1688+10	824.86
1683+20	830.52	1688+20	824.43
1683+30	830.09	1688+30	824.00
1683+40	829.66	1688+40	823.57
1683+50	829.23	1688+50	823.14
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1685+00	822.78	1690+00	816.69
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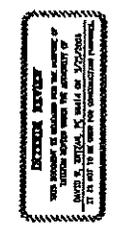
- NOTES:
1. ALL DIMENSIONS ARE RELATIVE TO THE C.O.P. UNLESS OTHERWISE NOTED.
 2. REFER TO HORIZONTAL ALIGNMENT DRAWINGS FOR ADDITIONAL INFORMATION.

LEGEND

---	PLANNED
---	EXISTING R.O.W.
---	PROPOSED R.O.W.
---	DIRECTION OF TRAVEL
---	CURVE NAME
---	CONCRETE PAVEMENT
---	ASPHALT PAVEMENT

SHEET TOTALS

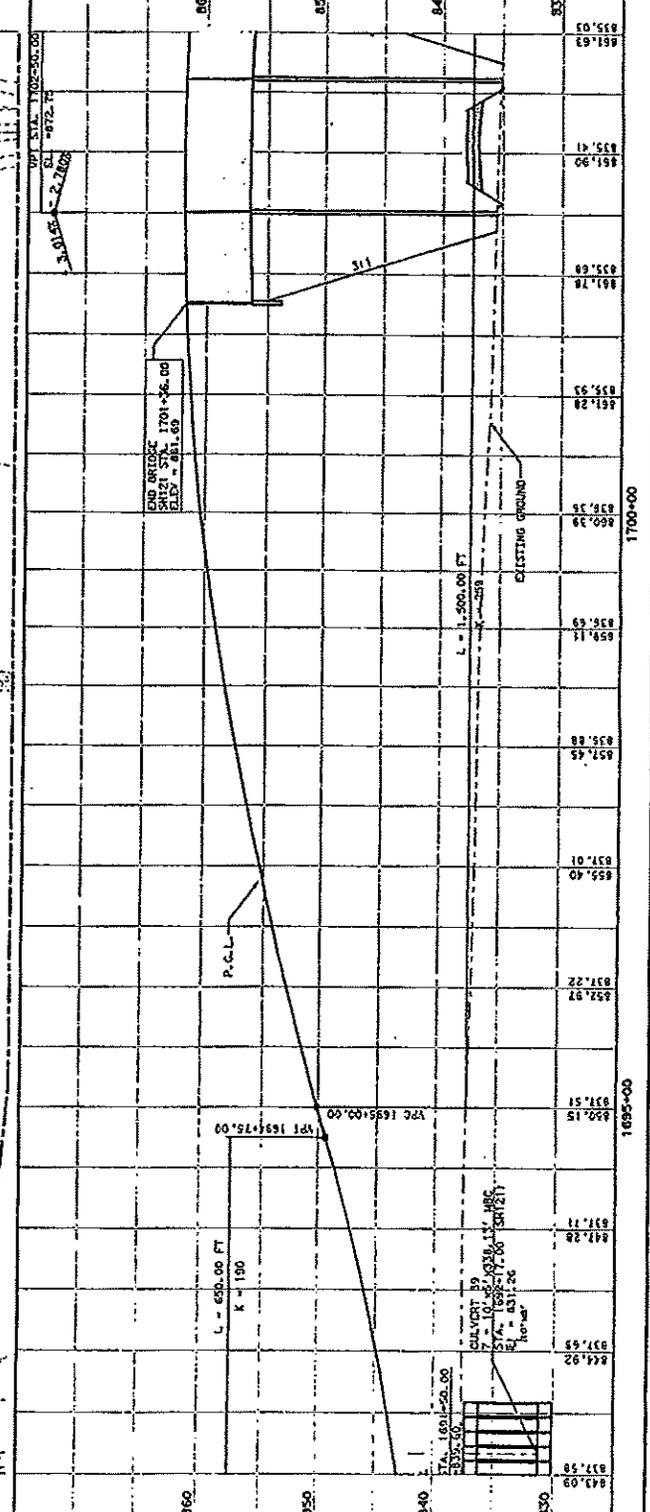
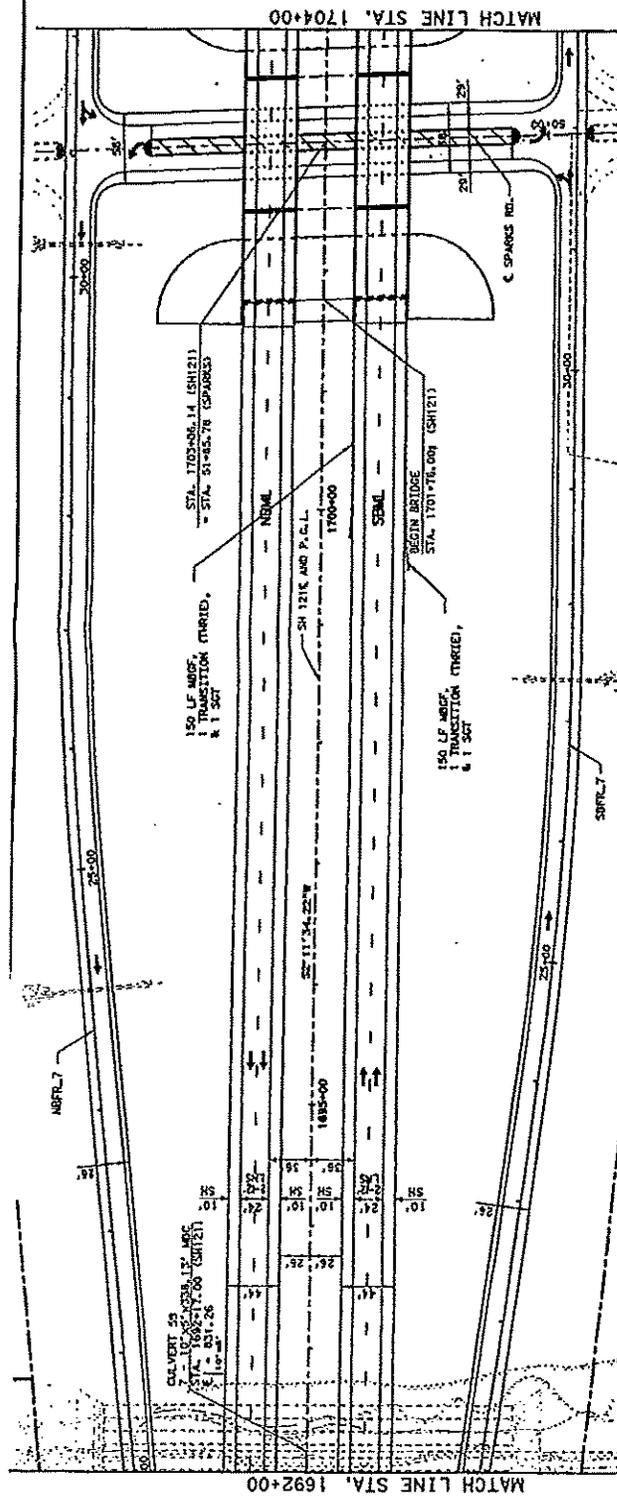
EST.	FINAL	UNIT	DESCRIPTION



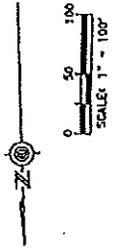
Texas Department of Transportation
 2024
 RODRIGUEZ TRANSPORTATION GROUP
 CONSULTING ENGINEERS

SH 121
 PLAN AND PROFILE
 STA. 1692+00 TO STA. 1704+00
 SCALES
 PLAN SCALE 1"=100' HORIZ.
 PROFILE SCALE 1"=10' VERT.
 SHEET 31 OF 81

NO. OF SHEETS	81	NO. OF SHEETS	81
NO. OF SHEETS	81	NO. OF SHEETS	81
NO. OF SHEETS	81	NO. OF SHEETS	81
NO. OF SHEETS	81	NO. OF SHEETS	81



STATION	ELEVATION	DESCRIPTION
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1694+90	837.79	PROPOSED GRADE
1695+00	837.80	EXISTING GROUND
1695+00	837.80	PROPOSED GRADE



- NOTES**
1. ALL DIMENSIONS ARE RELATIVE TO THE CLAYP, UNLESS OTHERWISE NOTED.
 2. DISTANCE FROM POINT OF ALIGNMENT INFORMATION.

LEGEND

	PLANTING
	EXISTING R.O.W.
	PROPOSED R.O.W.
	DIRECTION OF TRAVEL
	CURVE NAME
	CONCRETE PAVEMENT
	ASPHALT PAVEMENT

SHEET TOTALS

EST.	FINAL	UNIT	DESCRIPTION

INTEGRAL SERVICE

NO SERVICE IS REQUIRED FOR THE PURPOSE OF THIS CONTRACT UNDER THE AUTHORITY OF THE STATE OF TEXAS, CHAPTER 202, SUBCHAPTER C, SECTION 202.001, UNTIL THE CONTRACT IS COMPLETED.

Texas Department of Transportation
 TEXAS
 TRANSPORTATION GROUP
 CONSULTING ENGINEERS

SH 121

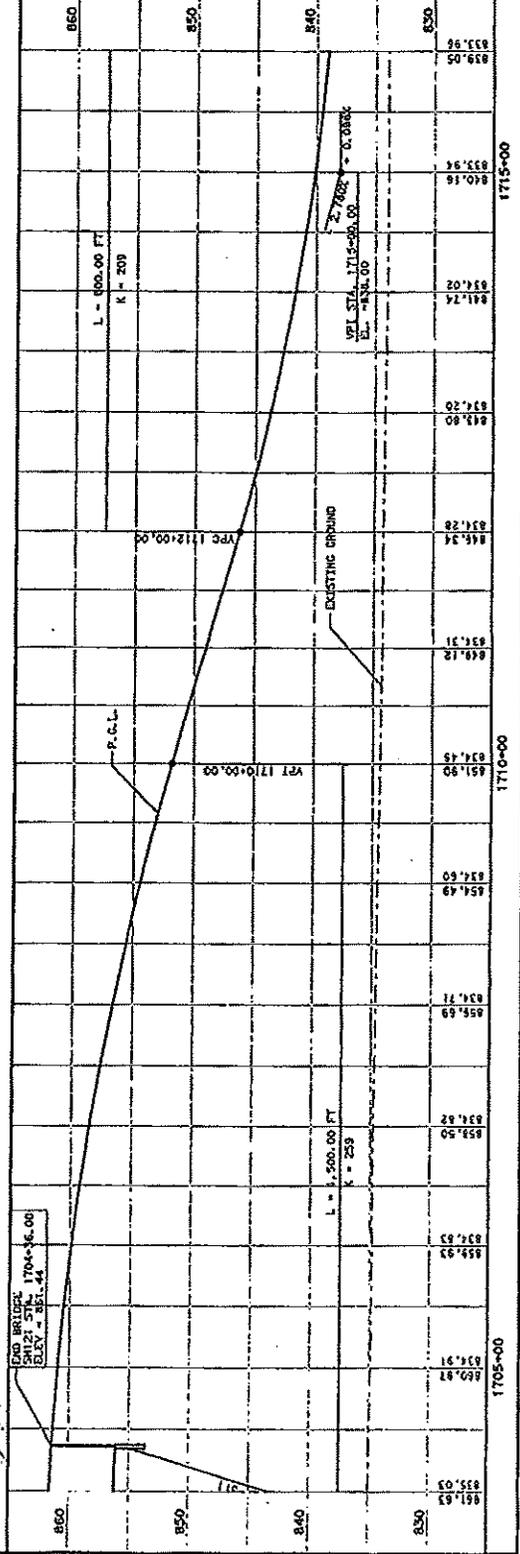
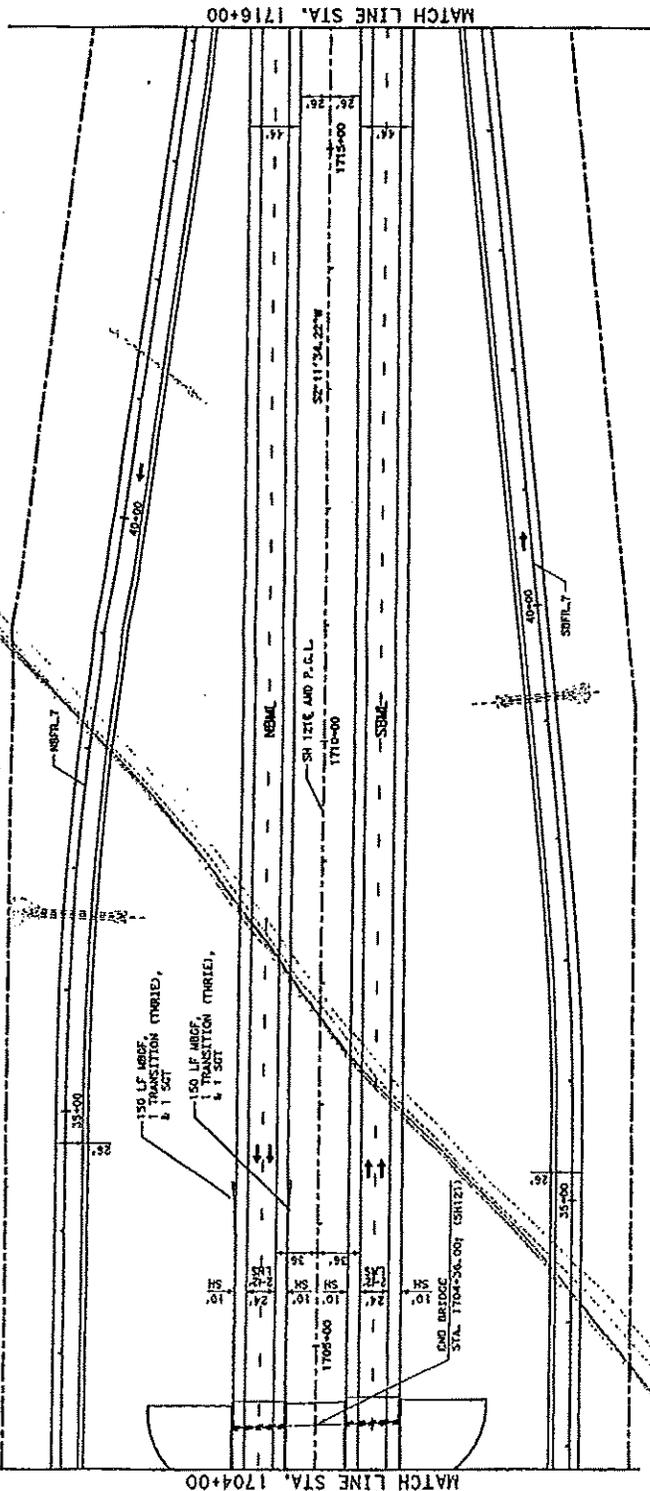
PLAN AND PROFILE

STA. 1704+00 TO STA. 1716+00

SCALE: PLAN 1"=100' HORIZ.
 PROFILE 1"=10' VERT.

SHEET 02 OF 01

DATE	BY	CHKD.	APP'D.





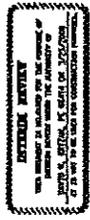
- NOTES
1. ALL DIMENSIONS ARE RELATIVE TO THE L.O.P. UNLESS OTHERWISE NOTED.
 2. REFER TO HORIZONTAL ALIGNMENT DATA SHEETS FOR ADDITIONAL INFORMATION.

LEGEND

---	PLANIMETRICS
---	EXISTING R.O.W.
---	PROPOSED R.O.W.
→	DIRECTION OF TRAVEL
(C/S)	CURVE NAME
▭	CONCRETE PAVEMENT
▭	ASPHALT PAVEMENT

SHEET TOTALS

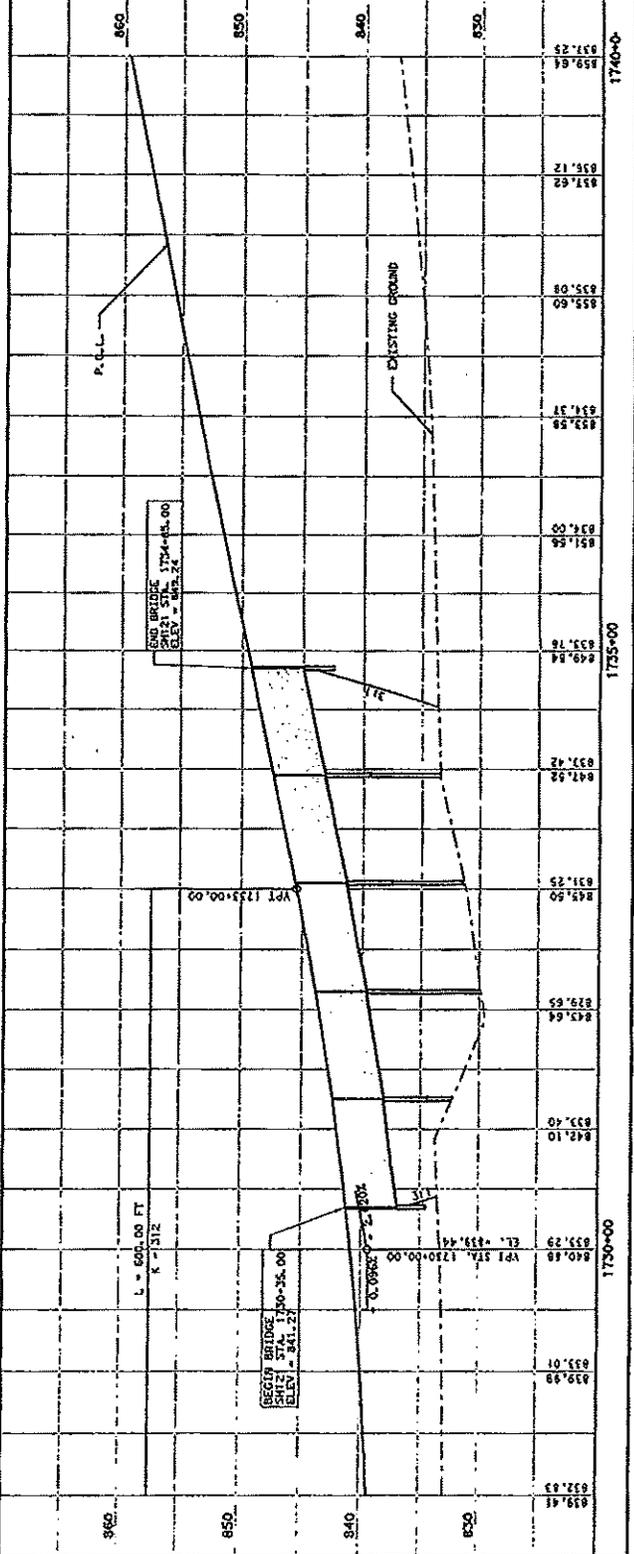
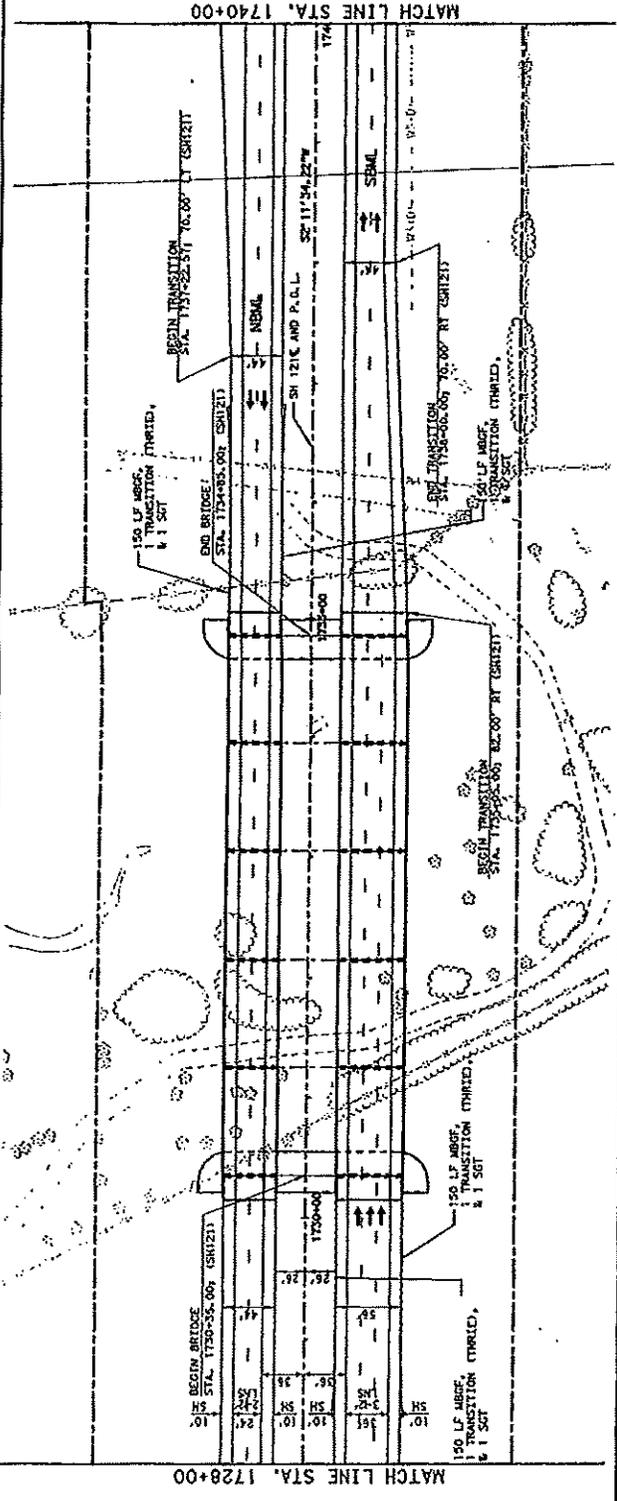
EST.	FINAL UNIT	DESCRIPTION

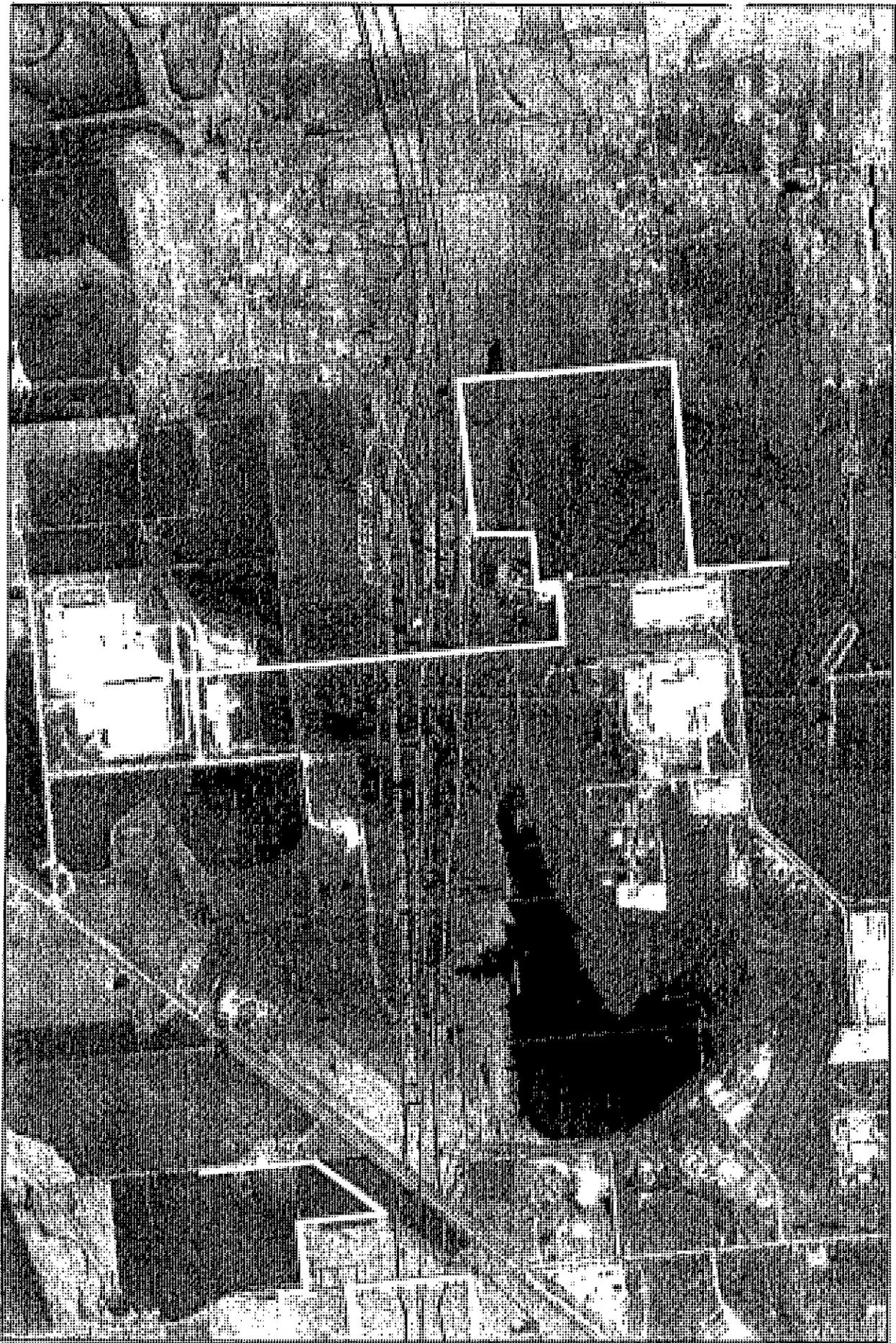


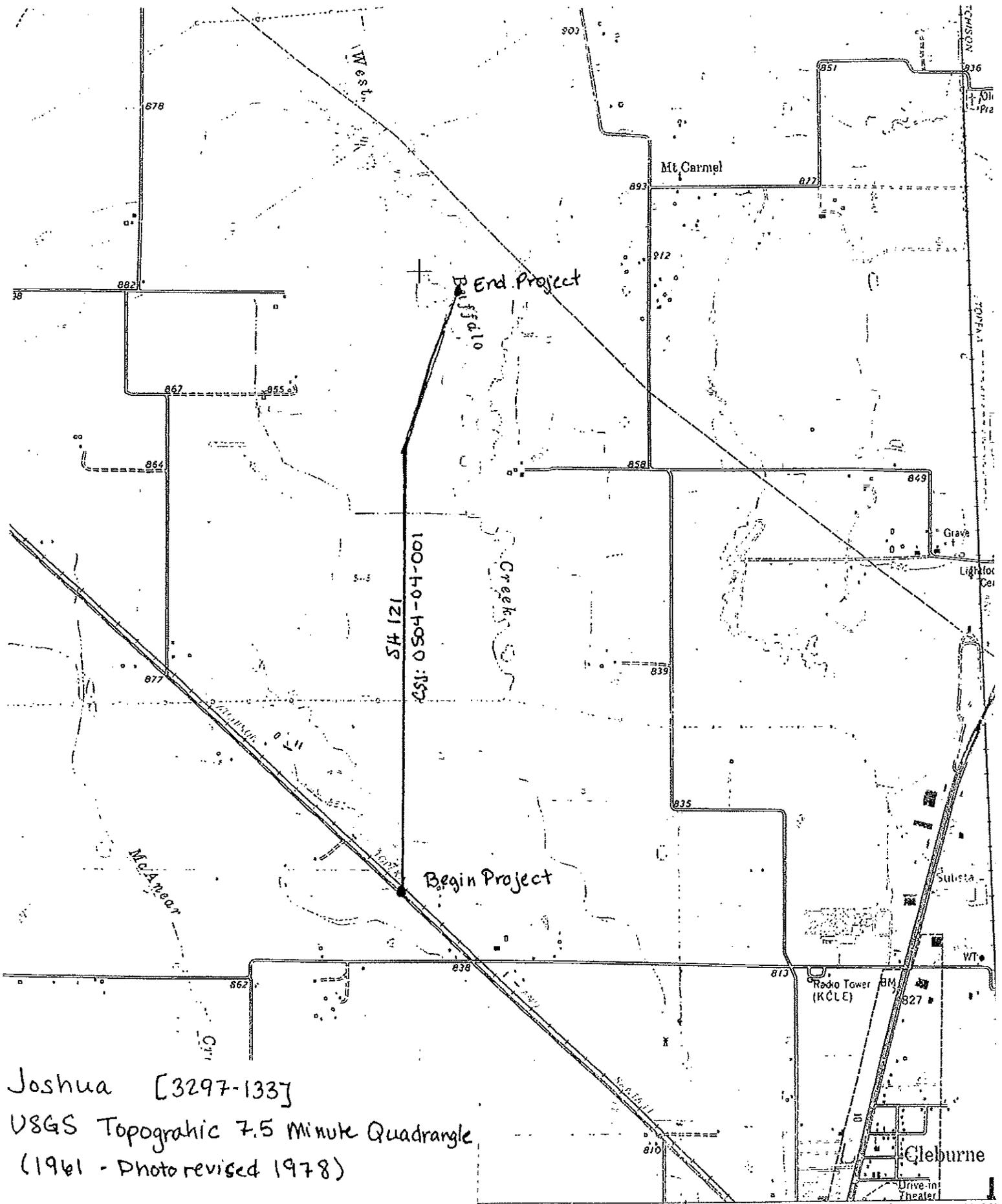
Texas Department of Transportation
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 ROBERT J. RODRIGUEZ GROUP
 CONSULTING ENGINEERS

SH 121
 PLAN AND PROFILE
 STA. 1728+00 TO STA. 1740+00
 SCALE: PLAN 1"=100' HORIZ.
 PROFILE 1"=10' VERT.
 SHEET 24 OF 61

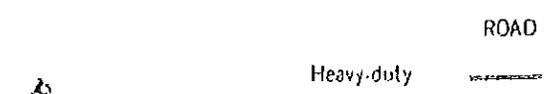
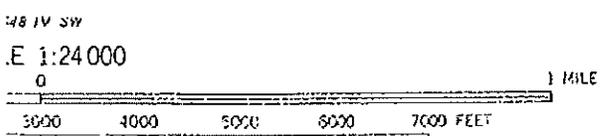
REVISED	DATE	BY	REASON	APPROVED	DATE







Joshua [3297-133]
 USGS Topographic 7.5 Minute Quadrangle
 (1961 - Photo revised 1978)



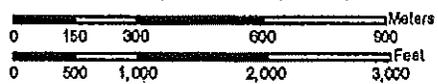
Soil Map
(CSJ: 0504-04-001 SH 121)



97° 25' 49"



Map Scale: 1:17,000 if printed on A size (8.5" x 11") sheet.



97° 24' 7"



MAP LEGEND

- Area of Interest (AOI)
 - Area of Interest (AOI)
- Soils
 - Soil Map Units
- Special Point Features
 - Blowout
 - Borrow Pit
 - Clay Spot
 - Closed Depression
 - Gravel Pit
 - Gravelly Spot
 - Landfill
 - Lava Flow
 - Marsh or swamp
 - Mine or Quarry
 - Miscellaneous Water
 - Perennial Water
 - Rock Outcrop
 - Saline Spot
 - Sandy Spot
 - Severely Eroded Spot
 - Sinkhole
 - Slide or Slip
 - Sodic Spot
 - Spoil Area
 - Stony Spot
- Special Line Features
 - Gully
 - Short Steep Slope
 - Other
- Political Features
 - Cities
- Water Features
 - Oceans
 - Streams and Canals
- Transportation
 - Rails
 - Interstate Highways
 - US Routes
 - Major Roads
 - Local Roads
- Very Stony Spot
- Wet Spot
- Other

MAP INFORMATION

Map Scale: 1:17,000 if printed on A size (8.5" x 11") sheet.
 The soil surveys that comprise your AOI were mapped at 1:20,000.
 Please rely on the bar scale on each map sheet for accurate map measurements.

Source of Map: Natural Resources Conservation Service
 Web Soil Survey URL: <http://websoilsurvey.nrcs.usda.gov>
 Coordinate System: UTM Zone 14N NAD83

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Johnson County, Texas
 Survey Area Data: Version 6, Apr 10, 2008
 Date(s) aerial images were photographed: 1995

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

Map Unit Legend

Johnson County, Texas (TX261)			
Map Unit Symbol	Map Unit Name	Acres In AOI	Percent of AOI
CuB	Culp clay loam, 0 to 3 percent slopes	23.3	17.6%
Fr	Frio silty clay, occasionally flooded	12.2	9.2%
LeB	Lewisville silty clay, 1 to 3 percent slopes	8.6	6.5%
PnB	Ponder clay loam, 1 to 3 percent slopes	21.5	16.2%
SaB	Sanger clay, 1 to 3 percent slopes	0.4	0.3%
SIA	Siddell clay, 0 to 1 percent slopes	48.8	36.8%
SIB	Siddell clay, 1 to 3 percent slopes	4.7	3.6%
WsA	Wilson silty clay loam, 0 to 1 percent slopes	12.9	9.7%
Totals for Area of Interest		132.4	100.0%

CSJ: 0504-04-001, SH 121, Texas Archeological Sites Atlas Map

— represents current proposed ROW alignment

Surveyed USDA-Rural Development 01/07
distance 230 meters from APE

0504-04-001
END
PROJECT

0504-04-001
Begin
Project

Josh

x

Legend

- ▲ Archeological Site Centroids
- ⎯ Archeological Site Boundaries
- Archeological Site Areas
- ⋯ Neighborhood Survey
- Historical Marker
- ★ National Register Property
- ▨ National Register District
- ▩ National Register District (address restricted)
- ⎯ Archeological Project [near]
- ▨ Archeological Project [area]
- ▩ Cemetery
- ▲ Shipwreck
- ⋯ USGS Quad Grid
- ⋯ Counties
- ⋯ USGS Topo Data

Texas Archeological Sites Atlas Copyright © 1995-2000 Texas Historical Geographic Information System 8x5.11

Surveyed FHWA 01/94
CSJ 2118-01-008

Surveyed Soil Conservation Service
12/81 distance from directly
adjacent to 320 meters from APE

Surveyed 02/96

Agency not listed

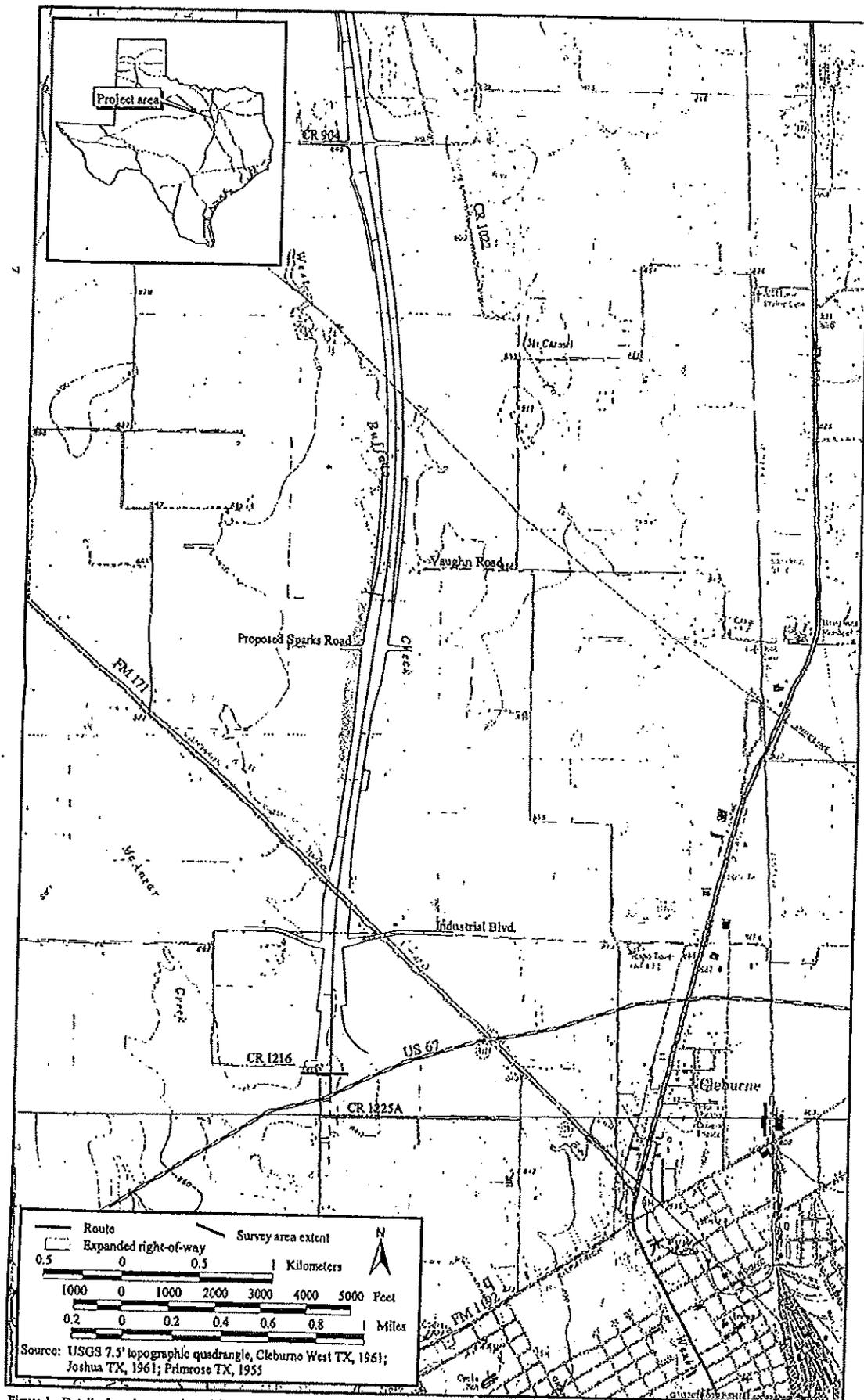
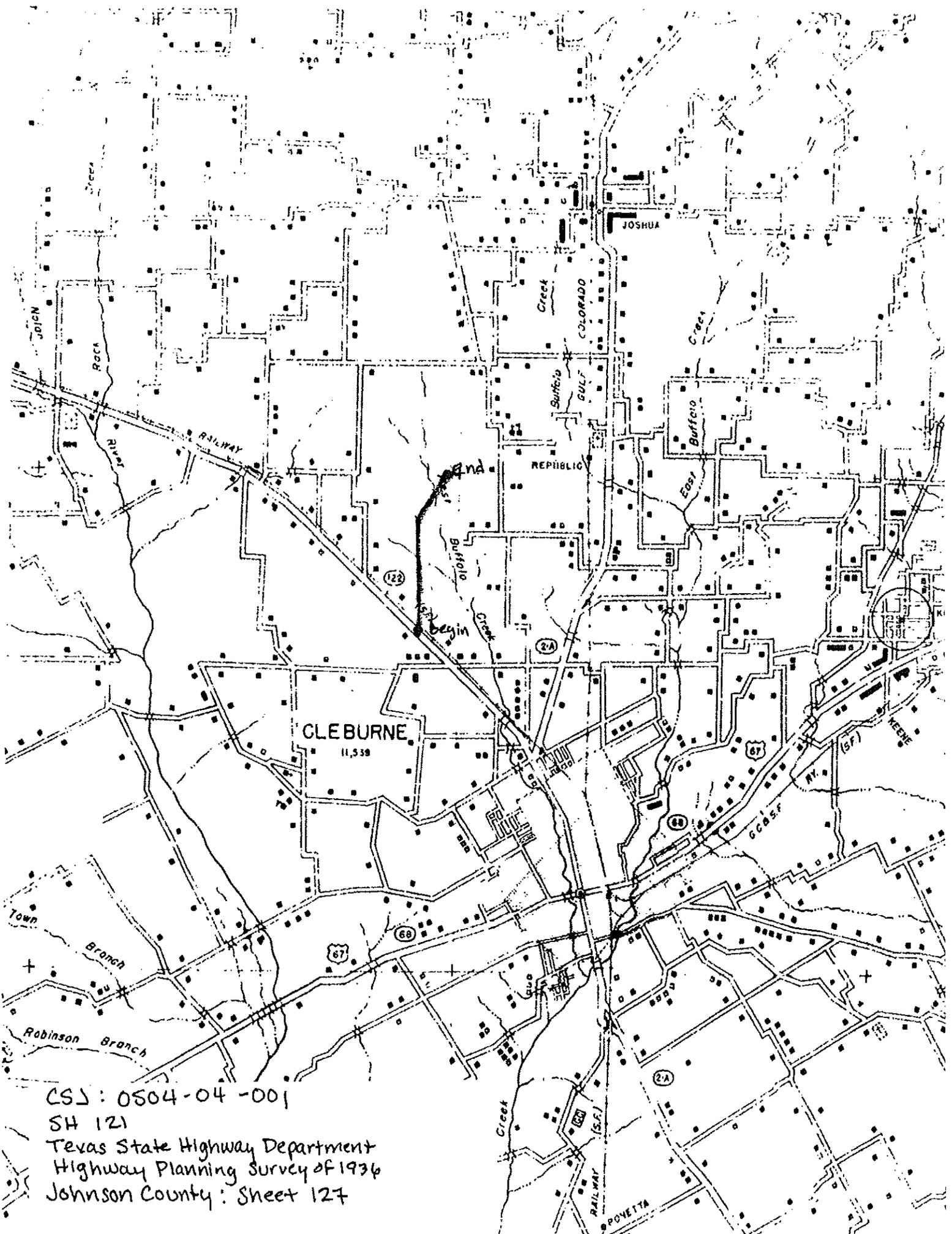


Figure 1. Detail of southern portion of the project area (CSJ 2118-02-008).



CSJ : 0504-04 -001

SH 121

Texas State Highway Department
Highway Planning Survey of 1936

Johnson County : Sheet 127



TONKAWA TRIBE OF OKLAHOMA
**NATIVE AMERICAN GRAVES PROTECTION
AND REPATRIATION ACT**

• 1 RUSH BUFFALO ROAD, TONKAWA, OKLAHOMA 74653 •
• PHONE (580) 628-2561 • FAX: (580) 628-9903 •
WEB SITE: www.tonkawatribe.com

Texas Department of Transportation
Dewitt C. Greer State Highway Bldg.
125 E. 11th Street
Austin, Texas 78701-2483

Date: December 9, 2008

Regarding the proposed construction projects listed we submit the following:

- Project No. CSJ: 0913-22-033 (YKM)
- Project No. CSJ: 0504-04-001 (previously 2118-01-008 & 2118-02-008) (FTW)
- Project No. CSJ: 0914-04-200 (AUS)
- Project No. CSJ: 0912-00-262 (HOU)

The Tonkawa Tribe has no specifically designated historical or cultural sites identified in the above listed project area. However if any human remains, funerary objects, or other evidence of historical or cultural significance is inadvertently discovered then the Tonkawa Tribe would certainly be interested in proper disposition thereof.

We appreciate notification by your office of the many projects on-going, and as always the Tonkawa Tribe is willing to work with your representatives in any manner to uphold the provisions of NAGPRA to the extent of our capability.

Respectfully,

NAGPRA Representative

Tonkawa Tribe Business Committee

TXDOT-ENV

DEC 10 2008

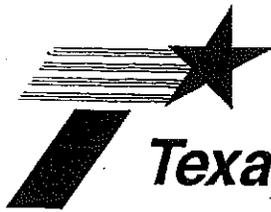
CRM

Concurrence:

Handwritten notes:
12/15/08
C-12/16/08
LHJ
Scott P
CJC

RECEIVED
DEC 10 2008
TXDOT

DIST 02 FT. WORTH
TXDOT MAILROOM
DEC 10 2008



Texas Department of Transportation

P.O. BOX 6868 • FORT WORTH, TEXAS 76115-0868 • (817) 370-6500

July 9, 2001

Mr. Stan Ellison
Natural Resources Conservation Service
103 B Poindexter
Cleburne, Texas 76031

Re: Proposed SH 121 in Johnson County
From: FM 1187
To: US 67
CSJ: 2118-02-008

Dear Mr. Ellison:

The Texas Department of Transportation (TxDOT) is currently preparing an Environmental Assessment for the proposed SH 121 project in Johnson County. At this time we are requesting a farmland conversion impact rating for this proposed project from you.

Enclosed are three (3) copies of form AD-1006, a location map, a composite United States Geological Survey roll (Joshua, Primrose and Cleburne West Texas Quadrangles) indicating the bounds of the proposed right-of-way (in dashed orange) and a copy of the Johnson County Soil Survey Maps with the alignment annotated in blue.

Please send your response to me at the address above. If you have any questions or need additional information, please call me at (817) 370-6755. Thank you for your assistance with this matter.

Sincerely,

Robert Hall
Environmental Coordinator

Enclosures

Copy w/o: Greg Parrent, P.E.
3141 Hood Street, Ste. 700
Dallas, TX 75219

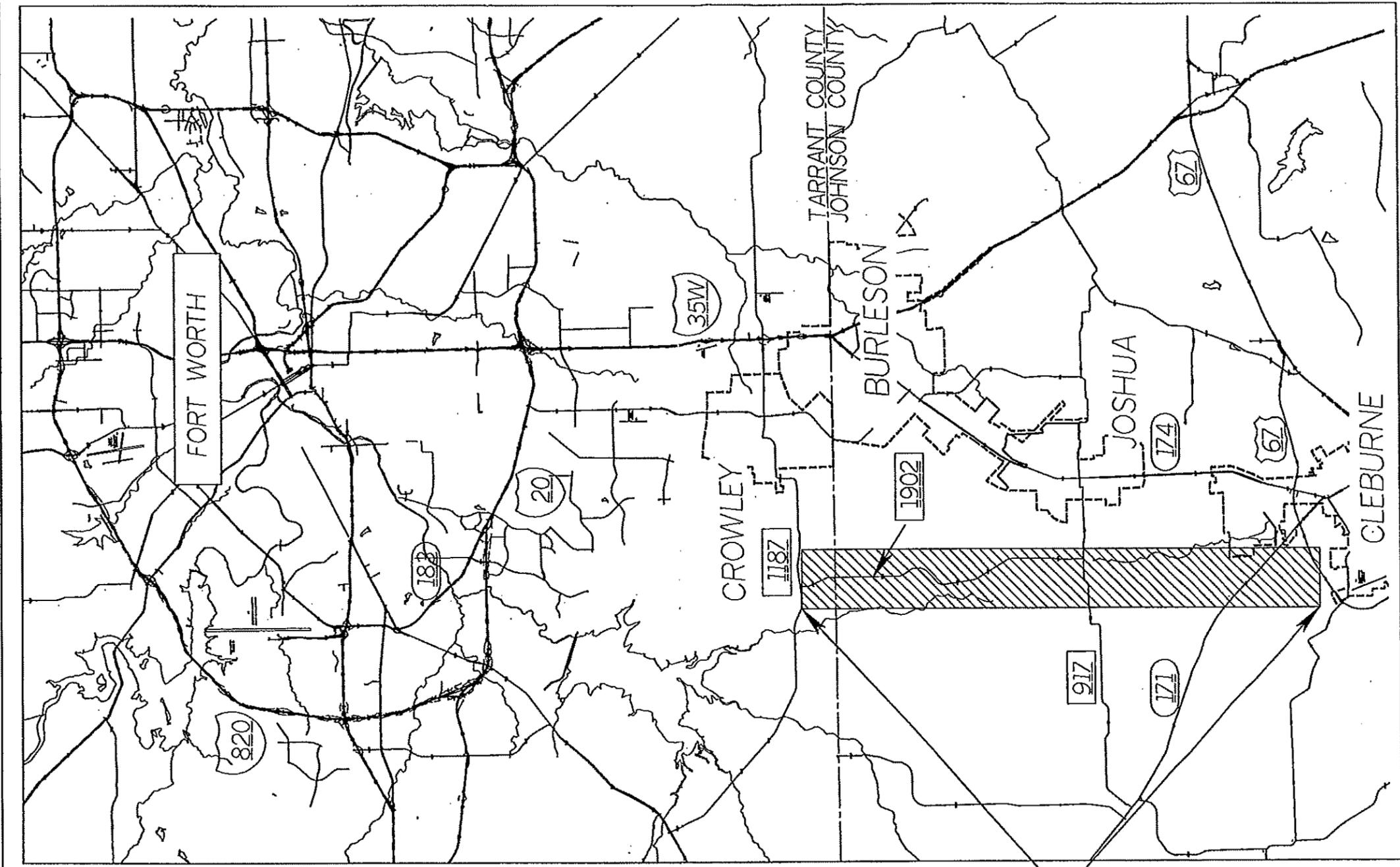
U.S. Department of Agriculture

FARMLAND CONVERSION IMPACT RATING

PART I (To be completed by Federal Agency)		Date Of Land Evaluation Request 6/25/01			
Name Of Project SH 121 Southern Extension		Federal Agency Involved Federal Highway Administration			
Proposed Land Use State Highway		County And State Johnson County, Texas			
PART II (To be completed by NRCS)		Date Request Received By NRCS			
Does the site contain prime, unique, statewide or local important farmland? <i>(If no, the FPPA does not apply - do not complete additional parts of this form).</i>		Yes <input type="checkbox"/>	No <input type="checkbox"/>	Acres Irrigated	Average Farm Size
Major Crop(s)	Farmable Land In Govt. Jurisdiction Acres: %	Amount Of Farmland As Defined In FPPA Acres: %		Date Land Evaluation Returned By NRCS	
Name Of Land Evaluation System Used	Name Of Local Site Assessment System				
PART III (To be completed by Federal Agency)		Alternative Site Rating			
		Site A	Site B	Site C	Site D
A. Total Acres To Be Converted Directly		407.3			
B. Total Acres To Be Converted Indirectly		24.1			
C. Total Acres In Site		431.4	0.0	0.0	0.0
PART IV (To be completed by NRCS) Land Evaluation Information					
A. Total Acres Prime And Unique Farmland					
B. Total Acres Statewide And Local Important Farmland					
C. Percentage Of Farmland In County Or Local Govt. Unit To Be Converted					
D. Percentage Of Farmland In Govt. Jurisdiction With Same Or Higher Relative Value					
PART V (To be completed by NRCS) Land Evaluation Criterion Relative Value Of Farmland To Be Converted (Scale of 0 to 100 Points)		0	0	0	0
PART VI (To be completed by Federal Agency) Site Assessment Criteria (These criteria are explained in 7 CFR 658.5(b))		Maximum Points			
1. Area In Nonurban Use		15	13		
2. Perimeter In Nonurban Use		10	8		
3. Percent Of Site Being Farmed		20	7		
4. Protection Provided By State And Local Government		20	0		
5. Distance From Urban Bullup Area		0	0		
6. Distance To Urban Support Services		0	0		
7. Size Of Present Farm Unit Compared To Average		10	3		
8. Creation Of Nonfarmable Farmland		25	22		
9. Availability Of Farm Support Services		5	5		
10. On-Farm Investments		20	5		
11. Effects Of Conversion On Farm Support Services		25	0		
12. Compatibility With Existing Agricultural Use		10	5		
TOTAL SITE ASSESSMENT POINTS		160	88	0	0
PART VII (To be completed by Federal Agency)					
Relative Value Of Farmland (From Part V)		100	0	0	0
Total Site Assessment (From Part VI above or a local site assessment)		160	88	0	0
TOTAL POINTS (Total of above 2 lines)		260	88	0	0
Site Selected:	Date Of Selection	Was A Local Site Assessment Used? Yes <input type="checkbox"/> No <input type="checkbox"/>			
Reason For Selection:					

SH 121

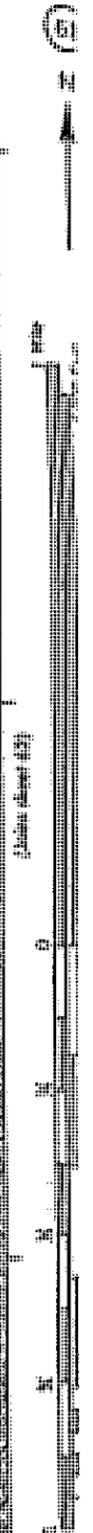
TARRANT AND JOHNSON COUNTIES



STUDY LIMITS

FROM
FM 1187
TO
US 67

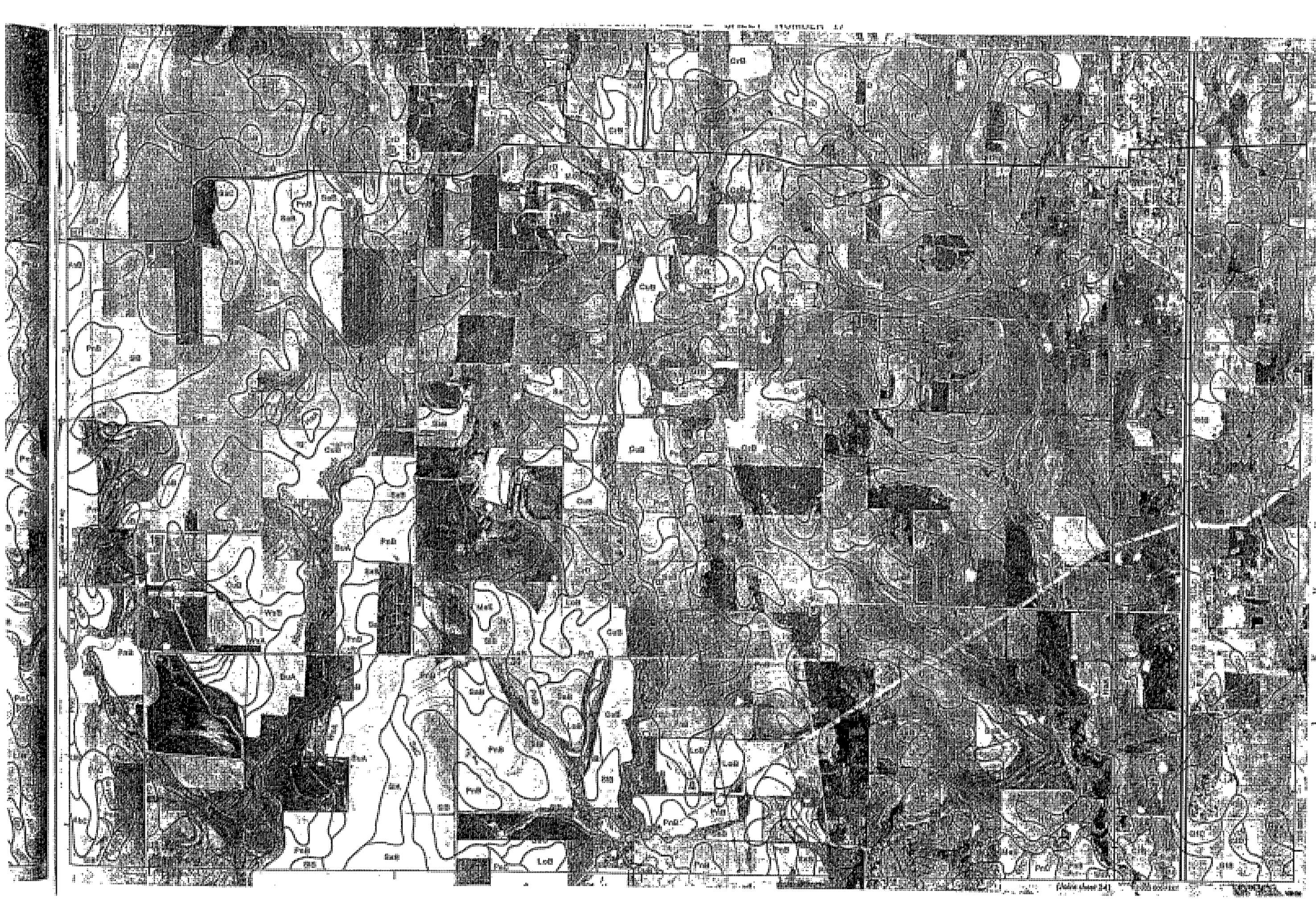
Water about 200'



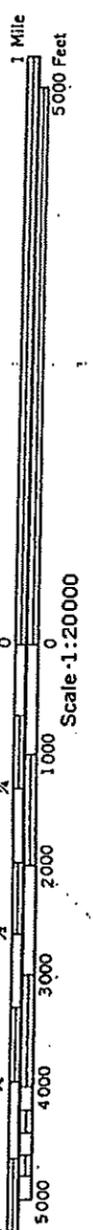


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17





United States
Department of
Agriculture

Natural
Resources
Conservation
Service

101 South Main
Temple, Texas
76501-7602

L. J. ORF, VICE PRES.
TXDOT MAILROOM

JUN 10 2002

Subject: LNU-Farmland Protection-
SH 121 Highway Proposed
Johnson County, Texas

June 7, 2002

Texas Department of Transportation
P.O. Box 6868
Fort Worth, Texas
76115-0686

Attention: Robert Hall, Environmental Coordinator

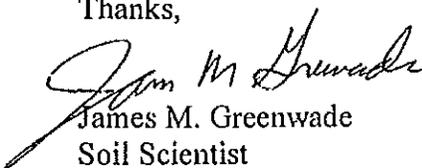
We have reviewed the information provided concerning proposed S. H. 121 in Johnson County, Texas. This is part of an Environmental Evaluation for the above-referenced highway being prepared for the TxDOT and FHWA. We have evaluated the soils for this project as required by the Farmland Protection Policy Act (FPPA).

The proposed project does contain Prime and Statewide Important Farmland soils as defined by the FPPA. Several map units identified in the Soil Survey of Johnson County are classified as Prime Farmland and Statewide Important Farmland. Approximately 431.4 acres of land will be acquired of which about 298.4 acres is classified as Important Farmland by the FPPA. These soils had a composite score of 74 and the Total Points on Part VII of the AD-1006 is 142. This site will require no additional consideration since the rating score is less than 160. The FPPA states, "Sites receiving a total score of less than 160 need not be given further consideration for protection and no additional sites need to be evaluated", 7CFR Part 658.4 (c) 2.

Attached is the completed AD-1006 (Farmland Conversion Impact Rating) form for this project indicating the exemption status of this proposed project.

Thanks for the quality resource materials you submitted to evaluate this project. If you have any questions please call James Greenwade at (254)-742-9960 or Sam Brown at (254)-742-9854, Fax (254)-742-9859.

Thanks,


James M. Greenwade
Soil Scientist
Soil Survey Section
USDA-NRCS, Temple, Texas

U.S. Department of Agriculture
FARMLAND CONVERSION IMPACT RATING

PART I (To be completed by Federal Agency)		Date Of Land Evaluation Request 6-25-01				
Name of Project SH 121		Federal Agency Involved Federal Highway Administration				
Proposed Land Use Transportation		County and State Johnson County, Texas				
PART II (To be completed by NRCS)		Date Request Received By NRCS 4-24-02		Person Completing Form: James Greenwade		
Does the site contain Prime, Unique, Statewide or Local Important Farmland? <i>(If no, the FPPA does not apply - do not complete additional parts of this form)</i>		YES <input checked="" type="checkbox"/>	NO <input type="checkbox"/>	Acres Irrigated 882	Average Farm Size 161	
Major Crop(s) Improved Bermudagrass	Farmable Land In Govt. Jurisdiction Acres: 285,324 % 61	Amount of Farmland As Defined In FPPA Acres: 283,624 %61				
Name of Land Evaluation System Used LESA	Name of State or Local Site Assessment System NONE	Date Land Evaluation Returned by NRCS 6-7-2002				
PART III (To be completed by Federal Agency)		Alternative Site Rating				
A. Total Acres To Be Converted Directly		Site A 407.3	Site B	Site C	Site D	
B. Total Acres To Be Converted Indirectly		24.1				
C. Total Acres In Site		431.4				
PART IV (To be completed by NRCS) Land Evaluation Information						
A. Total Acres Prime And Unique Farmland		263.2				
B. Total Acres Statewide Important or Local Important Farmland		35.2				
C. Percentage Of Farmland In County Or Local Govt. Unit To Be Converted		0.001				
D. Percentage Of Farmland In Govt. Jurisdiction With Same Or Higher Relative Value		77				
PART V (To be completed by NRCS) Land Evaluation Criterion Relative Value of Farmland To Be Converted (Scale of 0 to 100 Points)		74				
PART VI (To be completed by Federal Agency) Site Assessment Criteria <i>(Criteria are explained in 7 CFR 658.5 b. For Corridor project use form NRCS-CPA-106)</i>		Maximum Points	Site A	Site B	Site C	Site D
1. Area In Non-urban Use		(15)	13			
2. Perimeter In Non-urban Use		(10)	8			
3. Percent Of Site Being Farmed		(20)	7			
4. Protection Provided By State and Local Government		(20)	0			
5. Distance From Urban Built-up Area		(15)	0			
6. Distance To Urban Support Services		(15)	0			
7. Size Of Present Farm Unit Compared To Average		(10)	3			
8. Creation Of Non-farmable Farmland		(10)	22			
9. Availability Of Farm Support Services		(5)	5			
10. On-Farm Investments		(20)	5			
11. Effects Of Conversion On Farm Support Services		(10)	0			
12. Compatibility With Existing Agricultural Use		(10)	5			
TOTAL SITE ASSESSMENT POINTS		160	68			
PART VII (To be completed by Federal Agency)						
Relative Value Of Farmland (From Part V)		100	74			
Total Site Assessment (From Part VI above or local site assessment)		160	68			
TOTAL POINTS (Total of above 2 lines)		260	142			
Site Selected: A	Date Of Selection	Was A Local Site Assessment Used? YES <input type="checkbox"/> NO <input checked="" type="checkbox"/>				
Reason For Selection Points less than 160						
Name of Federal agency representative completing this form:					Date:	

(See instructions on reverse side)

07/12/2002 09:19 FAX 210 499 5167

LAN SAN ANTONIO
TIDOT TP D

+ LAN DALLAS

004
004
002

07/11/02 16:21 817 370 8759
07/11/2002 11:08 FAX



Texas Department of Transportation

DEWITT G. GREER STATE HIGHWAY BLDG. • 125 E. 11TH STREET • AUSTIN, TEXAS 78701-2483 • (512) 462-8535

May 24, 2002

received
May 24, 2002

NH ()
Environmental Assessment Coordination
Johnson and Tarrant Counties
CSJ 2118-01-008; 2118-02-008

PROJECT
JUN 05 2002
MANAGEMENT

SH 121 South: From FM 1187 to US 67

Dr. Ray C. Telfair
Texas Parks and Wildlife Department
Wildlife Division
Wildlife Habitat Assessment Program
11942 FM 848
Tyler, Texas 75707-9657

Dear Dr. Telfair:

Consistent with the Memorandum of Understanding signed by our two agencies, attached is a copy of the environmental assessment covering the subject project for your review and comment. Any comments you may have on this document will assist the Department in ensuring that the Department's projects are sensitive to the natural resources of the state.

Please submit any comments you may have within 45 days from the date of this letter. If you do not have any comments on the document, please sign and date the bottom of this letter and return a copy to the Environmental Affairs Division. If no response is received after the 45 days have expired, we will proceed with project development. If you have any questions regarding this project, please contact Mr. William Hood at (512) 416-2623.

Sincerely,

Michelle Skinner
Project Management
Environmental Affairs Division

Attachment

NO COMMENT:

Ray C. Telfair

Wildlife Habitat Assessment Program

DATE:

May 31, 2002

07/12/2002 09:20 FAX 210 499 5157

LAN SAN ANTONIO

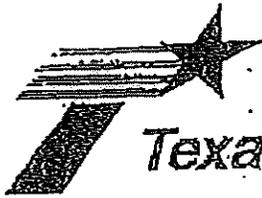
LAN DALLAS

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07/11/02 16:22 817 370 8759

TXDOT TP D

005



Texas Department of Transportation

DEWITT C. GREER STATE HIGHWAY BLDG. • 125 E. 11TH STREET • AUSTIN, TEXAS 78701-2483 • (512) 463-8585

May 24, 2002

DIST 02 FT. WORTH
TXDOT MAILROOM

MAY 30 2002

NH ()

Environmental Assessment Coordination
Johnson and Tarrant Counties
CSJ 2118-01-008; 2118-02-008

SH 121 South: From FM 1187 to US 67

Ms. Celeste Brancel-Brown
Texas Parks and Wildlife Department
Endangered Resources Branch
3000 S. I.H. 35, Suite 100
Austin, Texas 78704

Dear Ms. Brown:

Consistent with the Memorandum of Understanding signed by our two agencies, attached is a copy of the environmental assessment covering the subject project for your review and comment. Any comments you may have on this document will assist the Department in ensuring that the Department's projects are sensitive to the natural resources of the state.

Please submit any comments you may have within 45 days from the date of this letter. If you do not have any comments on the document, please sign and date the bottom of this letter and return a copy to the Environmental Affairs Division. If no response is received after the 45 days have expired, we will proceed with project development. If you have any questions regarding this project please contact Mr. William Hood at 512-416-2623.

Sincerely,

Michelle Skinner
Project Management
Environmental Affairs Division

Attachment

NO COMMENT:

Texas Biological and Conservation Data System

DATE: _____

MMS:M

bcc: Fort Worth District

ERG

Reference: ENV 850



Texas Department of Transportation

P.O. BOX 6868 • FORT WORTH, TEXAS 76115-0868 • (817) 370-6500

P.I
2118-02-008

May 31, 2002

Mr. James Greenwade
Natural Resources Conservation Service
W.R. Poage Federal Building
101 South Main Street
Temple, Texas 76501-7682

Re: Proposed SH 121 in Johnson County
From: FM 1187
To: US 67
CSJ: 2118-02-008

Dear Mr. Greenwade:

The Texas Department of Transportation (TxDOT) is currently preparing an Environmental Assessment for the proposed SH 121 project in Johnson County. At this time we are requesting a farmland conversion impact rating for this proposed project from you.

Enclosed is a copy of form AD-1006, a location map and a copy of the Johnson County Soil Survey Maps with the alignment annotated in blue. The project can be found on United States Geological Survey Quadrangles for Joshua, Primrose and Cleburne West Texas.

Please send your response to me at the address above. We previously sent this information to the Cleburne Field Office on July 9, 2001, therefore, and expedited response will be appreciated. If you have any questions or need additional information, please call me at (817) 370-6755. Thank you for your assistance with this matter.

Sincerely,

Robert Hall
Environmental Coordinator

Enclosures