

PUBLIC COMMENTS

ORAL PUBLIC COMMENTS

1 report is available upon request. You may send your
2 request to the address on the agenda and comment form.
3 The report will be available in late spring/early summer
4 2010 and may also be viewed on TxDOT's web page.

5 Following Federal Highway
6 Administration's final review, a Finding of No
7 Significant Impact will be requested.

8 Thank you for your attention.

9 I'll now turn it back over to Mr. Alford.

10 MR. MIKE ALFORD: Thank you, Casey.

11 Now we will begin the formal public
12 comment session. Only registered speakers will be called
13 upon. Please raise your hand if you would like to speak
14 for the record tonight and have not completed a
15 registration card. Our staff has cards for you to fill
16 out.

17 We will collect any additional speaker
18 registration cards after we hear from our elected
19 officials who are present tonight and wish to make a
20 public comment.

21 Okay. First, Council Member Ed Gonzalez.

22 COUNCIL MEMBER ED GONZALEZ: Good
23 evening. My name is Council Member Ed Gonzalez; and I
24 represent District H, which covers the location where the
25 detention basins would be placed.

1 And I just want to make sure that TxDOT
2 communicates with the community and updates them and
3 provides an avenue to where they can voice their concerns
4 beyond just this meeting, perhaps, if we need additional,
5 you know, follow-up meetings and be willing to host them.

6 And also, if we could make sure that
7 we're trying to include within the detention basins
8 different features that, perhaps, the community could use
9 as well, that, you know, maybe some trails or at least
10 create the basins to where these features could be added
11 at some point at least to where the City could perhaps
12 participate at some point. So I just want to comment,
13 listen to the comments and hopefully be able to follow up
14 after that. Thank you.

15 MR. MIKE ALFORD: Thank you.

16 And at the beginning I failed to
17 recognize Representative Bohac also present.

18 The next speaker will be Representative
19 Ferrar.

20 REPRESENTATIVE JESSICA FERRAR: Thank you
21 so much for allowing me this opportunity.

22 Really I want to enter into the record
23 what resulted from our meeting or what was discussed at
24 our meeting with you last Friday -- well, with Jim Koch
25 and Pat Henry and I appreciate the opportunity that they

1 gave us to present an idea to them; and it was a meeting
2 with Kevin Shanley from the White Oak Bayou Association.

3 So I don't know if he's going to be
4 presenting it or not, but I wanted to make sure it gets
5 into the record. And the request that we made was that
6 in the design work of these projects that the --
7 especially the earthwork that's done, it be left in a
8 condition to where a future project, whether it's a
9 public or a private entity, could come back and make a --
10 different ideas have been discussed: Dog parks, trails,
11 connections to bike trails that are already planned and
12 so on.

13 But basically the idea is that the costs
14 to the state agency in terms of earthwork is minimal to
15 the needs of a future park; but if a group comes in
16 subsequently to build that park, the cost for earthwork
17 would be substantial. So we're just asking if you would,
18 so to speak, leave the beds ready; and we'll come back
19 and plant the flowers later.

20 So that is my request, is that the agency
21 work with the groups that are interested and it's --
22 Council Member Gonzalez has asked the Houston Parks
23 Department be involved. It's questionable at this moment
24 whether there's funds to do so, but there are private
25 groups that are interested as well. But the interest is

1 there and that's what I would like to express is that if
2 you could help these groups in your -- and the work that
3 is being done in the near future to the work that these
4 groups can do later on in terms of making these spaces,
5 expanding the green space that we know that we're in such
6 need of in the Houston area.

7 So that's what I would like to
8 contribute, and I look forward to working with you more
9 on this. Thank you.

10 MR. MIKE ALFORD: Thank you.

11 Michael Halpin for Congresswoman Sheila
12 Jackson Lee.

13 MR. MICHAEL HALPIN: Thank you. Michael
14 Halpin with the office of Congresswoman Jackson Lee, who
15 asked me to bring a letter tonight that I'll leave with
16 you, and I'll just read a portion of it.

17 "I joined some very concerned neighbors
18 this past weekend on the banks of White Oak Bayou. I was
19 impressed by the cohesiveness of the neighbors and of the
20 massive greenbelt of which our bayous are part. However,
21 I was disappointed at the disregard of the greenbelt and
22 lack of development.

23 "Let us agree on one thing tonight. We
24 will all work together to make White Oak Bayou and the
25 detention ponds into park space and complimentary hike

1 and bike trails. Let's work together to connect our
2 bikeways and make them better.

3 "I strongly support recommendations by
4 White Oak Bayou Association and Greater Heights Super
5 Neighborhood Council encouraging TxDOT to include
6 provisions for park features to be added in the areas
7 identified as detention ponds for mitigation on the I-10
8 frontage road project.

9 "The cost to acquire the land alone will
10 provide multiple benefits to surrounding communities by
11 TxDOT working with the Flood Control District and the
12 City of Houston. We encourage a collaborative effort for
13 the inclusion of the park option. We want to encourage,
14 wherever possible, the pursuit of shared goals and
15 effective communication between the different
16 representative bodies. We look forward to working with
17 TxDOT to deliver this valuable benefit to the community.

18 "Thank you for your consideration.
19 Sincerely, Sheila Jackson Lee, member of Congress."

20 MR. MIKE ALFORD: Thank you.

21 Any other public official?

22 Our staff will now collect the speaker
23 registration cards. Anybody else have any more cards?

24 The purpose of this comment session is to
25 receive your comments on issues relevant to the project.

1 Both written and oral comments will be considered
2 equally.

3 If you would like to comment, but do not
4 wish to do a comment orally or if you would like to
5 comment in more detail, you may submit a written comment.
6 Written comments must be postmarked by March 4th, 2010.
7 Comment forms are available at the sign-in table and may
8 be turned in tonight, or e-mailed, or mailed to the
9 address on the slide.

10 The names of speakers will be called in
11 the order that they were received. When your name is
12 called, please come to the microphone. Use the
13 microphone in the front of the room and please face our
14 court reporter, so that she can make an accurate record
15 of your comments. Please state your name for the record
16 and whom you may be representing.

17 A timed display on this screen will
18 indicate the start of each speaker's three minutes. The
19 timer will count down your remaining time. When the
20 clock is at zero, your speaking time is over. Once the
21 three-minute period is over, we ask that you allow the
22 next speaker to present their comment.

23 The comment session will adjourn after
24 all who have registered to make an oral comment have been
25 given an opportunity to speak. We will not respond to

1 questions at this time. TxDOT representatives and
2 members of the consultant team will be available to
3 answer all questions following the comment session.

4 Okay. Representative Bohac would like to
5 speak, so --

6 REPRESENTATIVE DWAYNE BOHAC: Good
7 evening. My name is Representative Bohac, and I
8 represent Houston and West Houston in the Texas
9 legislature. I've also been a member of the White Oak
10 Bayou Association for more years than I can count. I
11 know they talked with you-all last week, and there's some
12 plans that they put on the table.

13 I just wanted to let you-all know that I
14 strongly support their plans. I would like for you-all
15 to take them very seriously and look at those plans and
16 adopting them on a go-forward basis.

17 So, thank you.

18 MR. MIKE ALFORD: Thank you.

19 Okay. Speaker Number 1, Jonathan
20 Smulian.

21 MR. JONATHAN SMULIAN: I would like to
22 pass.

23 MR. MIKE ALFORD: Anne Culotta.

24 (Ms. Culotta walks up the stairs to the
25 podium on stage instead of the microphone in front of the

1 stage.)

2 MS. CASEY HALL: Sorry. There's one
3 right here you can use.

4 MS. ANNE CULOTTA: Oh, I'm sorry. So I
5 talk this way?

6 MS. CASEY HALL: Sure.

7 MS. ANNE CULOTTA: Oh, okay.

8 Let me introduce myself. I'm the
9 president of the Houston Heights Association, the
10 neighborhood association; and I would simply like to read
11 the Houston Heights Association's position statement.

12 MR. MIKE ALFORD: Can you state your
13 name?

14 MS. ANNE CULOTTA: My name is Anne
15 Culotta.

16 MR. MIKE ALFORD: Thank you.

17 MS. ANNE CULOTTA: Because of this
18 mission to preserve and enhance the quality of life in
19 the Heights, the Houston Heights Association is opposed
20 in principle to road construction or other infrastructure
21 projects that could generate increased congestion or risk
22 of flooding in our historic neighborhood.

23 In the case of the proposed TxDOT project
24 to construct access ramps at Yale Street to and from
25 Interstate 10, based on information brought before us, we

1 consider that the project may, one, lead to a significant
2 increase in traffic volumes for through traffic between
3 the 610 Loop and Interstate 10 that does not have its
4 origin or destination within the Heights, resulting in an
5 increase in traffic congestion in our historic
6 neighborhood and, two, as currently planned, we consider
7 that the project may pose a risk of increased flooding in
8 our historic neighborhood and, three, the Houston Heights
9 Association specifically requests that TxDOT apply its
10 new policy of context sensitive design for this project
11 and coordinate with the City of Houston, the Harris
12 County Flood Control District and the community. And the
13 Houston Heights Association requests that all floodwater
14 detention and mitigation issues be addressed and
15 implemented concurrently with road construction.

16 The Houston Heights Association welcomes
17 the opportunity to work with TxDOT and our elected
18 officials to address these issues. However, without
19 further study and any necessary actions to alleviate
20 these community concerns, the Heights Association is
21 opposed to the current execution of the project.

22 Thank you.

23 MR. MIKE ALFORD: Tom Dornbusch.

24 MR. TOM DORNBUSCH: My name is Tom
25 Dornbusch. I live four blocks from Interstate 10, so I'm

1 aware of the flooding issues there. I am the
2 vice-president of Super Neighborhood 22, Washington
3 Avenue/Memorial Park Super Neighborhood, which I-10 moves
4 through.

5 And as much as I feel compelled to use my
6 time to address concerns over TxDOT's lack of community
7 involvement with regard to the IH 10 frontage lanes
8 project that necessitates these detention ponds; given
9 the seven-year lag between the only public hearing, which
10 was held outside the area of immediate impact; the
11 hundreds, possibly thousands, of new residents new to the
12 impact area since that hearing who have had no
13 opportunity to comment on that project; TxDOT's recent
14 severe reprimanding from its Sunset Review Commission for
15 lack of transparency and the disingenuousness of agreeing
16 to a formal meeting about the project, then announcing at
17 that meeting the letting of construction contracts
18 earlier in the same day, after having, a few weeks
19 earlier -- after having, a few weeks earlier, touted the
20 adoption of and commitment to context sensitive
21 solutions -- well, I don't need to address that.

22 Instead, I'll thank you for scheduling
23 this additional hearing for the detention facilities
24 project and I urge you to exercise that stated commitment
25 to context sensitive solutions by taking the steps

1 necessary to assure residents of the area, who are
2 concerned about these projects' impact on their property,
3 that this detention will be provided concurrently with
4 the roadway construction so that there will be no
5 opportunity for neighborhood flooding resulting from that
6 construction before detention is provided and that you
7 will act on the suggestions presented and assist with
8 making these facilities multipurpose community assets
9 rather than fenced off pits.

10 MR. MIKE ALFORD: Jim Mackey.

11 MR. JIM MACKEY: Good evening. I'm Jim
12 Mackey, vice-president of the White Oak Bayou
13 Association; and our organization goes by the acronym
14 WOBA. We join with the other community groups in the
15 Heights area on the following points: We believe that
16 the detention ponds should be designed and built in such
17 a way to support the future development of the site's
18 parks, complete with wet bottoms, native plants and
19 trails.

20 Additionally, we urge TxDOT to secure
21 funding and proceed quickly with the acquisition of the
22 detention pond sites. On this point we fully support
23 TxDOT's efforts and offer to advocate for funding.

24 It's somewhat of a disappointment this
25 evening that in the formal presentation we didn't hear

1 more about the proposed funding of the acquisition of the
2 detention sites.

3 Additionally, we strongly request, as did
4 some of the other speakers already, that all the
5 detention and mitigation measures be constructed
6 concurrently, if not before the feeder road construction,
7 that will impact White Oak Bayou and the drainage of the
8 below-grade section of I-10.

9 Our organization took the time to review
10 TxDOT's drainage study, the detention pond renditions and
11 the road plans; and we have several questions in that
12 regard.

13 We still would like to meet with TxDOT
14 and discuss the detention pond capacities. We feel there
15 is a little bit of discrepancy between our numbers and
16 TxDOT's, that we may be able to resolve that; but we
17 would like to meet on that matter.

18 While technically sound and the drainage
19 study used what are considered best practices, we're
20 somewhat troubled in the study in that TxDOT didn't run
21 any simulations that showed the bridges, the feeder road
22 bridges at different levels. We would have liked to have
23 seen a simulation run with the ponds -- or with the
24 bridges, excuse me, raised to see if any additional
25 mitigation may be required. We think that we would

1 encourage TxDOT to undertake that study.

2 And finally, some concern -- as time is
3 running out here -- we're very concerned about the
4 flooding and drainage situation of Yale at I-10 because
5 the bridges at that location are a problem.

6 Thank you.

7 MR. MIKE ALFORD: Becky Lindow.

8 MS. PEGGY LINDOW: No comment. There
9 will be a question period later, a public question
10 period?

11 MR. MIKE ALFORD: It will be done with
12 the staff after this.

13 MS. PEGGY LINDOW: That's a
14 disappointment.

15 MR. MIKE ALFORD: Ken Culotta.

16 MR. KEN CULOTTA: Thank you. I'm Ken
17 Culotta. I'm a resident of the Heights for over 20
18 years. I want to express my appreciation, first, to
19 TxDOT. You-all have fielded a lot of citizens' concerns,
20 sometimes very passionately expressed and often, I think,
21 outside of your authority to really change things and put
22 in motion. For that reason my comments are really
23 directed to the elected officials present tonight as much
24 as they are to TxDOT.

25 We have been told that this is a meeting

1 about detention ponds and flood control and indeed to the
2 extent that we are only talking about flood control, and
3 we're talking about flood control for this neighborhood,
4 I am sure that compromises can be arrived at. And, in
5 fact, I'm grateful for some of the positive suggestions
6 that have already been made tonight.

7 But integral to the flood control project
8 is a roadway project, that based on information that I
9 and others have reviewed, could make the detention ponds
10 less effective and, perhaps, from the neighborhood
11 perspective, ineffective or worse in controlling of
12 floods in our neighborhood, not to mention other issues
13 to which this roadway work may give rise to in the
14 neighborhood, which I won't go into here tonight because
15 that is not the subject of this meeting.

16 But again, for our elected officials
17 present, I want to make it clear that those are our
18 concerns, and we would like to continue to take this up
19 with them. So for the record, for the neighborhood I ask
20 that our elected officials review the project in its
21 entirety carefully in cooperation with the Houston
22 Heights Association and the White Oak Bayou Association
23 to ensure that it improves the neighborhood and not just
24 the freeway.

25 Thank you.

1 MR. MIKE ALFORD: Mark Williamson.

2 MR. MARK WILLIAMSON: I'm not as tall as
3 Ken. My name is Mark Williamson. I'm the president of
4 the Greater Heights Super Neighborhood Council, and I
5 would like to read a letter that we approved and mailed
6 to you. And I will provide a hard copy also to you and
7 various elected officials.

8 The Greater Heights Super Neighborhood
9 Council strongly supports the inclusion of park features
10 and/or park convertibility on both of the areas
11 previously identified as detention ponds for use as
12 mitigation on the I-10 frontage road project. We ask
13 that formal plans be developed to convert the detention
14 pond sites as public parks or sites with park features
15 and amenities.

16 Our super neighborhood council feels that
17 a public park will be of great value to the surrounding
18 community and area as a whole. This letter follows a
19 formal adoption of a resolution in support of the
20 inclusion of the public park as part of the detention
21 ponds that are to be designed as part of the
22 above-referenced project.

23 We understand that TxDOT is willing to
24 support and facilitate a design that would allow for
25 accommodate the use of one or both of the proposed

1 detention ponds as a public park.

2 We strongly urge all parties to come
3 together and reach an agreement regarding the direction
4 of design for this option and to determine an amenable
5 solution for future acceptance, maintenance and upkeep of
6 this park. We specifically ask that one of our
7 representative bodies volunteer to become a sponsor for
8 the park and to agree to future maintenance in a manner
9 acceptable to TxDOT so that final design of the detention
10 ponds may incorporate these aspects.

11 Lastly, Greater Heights Super
12 Neighborhood Council requests that the necessary
13 properties for both detention ponds required for
14 mitigation of the frontage road projects be acquired and
15 constructed prior to or contemporaneously with the
16 construction of the frontage road projects.

17 Furthermore, we understand that a prompt
18 decision must be made if the park option is to be
19 included as part of the detention pond projects.

20 The Greater Heights Super Neighborhood
21 Council requests that a resolution on this matter be made
22 prior to any, quote, point of no return, unquote,
23 decisions from TxDOT on the direction of the detention
24 pond design. We humbly ask that the involved parties
25 coordinate with TxDOT to discuss a date by which a

1 decision can be made.

2 The Greater Heights Super Neighborhood
3 Council understands this must be a collaborative effort
4 on the part of several different entities if the
5 inclusion of the park option to be successful. We want
6 to encourage, wherever possible, the pursuit of shared
7 goals and effective communication between the different
8 representative bodies. We look forward to working with
9 the involved parties to provide the valuable benefits to
10 the community.

11 Thank you for your time and consideration
12 on this matter.

13 And on a personal note, I support the
14 proposal of the White Oak Bayou Association. They look
15 very nice.

16 MR. MIKE ALFORD: Blake Masters.

17 MR. BLAKE MASTERS: No additional
18 comment.

19 MR. MIKE ALFORD: Laura Virgadamo. Laura
20 Virgadamo?

21 (No response.)

22 MR. MIKE ALFORD: Robin Holzer.

23 MS. ROBIN HOLZER: Good evening. My name
24 is Robin Holzer, and I chair the board of the Citizens'
25 Transportation Coalition, which is an all-volunteer

1 grassroots group that works to engage neighborhoods in
2 the transportation pattern.

3 I want to start by saying that we support
4 the extensive work and recommendations of the White Oak
5 Bayou Association, whose volunteers have done a heroic
6 effort to figure out how to make sure these detention
7 facilities are a neighborhood amenity and not merely a
8 flood mitigation element.

9 We also share the concerns of the Houston
10 Heights Association and other neighborhoods along I-10
11 who still have unaddressed issues with the roadway
12 project that is in concert with the detention facilities
13 tonight's hearing is about.

14 To that end, I want to reiterate
15 something you have heard tonight already, which is that
16 given the concerns about flooding in the neighborhood,
17 it's essential that the roadway project and the flood
18 detention project be synchronized, such that the new
19 flood mitigation measures come on-line in conjunction
20 with the roadway. You can't rush forward with the road
21 and get to the road so quickly. It's key, essentially.

22 I also want to support the recommendation
23 you've heard several times tonight to employ context
24 sensitive design. In June of 2009, the Texas
25 Transportation Commission at the highest level said

1 context matters and it's essential on all projects that
2 TxDOT develops to work with the neighborhood to figure
3 out what they value and how to make sure the new projects
4 meet their needs as much as possible; and this project is
5 a great opportunity to do that. So I want to exhort you
6 to embrace context sensitivity as you move forward with
7 the design of this project.

8 Finally, I want to wrap up by saying
9 tonight's hearing, this formal public hearing, is a
10 required piece of an extensive Federal process, and you
11 guys know that. We know that. And it includes the
12 environmental assessment that has been done and so on.
13 It is a step of many steps, a long process to ultimately
14 lead to the funding of the project with Federal funds.

15 However, I urge both TxDOT and the
16 elected officials and the neighbors in this room not to
17 let this hearing be the extent -- or the final extent of
18 the engagement on this project. While this is one
19 opportunity for you to hear from us, please don't let it
20 be the last, and please don't let the communication be
21 one direction.

22 With that, I would conclude by saying
23 public participation matters, and we strongly believe
24 that the input you get from the community will make your
25 project better. Work with us.

1 Thank you.

2 MR. MIKE ALFORD: Nancy Wilcox.

3 MS. NANCY WILCOX: Thank you. I
4 appreciate the opportunity to speak. I would like to
5 reiterate the concern of concurrent construction of the
6 detention basins and the project.

7 I am concerned that the land has yet to
8 be acquired; nor has there been much discussion, if any
9 discussion, as to where the funding is going to come from
10 for the acquisition of these detention basins. At this
11 point, as I understand, the design work is not even
12 completed because the land has yet to be acquired.

13 So I am concerned about how a project
14 that has already been let is going to be constructed
15 concurrent to the construction of the detention basins
16 when the property has yet to be acquired.

17 Given that situation, I would propose
18 that there would be no grave raising and no pump station
19 installed in the area where the water leaves White Oak
20 Bayou and then floods into I-10, so that I-10 continues
21 as the default detention basin until -- until the
22 detention basins are, in fact, functioning and on-line.

23 There is another safety concern that I
24 would like to articulate. At the westbound feeder, as it
25 goes over White Oak Bayou at Yale, drops down to the

1 level -- or to below the BFE so that, in fact, when
2 traffic exits on a dark rainy night, it is conceivable
3 that the driver would not see the water flowing over the
4 bridge because it is well below BFE and, in fact, run
5 into several feet of water and could conceivably be
6 whisked off the bridge because that bridge is well below
7 the BFE and I'm concerned that we would be creating a
8 very serious safety issue there.

9 I think that would conclude my comments.

10 Thank you.

11 MR. MIKE ALFORD: David Danburg.

12 MR. DAVUD DANBURG: I'm David Danburg and
13 I own the majority of the property between 4th, 5th and
14 6th Street, to the west of Yale and this is where the
15 detention pond is going to be dug out. And at the same
16 time the Yale Street from I-10 to 610 is going to be
17 under construction, I have no access or egress from my
18 property if those projects are simultaneously being
19 done -- worked on.

20 On a more personal note, the maintenance
21 of 5th Street and 6th Street between Yale and that
22 detention pond has been left solely to me for the last 40
23 years. I have permitted, built and paid for the sanitary
24 sewer system back there and the street repairs on both
25 5th and 6th Street and I would respectfully submit that

1 you-all pay a little bit of homage to me when you are
2 tearing them up with trucks going in and out of the
3 street.

4 But particularly I'm concerned about the
5 fact that Yale would be built and torn up at the same
6 time that the detention pond work is being done, and it's
7 going to create a great hardship for us. We've been
8 there for 60-something years, and we don't want the
9 discomfort of having to tell people we can't get to our
10 property because of the work on Yale Street and the work
11 back on the detention pond.

12 Also, on another personal note, Debbie
13 Boreman is here tonight. She talked me into,
14 20-something years ago, planting those many oak trees
15 along Yale Street and when I built the building on Yale
16 between 5th and 6th, all of that landscaping has just
17 been torn up last year.

18 I don't want my trees destroyed. I don't
19 know how to prevent it but between me crying to Dale
20 Garchintzky (phonetically), Adrian Garcia, Felix Fraga,
21 and I don't want to go to Councilman Gonzalez and start
22 crying about all the work that's being done to tear down
23 all of the money that I've spent over the last 40 or 50
24 years.

25 I would respectfully submit that someone

1 give me some consideration when all of this work goes on.

2 Thank you.

3 MR. MIKE ALFORD: John Kleiber.

4 MR. JOHN KLEIBER: Hello. My name is
5 John Kleiber with the Floodway Coalition of Houston. One
6 of the primary concerns that I have is the feeder road
7 bridges at Yale. The feeder road bridges at Yale are
8 designed to be 4 feet below the base flood elevation.
9 City of Houston Ordinance, 1943, dictates that no
10 structure shall be built unless it is 18 inches above the
11 base flood elevation. State of Texas, TxDOT should not
12 be above the laws that us, as the citizens of Houston,
13 have got to abide by.

14 Secondly, it's a hazard because,
15 effectively, if you put a bridge 4 feet below the base
16 flood elevation, you are creating a dam. Where is the
17 water going to go? It has but one place, and that's into
18 the neighborhood.

19 In addition, there are several spillways,
20 more specifically the one in front of the Firefighters'
21 Credit Union. During major flooding events, that water
22 is as much as 10 feet high. If the ramp that you guys
23 are proposing to put in is put in place, that is going to
24 create yet another wall if you do not have detention in
25 place to hold the water. Again, effectively, what you

1 will have is a 10-foot dam. You'll have a nice dry I-10,
2 but your neighborhoods are going to be flooding.

3 I look around this room and I see a lot
4 of signs and it looks interesting when I see these icons
5 of flooding on I-10, but I dare say if you ask anyone
6 here, they would rather see a dozen 18 wheelers with some
7 water than hundreds upon hundreds of their homes.

8 Thank you very much.

9 (Applause.)

10 MR. MIKE ALFORD: Darryl Heine.

11 MR. DARRYL HEINE: My name is Darryl
12 Heine.

13 Thank you, Mr. Gonzalez, for coming to
14 this hearing. You-all have got a good councilman out
15 here.

16 I just found out about this project
17 yesterday. You would think that a person who is a native
18 Houstonian, that grew up in this neighborhood would have
19 gotten some kind of notice from TxDOT, when I own a piece
20 of property that I just saw on your map up here that's
21 going to be impacted by this flood -- flood pond. And
22 I'm wondering how in the world that my house won't be in
23 it if the streets are gone and my house. That is kind of
24 interesting.

25 Even I know that the Federal Government,

1 we had to give them notice a year in advance when we had
2 to get rid of the HUD people that we had up in Brenham;
3 and I'm wondering why in the world TxDOT didn't give me
4 any kind of notice that my property is going to be
5 impacted.

6 What's interesting also is I'm wondering
7 if whether or not the City of Houston and TxDOT have been
8 in collaboration with this whole program for who knows
9 how far back. It seems that this floodway was put into
10 fruition about two or three years ago to affect 9900
11 properties and the people got no compensation for their
12 land but their tax values are somewhat still in place.

13 I'm dumbfounded, dumbfounded by all of
14 this; and I would like to know why I haven't gotten any
15 information.

16 That's all I've got to say.

17 Thank you.

18 MR. MIKE ALFORD: Okay. We will now
19 close this hearing. It is 7:52, and the hearing is
20 adjourned.

21 Thank you for coming.

22 * * * * *

23
24
25

WRITTEN PUBLIC COMMENTS

**Public Hearing
IH 10 at White Oak Bayou Detention Basins
Houston, Texas**

COMMENT FORM (FORMA DE COMENTARIO)
February 18, 2010

Thank you for attending the IH 10 at White Oak Bayou Detention Basins Public Hearing. Your comments are important and will be taken into consideration regarding the proposed project.

Gracias por asistir la Audiencia Pública de IH 10 y White Oak Bayou para Palanganas de Detención. Sus comentarios son importantes y serán tomados en consideración con respecto al proyecto propuesto.

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Address (Dirección): 607 AUSTON, + 22003 2203 GROSSMITH ST

PLEASE COMPLETE THE APPROPRIATE ITEMS BELOW:
POR FAVOR COMPLETE LOS ARTÍCULOS APROPIADOS ABAJO:

1. How did you hear about this hearing? / Como se informo de esta audiencia?

Newspaper / Periodico

Received Notice by Mail / Aviso por Correo

Told by Friend or Neighbor / Amigo o vecino

Other (please explain) / Otro (por favor explique)

2. I am primarily interested in the project from the standpoint of a:

Estoy interesado principalmente en el proyecto del punto de vista de:

Residential property owner or renter
Dueño de propiedad residencial o inquilino

Highway User
Usuario de Carretera

Business property owner or lessee Apart @ 607 Auston
Dueño de propiedad comercial o inquilino

Other (please explain)
Otro (por favor explique)

COMMENTS (COMENTARIOS):

I have concerns about mosquitoes, whether the land will have any kind of park / greenspace use and land ethy / foundation issues that may arise from having this retention pond placed.

I would be fully in support of this project if the mosquito + park issues were addressed.

**Public Hearing
IH 10 at White Oak Bayou Detention Basins
Houston, Texas**

COMMENT FORM (FORMA DE COMENTARIO)
February 18, 2010

Thank you for attending the IH 10 at White Oak Bayou Detention Basins Public Hearing. Your comments are important and will be taken into consideration regarding the proposed project.

Gracias por asistir la Audiencia Pública de IH 10 y White Oak Bayou para Palanganas de Detención. Sus comentarios son importantes y serán tomados en consideración con respecto al proyecto propuesto.

Name (Nombre): J MICHAEL GOODWIN Phone (Teléfono): 713 861 6151
Address (Dirección): 718 RUTLAND ST 77007

PLEASE COMPLETE THE APPROPRIATE ITEMS BELOW:
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Estoy interesado principalmente en el proyecto del punto de vista de:

Residential property owner or renter Highway User
Dueño de propiedad residencial o inquilino Usuario de Carretera
 Business property owner or lessee Other (please explain)
Dueño de propiedad comercial o inquilino Otro (por favor explique)

COMMENTS (COMENTARIOS):

I would strongly support TXDOT working w/ White Oak Bayou Assoc to ensure initial construction of the detention ponds makes them amenable to development as a multi-use detention facility. The intent would be to maximize the benefits that accrue from the expenditure from both public safety & utility.

**Public Hearing
IH 10 at White Oak Bayou Detention Basins
Houston, Texas**

COMMENT FORM (FORMA DE COMENTARIO)
February 18, 2010

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Name (Nombre): JIM OHMART Phone (Teléfono): 713 674 2528

Address (Dirección): MY RENTAL PROPERTY 4510 CORNISH
MY HOME 4510 INMAN HOUSTON 77020

PLEASE COMPLETE THE APPROPRIATE ITEMS BELOW:
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Other (please explain) / Otro (por favor explique)

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Dueño de propiedad residencial o inquilino

Highway User
Usuario de Carretera

Business property owner or lessee
Dueño de propiedad comercial o inquilino

Other (please explain)
Otro (por favor explique)

COMMENTS (COMENTARIOS):

MY TENANT IS A RETIRED SCHOOL TEACHER
WHO HAS LIVED THERE SINCE 1980. HE IS
DISABLED AND I HAVE KEPT HIS RENT WELL
BELOW MARKET. I AM CONCERNED ABOUT
HIS ABILITY TO FIND AFFORDABLE
HOUSING

**Public Hearing
IH 10 at White Oak Bayou Detention Basins
Houston, Texas**

**COMMENT FORM (FORMA DE COMENTARIO)
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Name (Nombre): DENNIS M. O'BRIEN Phone (Teléfono): 281-450-2953
Address (Dirección): 310 W 6th St. Houston, TX 77007

PLEASE COMPLETE THE APPROPRIATE ITEMS BELOW:
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Highway User
Usuario de Carretera

Business property owner or lessee
Dueño de propiedad comercial o inquilino

Other (please explain)
Otro (por favor explique)

COMMENTS (COMENTARIOS):

**PLEASE RETURN THIS FORM DURING THE MEETING OR
SUBMIT BY MAIL BY March 4, 2010**

**POR FAVOR DEVUELVA ESTA FORMA DURANTE LA REUNIÓN O
ENVIAR POR CORREO EL 4 DE MARZO DE 2010**

**Public Hearing
IH 10 at White Oak Bayou Detention Basins
Houston, Texas**

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Name (Nombre): DEBORAH OLIVER Phone (Teléfono): (281) 208-8587

Address (Dirección): 2219 PATTERSON ST, HOUE, TX 77007

PLEASE COMPLETE THE APPROPRIATE ITEMS BELOW:
POR FAVOR COMPLETE LOS ARTÍCULOS APROPIADOS ABAJO:

1. How did you hear about this hearing? / Como se informo de esta audiencia?

- | | |
|--|--|
| <input type="checkbox"/> Newspaper / Periodico | <input checked="" type="checkbox"/> Received Notice by Mail / Aviso por Correo |
| <input type="checkbox"/> Told by Friend or Neighbor / Amigo o vecino | <input type="checkbox"/> Other (please explain) / Otro (por favor explique) |

2. I am primarily interested in the project from the standpoint of a:
Estoy interesado principalmente en el proyecto del punto de vista de:

- | | |
|--|--|
| <input checked="" type="checkbox"/> Residential property owner or renter
Dueño de propiedad residencial o inquilino | <input type="checkbox"/> Highway User
Usuario de Carretera |
| <input type="checkbox"/> Business property owner or lessee
Dueño de propiedad comercial o inquilino | <input type="checkbox"/> Other (please explain)
Otro (por favor explique) |

COMMENTS (COMENTARIOS):

THE RIGHT TO OWNERSHIP IS IN COURT, AS

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Name (Nombre): TAMMY MOORE Phone (Teléfono): 713-907-0354
Address (Dirección): 310 W btn

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Usuario de Carretera

Business property owner or lessee
Dueño de propiedad comercial o inquilino

Other (please explain)
Otro (por favor explique)

COMMENTS (COMENTARIOS):

**Public Hearing
IH 10 at White Oak Bayou Detention Basins
Houston, Texas**

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Name (Nombre): DONNA BENNETT Phone (Teléfono): 713 868 6979
Address (Dirección): 528 HARVARD HOUSTON TX 77007

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Highway User
Usuario de Carretera

Business property owner or lessee
Dueño de propiedad comercial o inquilino

Other (please explain)
Otro (por favor explique)

COMMENTS (COMENTARIOS):

- REQUEST CONSTRUCTION OF DETENTION PONDS IN CONJUNCTION
WITH CONSTRUCTION OF 14.10 FEEDER PROJECT

- REQUEST DETENTION PONDS BE CONSTRUCTED SUCH THAT
THEY CAN BE USED BY THE COMMUNITY AS MUCH NEEDED
GREEN SPACE

**Public Hearing
IH 10 at White Oak Bayou Detention Basins
Houston, Texas**

COMMENT FORM (FORMA DE COMENTARIO)

February 18, 2010

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Name (Nombre): Jim Bennett Phone (Teléfono): 713 868 6979

Address (Dirección): 528 HARVARD Street 77007

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Other (please explain) / Otro (por favor explique)

HOUSTON HEIGHTS ASSOCIATION

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Highway User
Usuario de Carretera

Business property owner or lessee
Dueño de propiedad comercial o inquilino

Other (please explain)
Otro (por favor explique)

COMMENTS (COMENTARIOS):

If TxDOT will use tax dollars for this project, maximum public benefit must sought. Please work with City of Houston, Harris County Flood Control District, Houston Heights Association, White Oak Bayou Association, and other interested groups to maximize public use/benefit of this project. This must include leaving the project as "park ready".

**Public Hearing
IH 10 at White Oak Bayou Detention Basins
Houston, Texas**

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February 18, 2010

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Name (Nombre): Christine Spin Phone (Teléfono): 713.787.0258

Address (Dirección): 622 East 8th Street

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Dueño de propiedad residencial o inquilino

Highway User
Usuario de Carretera

Business property owner or lessee
Dueño de propiedad comercial o inquilino

Other (please explain)
Otro (por favor explique)

COMMENTS (COMENTARIOS):

Because of the proximity of IH-10 and the
detention basins to a vibrant, active neighborhood,
TxDOT would be considerate and cooperative
to create a visually appealing basin area. ~~The~~
The neighborhood strongly urges TxDOT to assess
and design an area basin that can expand
its use as a detention pond to a park that is
pedestrian-friendly. By making the additional
investment of time + money to create an environment
that serves and incorporates the neighborhood, TxDOT
would improve the quality of life beyond the intended
flood mitigation.

CSJ: 0271-07-292

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**POR FAVOR DEVUELVA ESTA FORMA DURANTE LA REUNIÓN O
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**Public Hearing
IH 10 at White Oak Bayou Detention Basins
Houston, Texas**

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Name (Nombre): KEN HOGE Phone (Teléfono): 713-869-8835
Address (Dirección): 505 HEIGHTS BLVD

PLEASE COMPLETE THE APPROPRIATE ITEMS BELOW:
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Hou Heights Assoc.

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Residential property owner or renter Highway User
 Dueño de propiedad residencial o inquilino Usuario de Carretera
 Business property owner or lessee Other (please explain)
 Dueño de propiedad comercial o inquilino Otro (por favor explique)

COMMENTS (COMENTARIOS):

My home at 505 Heights Blvd is just adjacent to the 100 year flood plain of White Oak Bayou. I also reside just 3 blocks from I-10. I am concerned about the increase in traffic & traffic noise on the access road for I-10 on the north & south sides. which will be caused by the Yale access ramps. I am also very concerned about the impact of this project on the flood plain. It is very difficult to see either the benefit to our neighborhood of this project or the possibility that it will not have an adverse impact on our quality of life and property. I strongly oppose this project as described and request it not proceed.

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**Public Hearing
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Name (Nombre): Scott Johnson Phone (Teléfono): 713 300 1400

Address (Dirección): 6310 Haskell St., Houston, TX 77007

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Highway User
Usuario de Carretera

Business property owner or lessee
Dueño de propiedad comercial o inquilino

Other (please explain)
Otro (por favor explique)

COMMENTS (COMENTARIOS):

I strongly support modifications of detention pond
plans to accommodate park uses as proposed
by the White Oak Bayou Association.

**PLEASE RETURN THIS FORM DURING THE MEETING OR
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Name (Nombre): Mark Williamson Phone (Teléfono): 713-864-4230

Address (Dirección): 1433 Columbus, Houston, TX 77008-4532

PLEASE COMPLETE THE APPROPRIATE ITEMS BELOW:
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Dueño de propiedad comercial o inquilino

Other (please explain)
Otro (por favor explique)

President of
Super Neighborhood
Council

COMMENTS (COMENTARIOS):

Please see attached (if I can find a paper clip) letter from Greater Heights Super Neighborhood Council.
(I am the President of the Council.)

Personally, I support the proposals of the White Oak Bayou Association



Greater Heights

Super Neighborhood Council

February 16, 2010

To: Delvin Dennis, Houston District Engineer
Texas Department of Transportation (TxDOT) Houston District
PO Box 1386
Houston, TX 77251-1386

Pat Henry, Director of Project Development
Texas Department of Transportation (TxDOT) Houston District
PO Box 1386
Houston, TX 77251-1386

Mayor Annise Parker, City of Houston
PO Box 1562
Houston, TX 77251

Honorable Stephen Costello, City of Houston Council Member At Large Pos. 1
900 Bagby, 1st Floor
Houston, TX 77002

Honorable Edward Gonzalez, City of Houston Council Member District H
900 Bagby, 1st Floor
Houston, TX 77002

Honorable Brenda Stardig, City of Houston Council Member District A
900 Bagby, 1st Floor
Houston, TX 77002

Judge Ed Emmett, Harris County
1001 Preston, 9th Floor
Houston, TX 77002

Honorable El Franco Lee, Harris County Commissioner Precinct 1
1001 Preston, 9th Floor
Houston, TX 77002

Honorable Sylvia R. Garcia, Harris County Commissioner Precinct 2
1001 Preston, 9th Floor
Houston, TX 77002

Honorable Jerry Eversole, Harris County Commissioner Precinct 4
1001 Preston, 9th Floor
Houston, TX 77002-1863

Honorable Shelia Jackson Lee, US House of Representatives, District 18
420 W. 19th St
Houston, TX 77008

Honorable Mario Gallegos, State Senate District 6
PO Box 41
Galena Park, TX 77547

Honorable John Whitmire, State Senate District 15
803 Yale Street
Houston, TX 77007

Honorable Dwayne Bohac, State Representative District 138
PO Box 926093
Houston, TX 77292

Honorable Jessica Farrar, State Representative District 148
PO Box 30099
Houston, TX 77249

Cc: Joe Turner, Director, Houston Parks and Recreation Department
2999 South Wayside
Houston, TX 77023

Mike Talbott, Director, Harris County Flood Control District
9900 Northwest Freeway
Houston, TX 77092

Paul Mauricio, Harris County Precinct 1 Park Superintendent
7901 El Rio St
Houston, TX 77054

Gilbert Smith, Harris County Precinct 2 Park Superintendent
16003 Lorenzo
Channelview, TX 77530

Dennis Johnston, Harris County Precinct 4 Park Administrator
22540 Aldine Westfield
Spring, TX 77373

From: Greater Heights Super Neighborhood Council

**Re: Proposed I-10 Frontage Road and Detention Pond Projects Between
Washington/Wescott and Taylor
TxDOT projects # CSJ 0271-07-242 and CSJ 0271-07-292**

Dear Ladies and Gentlemen:

The Greater Heights Super Neighborhood Council strongly supports the inclusion of park features and/or park convertibility on both of the areas previously identified as detention ponds for use as mitigation on the I-10 frontage road project. We ask that formal plans be developed to convert the detention pond sites into public parks or sites with park

features and amenities. Our Super Neighborhood Council feels that a public park will be of great value to the surrounding community and area as a whole. This letter follows a formal adoption of a resolution in support of the inclusion of a public park as part of the detention ponds that are to be designed as part of the above referenced projects.

We understand that the TxDOT is willing to support and facilitate a design that would allow for and accommodate the use of one or both of the proposed detention ponds as a public park. We strongly urge all parties to come together and reach an agreement regarding the direction of design for this option and to determine an amenable solution for future acceptance, maintenance, and upkeep of this park. We specifically ask that one of our representative bodies volunteer to become the sponsor for the park and to agree to future maintenance in a manner acceptable to TxDOT so that final design of the detention ponds may incorporate these aspects.

Lastly, Greater Heights Super Neighborhood Council requests that the necessary property for both detention ponds required for mitigation of the frontage road projects be acquired and constructed prior to or contemporaneously with the construction of the frontage road projects. Furthermore, we understand that a prompt decision must be made if the park option is to be included as part of the detention pond projects. The Greater Heights Super Neighborhood Council requests that a resolution on this matter be made prior to any "point of no return" decisions from TxDOT on the direction of the detention pond design. We humbly ask that the involved parties coordinate with TxDOT to discuss a date by which a decision must be made.

The Greater Heights Super Neighborhood Council understands that this must be a collaborative effort on the part of several different entities if the inclusion of the park option is to be successful. We want to encourage, wherever possible, the pursuit of shared goals and effective communication between the different representative bodies. We look forward to working with the involved parties to provide a valuable benefit to the community. Thank you for your time and consideration of this matter.

Regards,

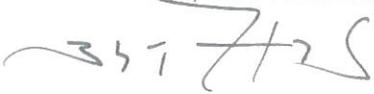
Greater Super Heights Neighborhood Council



Mark Williamson, President



Glen Clement, Vice President



Blake Masters, Secretary/Treasurer



Sid Edmonds, Infrastructure, Drainage, and Flooding Committee Chair



Greater Heights

Super Neighborhood Council

February 16, 2010

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PO Box 1386
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803 Yale Street
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PO Box 926093
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TxDOT projects # CSJ 0271-07-242 and CSJ 0271-07-292**

Dear Ladies and Gentlemen:

The Greater Heights Super Neighborhood Council strongly supports the inclusion of park features and/or park convertibility on both of the areas previously identified as detention ponds for use as mitigation on the I-10 frontage road project. We ask that formal plans be developed to convert the detention pond sites into public parks or sites with park

features and amenities. Our Super Neighborhood Council feels that a public park will be of great value to the surrounding community and area as a whole. This letter follows a formal adoption of a resolution in support of the inclusion of a public park as part of the detention ponds that are to be designed as part of the above referenced projects.

We understand that the TxDOT is willing to support and facilitate a design that would allow for and accommodate the use of one or both of the proposed detention ponds as a public park. We strongly urge all parties to come together and reach an agreement regarding the direction of design for this option and to determine an amenable solution for future acceptance, maintenance, and upkeep of this park. We specifically ask that one of our representative bodies volunteer to become the sponsor for the park and to agree to future maintenance in a manner acceptable to TxDOT so that final design of the detention ponds may incorporate these aspects.

Lastly, Greater Heights Super Neighborhood Council requests that the necessary property for both detention ponds required for mitigation of the frontage road projects be acquired and constructed prior to or contemporaneously with the construction of the frontage road projects. Furthermore, we understand that a prompt decision must be made if the park option is to be included as part of the detention pond projects. The Greater Heights Super Neighborhood Council requests that a resolution on this matter be made prior to any "point of no return" decisions from TxDOT on the direction of the detention pond design. We humbly ask that the involved parties coordinate with TxDOT to discuss a date by which a decision must be made.

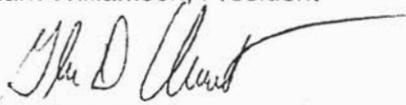
The Greater Heights Super Neighborhood Council understands that this must be a collaborative effort on the part of several different entities if the inclusion of the park option is to be successful. We want to encourage, wherever possible, the pursuit of shared goals and effective communication between the different representative bodies. We look forward to working with the involved parties to provide a valuable benefit to the community. Thank you for your time and consideration of this matter.

Regards,

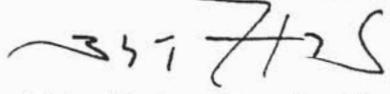
Greater Super Heights Neighborhood Council



Mark Williamson, President



Glen Clement, Vice President



Blake Masters, Secretary/Treasurer



Sid Edmonds, Infrastructure, Drainage, and Flooding Committee Chair

SHEILA JACKSON LEE
18TH DISTRICT, TEXAS

WASHINGTON OFFICE:
2435 Rayburn House Office Building
Washington, DC 20515
(202) 225-3816

DISTRICT OFFICE:
1919 SMITH STREET, SUITE 1180
THE GEORGE "MICKEY" LELAND FEDERAL BUILDING
HOUSTON, TX 77002
(713) 655-0050

ACRES HOME OFFICE:
6719 WEST MONTGOMERY, SUITE 204
HOUSTON, TX 77019
(713) 691-4882

HEIGHTS OFFICE:
420 WEST 19TH STREET
HOUSTON, TX 77008
(713) 961-4070

FIFTH WARD OFFICE:
3300 LYONS AVENUE, SUITE 301
HOUSTON, TX 77020

Congress of the United States
House of Representatives
Washington, DC 20515

February 16, 2010

COMMITTEES:
JUDICIARY

SUBCOMMITTEES:
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IMMIGRATION, CITIZENSHIP, REFUGEES, BORDER
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CRIME, TERRORISM AND HOMELAND SECURITY

HOMELAND SECURITY

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PROTECTION
BORDER, MARITIME, AND GLOBAL COUNTERTERRORISM

FOREIGN AFFAIRS

SUBCOMMITTEES:
AFRICA AND GLOBAL HEALTH
MIDDLE EAST AND SOUTH ASIA

SENIOR WHIP
DEMOCRATIC CAUCUS

WHIP
CONGRESSIONAL BLACK CAUCUS

CHAIR
CONGRESSIONAL CHILDREN'S CAUCUS

Delvin Dennis, P.E., District Engineer (TxDOT)
Texas Department of Transportation
7721 Washington Avenue, Fifth Floor
Houston, TX 77007

Dear Mr. Dennis:

I joined some very concerned neighbors this past weekend on the banks of White Oak Bayou. I was impressed by the cohesiveness of the neighbors and of the massive green belt of which our bayous are part. However, I was disappointed at the disregard of the greenbelt and lack of development. Let us agree on one thing tonight. We will all work together to make White Oak Bayou (and the detention ponds) into park space with complimentary hike and bike trails. Let's work together to connect our bikeways and make them better.

I strongly support recommendations by White Oak Bayou Association and Greater Heights Super Neighborhood Council encouraging Texas Department of Transportation to include provisions for park features to be added in the areas identified as detention ponds for mitigation on the I-10 frontage road project.

The \$40 million that it will cost to acquire the land alone will provide multiple benefits to the surrounding communities by TxDOT working with Harris County Flood Control District and the City of Houston to develop park amenities. We encourage a collaborative effort for the inclusion of the park option.

The recommendations also make reference to potential trails and other park improvements. We understand that construction and maintenance of the park improvements would be funded through this collaborative effort with community support.

We want to encourage, wherever possible, the pursuit of shared goals and effective communication between the different representative bodies. We look forward to working with TxDOT to deliver this valuable benefit to the community.

Thank you for your consideration.

Sincerely,



Sheila Jackson Lee
Member of Congress

**Public Hearing
IH 10 at White Oak Bayou Detention Basins
Houston, Texas**

**COMMENT FORM (FORMA DE COMENTARIO)
February 18, 2010**

Thank you for attending the IH 10 at White Oak Bayou Detention Basins Public Hearing. Your comments are important and will be taken into consideration regarding the proposed project.

Gracias por asistir la Audiencia Pública de IH 10 y White Oak Bayou para Palanganas de Detención. Sus comentarios son importantes y serán tomados en consideración con respecto al proyecto propuesto.

Name (Nombre): Carole C Johnston Phone (Teléfono): 281.774.2029
Address (Dirección): 1046 Harvard; Houston, TX 77008

**PLEASE COMPLETE THE APPROPRIATE ITEMS BELOW:
POR FAVOR COMPLETE LOS ARTÍCULOS APROPIADOS ABAJO:**

1. How did you hear about this hearing? / Como se informo de esta audiencia?

Newspaper / Periodico Received Notice by Mail / Aviso por Correo
 Told by Friend or Neighbor / Amigo o vecino Other (please explain) / Otro (por favor explique)
White Oak Bayou Assn. email

2. I am primarily interested in the project from the standpoint of a:
Estoy interesado principalmente en el proyecto del punto de vista de:

Residential property owner or renter Highway User
Dueño de propiedad residencial o inquilino Usuario de Carretera
 Business property owner or lessee Other (please explain)
Dueño de propiedad comercial o inquilino Otro (por favor explique)

COMMENTS (COMENTARIOS):

Am delighted at the prospect of these detention ponds, not only for the floodwater mitigation but especially for the add'l greenspace it affords Houstonians, enhancing our quality of life.

The addition of an exit ramp at Yale from I-10, on the other hand, does not meet my approval. It is completely unnecessary and unwanted.

CSJ: 0271-07-292

**PLEASE RETURN THIS FORM DURING THE MEETING OR
SUBMIT BY MAIL BY March 4, 2010**

**POR FAVOR DEVUELVA ESTA FORMA DURANTE LA REUNIÓN O
ENVIAR POR CORREO EL 4 DE MARZO DE 2010**

**: TXDOT
DIRECTOR OF PROJECT
DEVELOPMENT
P.O. BOX 1386
HOUSTON, TX**

DPD

**Public Hearing
IH 10 at White Oak Bayou Detention Basins
Houston, Texas**

COMMENT FORM (FORMA DE COMENTARIO)
February 18, 2010

Thank you for attending the IH 10 at White Oak Bayou Detention Basins Public Hearing. Your comments are important and will be taken into consideration regarding the proposed project.

Gracias por asistir la Audiencia Pública de IH 10 y White Oak Bayou para Palanganas de Detención. Sus comentarios son importantes y serán tomados en consideración con respecto al proyecto propuesto.

Name (Nombre): Tom Dornbusch Phone (Teléfono): 713-869-1185
Address (Dirección): 6110 Tvne St.. Houston. 77007

PLEASE COMPLETE THE APPROPRIATE ITEMS BELOW:
POR FAVOR COMPLETE LOS ARTÍCULOS APROPIADOS ABAJO:

1. How did you hear about this hearing? / Como se informo de esta audiencia?

Newspaper / Periodico Received Notice by Mail / Aviso por Correo
 Told by Friend or Neighbor / Amigo o vecino Other (please explain) / Otro (por favor explique)

2. I am primarily interested in the project from the standpoint of a:
Estoy interesado principalmente en el proyecto del punto de vista de:

Residential property owner or renter Highway User
 Dueño de propiedad residencial o inquilino Usuario de Carretera
 Business property owner or lessee Other (please explain)
 Dueño de propiedad comercial o inquilino Otro (por favor explique)

Superneighborhood 22 Council VP Board Member: White Oak Bayou Association Citizen's Transportation Coalition

COMMENTS (COMENTARIOS): This project is designed to displace the stormwater holding capacity of the below-grade IH10 mainlanes. Without that holding capacity, IH10 adjacent neighborhoods would surely have flooded during TS Allison, and are likely to be severely impacted by this project if the proposed detention facilities – for which right-of-way acquisition has not yet occurred – are not functional prior to, or concurrent with, the roadway construction.

As requested by many neighborhood organizations and White Oak Bayou Association, these detention facilities should be constructed to be park-ready neighborhood amenities, rather than inaccessible and neighborhood unfriendly pits.

Additionally, there are multiple bridges planned as part of the roadway expansion connected to this project that are designed to be constructed below the base flood elevation. This is inconsistent with the City of Houston ordinance regulating the elevation of structures in the floodplain. FHWA FEDERAL-AID POLICY GUIDE December 7, 1994; SUBCHAPTER G - ENGINEERING AND TRAFFIC OPERATIONS; PART 650 - BRIDGES, STRUCTURES, AND HYDRAULICS; Subpart A - Location and Hydraulic Design of Encroachments on Flood Plains: Sec.650.115 Design Standards; states that: (5) The design of encroachments shall be consistent with standards established by the FEMA, State, and local governmental agencies for the administration of the National Flood Insurance Program.

CSJ: 0271-07-292

**PLEASE RETURN THIS FORM DURING THE MEETING OR
SUBMIT BY MAIL BY March 4, 2010**

POR FAVOR DEVUELVA ESTA FORMA DURANTE LA REUNIÓN O

DIRECTOR OF PROTE
DEVELOPMENT
P.O. BOX 1386
HOUSTON, TX



March 2, 2010

Mr. Delvin Dennis
District Engineer
Texas Department of Transportation
P.O. Box 1386
Houston, Texas 77251-1386

By email: ddennis@dot.state.tx.us

RE: TxDOT Project # CSJ 0271-07-292
Proposed Flood Mitigation (detention) ponds (along the White Oak Bayou Watershed)

Dear Mr. Dennis,

The Greater Houston Partnership's Quality of Life Committee in collaboration with the *Quality of Life Coalition*, an organization with 92 endorsing organizations in the Houston region, advocates for increased linear parks and open space, development of multi-use trails, improvement in water quality and enhanced flood control along all 9 of Houston's major bayou arteries.

In this regard, we support TxDOT's proposal to construct flood mitigation ponds and minor drainage improvements along the White Oak Bayou watershed near IH 10, in conjunction with IH 10 roadway improvements. We strongly urge TxDOT to proceed with any remaining purchases of land needed for the six (6) recommended flood mitigation pond sites, located along sections of White Oak Bayou from Shepherd Drive to Yale Boulevard, so that construction can begin in the fall of 2010.

We also encourage TxDOT to construct these detention ponds so that they may provide multiple benefits to the community. Over the past 5 years, the City of Houston and Harris County Flood Control District have discovered that use of water quality filtering features and recreational facilities (parks, trails, nature areas) are an important multi-use of flood mitigation detention ponds. An extensive system of linear parks and trails/green space along Houston's major bayou arteries contributes to health and wellness, economic development, flood control, air and water quality. The costs of acquiring the land, excavating the detention ponds and maintaining the ponds can be productively balanced to also provide long-term water quality and recreational benefits to the communities around White Oak Bayou.

Since 2006, TxDOT has targeted construction of these detention ponds along White Oak Bayou as a critical component of flood reduction along the I-10 corridor in the Heights area. Beginning construction this fall is important and timely in conjunction with Harris County and the City of Houston's continued flood control and water quality efforts along White Oak Bayou. We appreciate your cooperation in moving this project forward as quickly as possible. Please contact us with any questions or comments in this regard.

Letter to TxDOT Re: Project # CSJ 0271-07-292
Page 2

Sincerely,



Ed Wulfe
Chair
Quality of Life Policy Committee
Greater Houston Partnership



Charles McMahan
Co-Chair
Steering Committee member
Quality of Life Coalition

Cc: Director of Project Development
Ned Holmes, TxDOT Commissioner

By email: HOU-Piowebmail@DOT.State.Tx.US

Quality of Life Coalition Endorsing Organizations:

- 1960 Area Community Alliance
- Afton Oaks Civic Club
- American Institute of Architects - Houston
- American Planning Association - Houston Section
- American Society of Landscape Architects – Texas Chapter, Houston Gulf Coast Section
- Catholic Charities – Gulf Coast Section
- Bayou Preservation Association
- Bike Houston
- Boulevard Oaks Civic Association
- Brays Bayou Association
- Buffalo Bayou ArtPark
- Buffalo Bayou Partnership
- Center for Houston’s Future
- Central Houston, Inc.
- Christ Church Cathedral
- Commercial Real Estate Women - Houston
- Cypress Creek Greenway Coalition
- Downtown Houston Historic District
- Emerson Environmental Group
- Energy Corridor Management District
- Friends of Anahuac Refuge
- GHASP
- Girl Scouts – San Jacinto Girl Scout Council
- Greater East End Management District
- Greater Houston Partnership
- Greater Houston Preservation Alliance
- Greens Bayou Corridor Coalition
- Gulf Coast Institute
- Hermann Park Conservancy
- Houston Advanced Research Center (HARC)
- Houston Association of Realtors
- Houston Audubon Society
- Houston Canoe Club
- Houston Citizens Chamber of Commerce
- Houston Dog Park Association
- Houston Green
- Houston Intown Chamber of Commerce
- Houston Museum of Natural Science
- Houston Northwest Chamber of Commerce
- Houston Parks Board
- Houston West Chamber of Commerce
- Houston Wellness Association
- Houston Wilderness
- Keep Houston Beautiful
- Katy Prairie Conservancy
- League of Women Voters of Houston Area
- Legacy Land Trust
- Main Street Coalition
- Making Main Street Happen, Inc.
- Memorial Park Conservancy, Inc.
- Midtown Management District
- Midtown Redevelopment Authority/TIRZ
- HARC
- Museum District Business Alliance
- Neartown Association
- North Houston Greenspoint C of C
- Real Estate Association Latinos (R.E.A.L.)
- Ridgecrest Civic Club
- Rotary Club of Harrisburg
- Scenic Houston
- Scenic Texas
- Sierra Club of Houston
- SE Texas Nursery Growers Association
- Spring Branch Oaks Civic Association
- Stella Link Redevelopment Association
- Tanglewood Homes Association, Inc.
- Texas Economic & Demographic Assn
- Texas Department of Transportation
- Texas Children’s Hospital
- The Houston Garden Club
- Trees for Houston
- Trust for Public Land
- University Place Association
- Upper Kirby District Foundation
- Upper Kirby Management District
- Uptown Management District
- Urban Land Institute - Houston District Council
- Washington on Westcott Roundabout Initiative, Inc.
- West Oaks Management District
- Westchase District
- White Oak Bayou Association
- Willow Waterhole Greenspace Conservancy
- Woodcrest Neighborhood Civic Association
- The YMCA of the Greater Houston Area



Mailing Address:
P.O. Box 920510
Houston, Texas 77292
WhiteOakBayouAssociation@Comcast.net

Board of Directors

Evelyn Born Shanley	President
Kyle Blank P.E.	Vice President
Jim Mackey	Vice President
Tom Dornbusch	Vice President
Teresa Matlock	Treasurer
Margaret Dower	Secretary

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Shanna Barnstone	Little White Oak Bayou
Evelyn Born Shanley	Liaison, BPA
Pete Little	Long Time Volunteer
Linda Pechacek	WOB Watershed

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Fred Lazare	Past President
Ernie Sears, M.D.	Past President
Jim Pulliam	Past President
Kevin Shanley	Chairman BPA

Neighborhood Partners

Clark Pines Civic Association
Cottage Grove Civic Club
Greater Inwood Partnership
Houston Heights Association
Inwood Pines Civic Club
Lazybrook Civic Club
Mangum Manor Civic Club
Oak Forest Homeowners Association
Shady Acres Civic Club
Timbergrove Manor Civic Club
Timbergrove Manor Neighborhood Association
Woodland Heights Civic Association

Association Founder

Lynda Mifsud

March 3, 2010

Via e-mail

Texas Department of Transportation
Attn: Director of Project Development
P.O. Box 1386
Houston, Texas 77251-1386

Re: TxDOT Public Hearing 2-18-2010
TxDOT Project# CSJ 0271-07-292
Proposed flood mitigation (detention) ponds
along the White Oak Bayou Watershed

Thank you for holding a public hearing which allowed representatives of the adjacent communities to voice their opinions. WOBA's Detention Basin Design Alternative exhibit was well received and supported by most public hearing attendees, including elected officials who represent the area.

Proposed Design Alternative

The proposed design alternative will not only mitigate storm water impacts but also offset noise, air, visual and roadway runoff pollution, which are all by-products of highway expansion. It will further set the stage for needed urban park space and considers the values of the surrounding neighborhoods. The benefits of a park-ready detention basin construction will greatly offset the minimal additional construction cost.

WOBA partners with fellow Community Organizations

WOBA joins with community groups in the Heights area, the Citizens Transportation Coalition and the Floodway Coalition of Houston to request the following:

- **Storm Water Detention be implemented prior to, or concurrent with Feeder Road Construction**

The historic & existing I-10 storm water storage capacity of I-10 between Patterson Street and Washington Avenue should remain intact until the new detention pond project is completed at a capacity that will offset the loss of this I-10 main lanes detention.

Likewise, the new bayou feeder road bridges in



the Yale Street area should not be constructed until the detention ponds are fully functional.

- **Clarification of Feeder Road Sections and Bridges below the BFE**

WOBA asks that TxDOT explain how the hazardous conditions created by sections of the feeder roads and bridges constructed well below the Base Flood Elevation (BFE) will be resolved. Specifically, the community concern that the new eastbound Yale/Heights exit ramp will take exiting drivers from the I-10 Freeway over a rise and then drop them immediately onto the bayou feeder road bridge at Yale Street which will be located below the BFE. During a heavy rain event this is particularly hazardous.

- **Coordination with other Public Entities and Community Organizations**

We understand that Stimulus Fund requirements for the I-10 Ramp/Feeder Road Project –CSJ:0271-07-242 impose time restrictions. We feel that TxDOT is rushing both the road and detention projects without full community involvement and coordination with local government entities. Improved updated designs and planning would result in better local traffic flow, traffic safety, storm water mitigation and over the long term substantial savings in taxpayer dollars.

WOBA's recommended **Detention Basin Design Alternative** and **Detention and Drainage Study Review and Recommendations by WOBA Vice President Jim Mackey** are attached for inclusion in the public record.

Conclusion

We have long advocated for the mutually inclusive goals of sound flood reduction policy and projects as well as bayou parks and trails. We are able to offer a unique perspective as we work with our fellow community based organizations and elected officials in urging TxDOT to refine their plans for the benefit of their department and the community as a whole.

The 2-18-2010 Public Hearing was another step as we work toward the goal of formulating a project we can all live with and be proud of. Our organization would welcome the opportunity to meet again with TxDOT to discuss flood risks and detention pond designs.

We also ask that stakeholders be kept advised of TxDOT's project progress, preferably by e-mail. Please let the 2-28-2010 Public Hearing not be the last communication.

For instance, it was troubling to hear from owners of properties adjacent to the proposed detention basins who stated that access to their properties would be impaired and that they had not been notified of TxDOT's plans as of the date of the public hearing. We presume this is because TxDOT has not yet purchased the properties. All the more reason to act swiftly.



Sincerely,

A handwritten signature in cursive script that reads "Evelyn Born Shanley".

Evelyn Born Shanley
President
ebshanley@comcast.net
713-826-0457 Cell
713-864-1548 Home

cc: by e-mail:

U.S. Congresswoman Sheila Jackson Lee, 18th District
State Representative Jessica Farrar, District 148
State Representative Dwayne Bohac, District 138
Council Member Edward Gonzales, District H
Council Member Brenda Stardig, District A
Commissioner El Franco Lee, Precinct 1
Commissioner Sylvia Garcia, Precinct 2
Commissioner Jerry Eversole, Precinct 4
Wayne Crull, Harris County Flood Control District
Mark Loethen, City Engineer, City of Houston, PW&E
CM Stephen C. Costello, CoH, At-Large Position 1

Detention and Drainage Study Review and Recommendations

By WOBA Vice President Jim Mackey

TxDOT project # CSJ 0271-07-242 & TxDOT project# CSJ 0271-07-292

WOBA undertook a detailed review of TxDOT's 2/09 Drainage Mitigation Study for the I-10 Project as well as the 2/10 detention pond renditions and the road construction plan sets. In view of the following questions and comments, it is our hope that TxDOT will revisit their drainage study and make modifications to their plans as they relate to detention and mitigation of flows on lower WOB.

For example:

Limits of Drainage Study

While utilizing best practices and technically sound, TxDOT's 2/09 Drainage Study is flawed by the fact that alternative simulations for the proposed new and replacement feeder road bridges did not include models for raising these components to feasible and practical levels close to or better yet above the Base Flood Elevation.

If run, such modeling simulations would have provided TxDOT and the community with solid information about whether additional mitigation measures would be needed to offset any increased flows passing under the raised bridges.

WOBA asks TxDOT to explain in detail how the capacities of the proposed detention pond facilities were calculated. WOBA is concerned that TxDOT may be unknowingly claiming existing storm water storage capacity as their own at site A-5. Please see attached document: I-10 A-5 Existing Detention

Patterson Street

WOBA encourages TxDOT to secure an agreement with the City of Houston (COH) to abandon Patterson Street north of Cornish and to include that street ROW to expand the A-5 Detention Site.

In addition to the benefit of increased detention, removing the Patterson dead-end near White Oak Bayou, reduces the likelihood that this area will become an illegal dumping ground as so often occurs on abandoned streets.

Additionally, WOBA recommends that TxDOT and/or the COH purchase the undeveloped townhome lots along the north side of Cornish for further expansion of the A-5 site.

Park-Ready Wet Bottom Detention

In conjunction with WOBA's proposed "park ready" wet bottom detention sites. We encourage TxDOT and the COH to coordinate on the A9-10 design and construction.

If CoH drainage were to be rerouted to provide semi-permanent flows into the site, additional detention capacity may be required. The adjoining CoH-owned MKT ROW south of the hike-n-bike trail could be sculpted to provide additional storm water storage. Please refer to WOBA's attached Detention Pond Design Alternative.

Detention and Drainage Study Review and Recommendations
By WOBA Vice President Jim Mackey
TxDOT project # CSJ 0271-07-242 & TxDOT project# CSJ 0271-07-292

Project Components below the Base Flood Elevations

The more than 200' of open land between Yale and White Oak Bayou on the westbound feeder ROW could be utilized to raise the approach for the westbound bayou feeder road bridge at Yale. By bringing the elevation of the approach up, the bridge itself could be raised 2-3'.

Yale Street at I-10 and the nearby Yale bayou bridge are located at 42' which is 2-7' below the BFE at this location. The lower chord of the planned eastbound bayou feeder road bridge at this location is at 3-7', again several feet below the BFE. We encourage TxDOT, the COH and Harris County Flood Control District (HCFCD) to coordinate and address flood risks at this location.

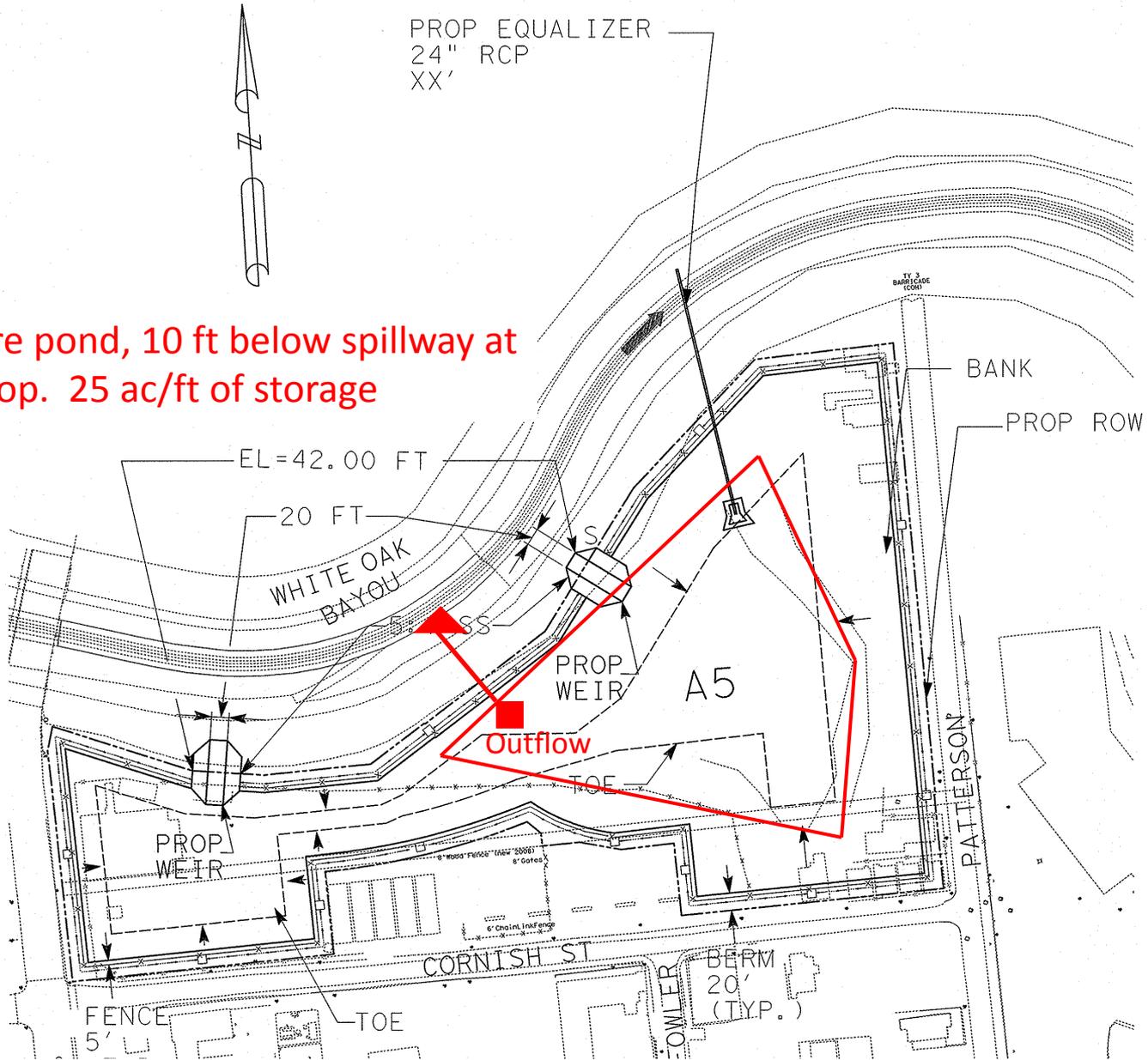
If TxDOT proceeds as planned with the eastbound feeder connection to Yale, it will make it extremely difficult and expensive to raise the grade at Yale and/or replace the 80 year old Yale bayou bridge at a higher elevation in the future. Additionally, TxDOT's feeder roads and bridges will pull increased traffic into this high risk flood area. This is a public safety concern as noted earlier in this letter.

WOBA asks TxDOT to study the impacts of raising the replacement bayou feeder road bridges just west of Studemont to an elevation above the current low lying flow obstructing bridges. While probably not practical from a traffic engineering perspective to raise the new bridges above the BFE, it may be feasible to raise the bridges 1-2'. This is especially the case for the westbound bridge. There appears to be adequate open space west of the Studemont intersection to raise the approaches for the westbound bridge.

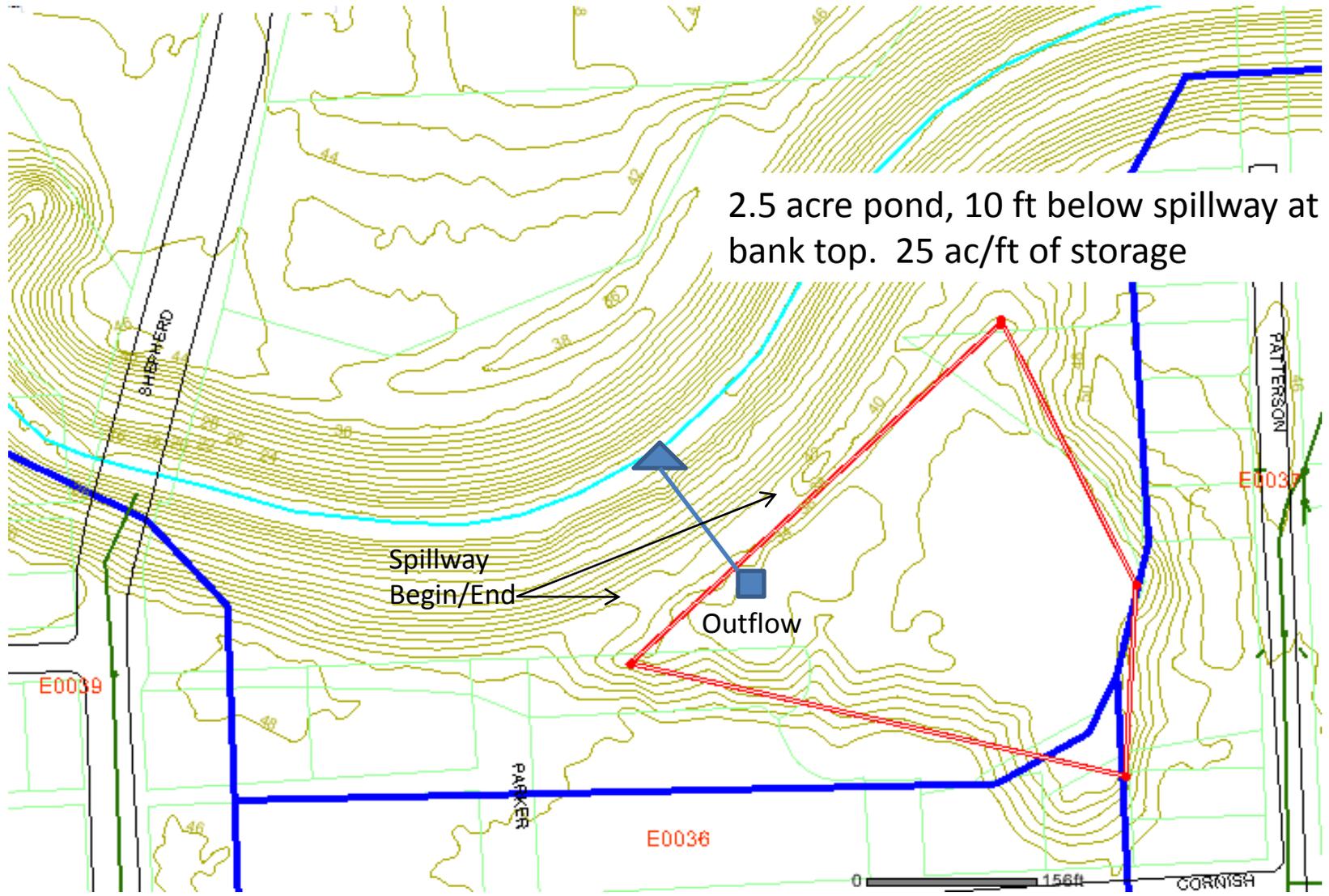
WOBA realizes that raising the bridges at this location may require additional mitigation measures. We once again encourage TxDOT to coordinate with the COH and HCFCD to develop long lasting cost effective flood reduction strategies.

Existing Detention Inside Proposed A5 Site

2.5 acre pond, 10 ft below spillway at bank top. 25 ac/ft of storage



Existing Detention Inside Proposed A5 Site



2.5 acre pond, 10 ft below spillway at bank top. 25 ac/ft of storage

Spillway
Begin/End

Outflow

E0036

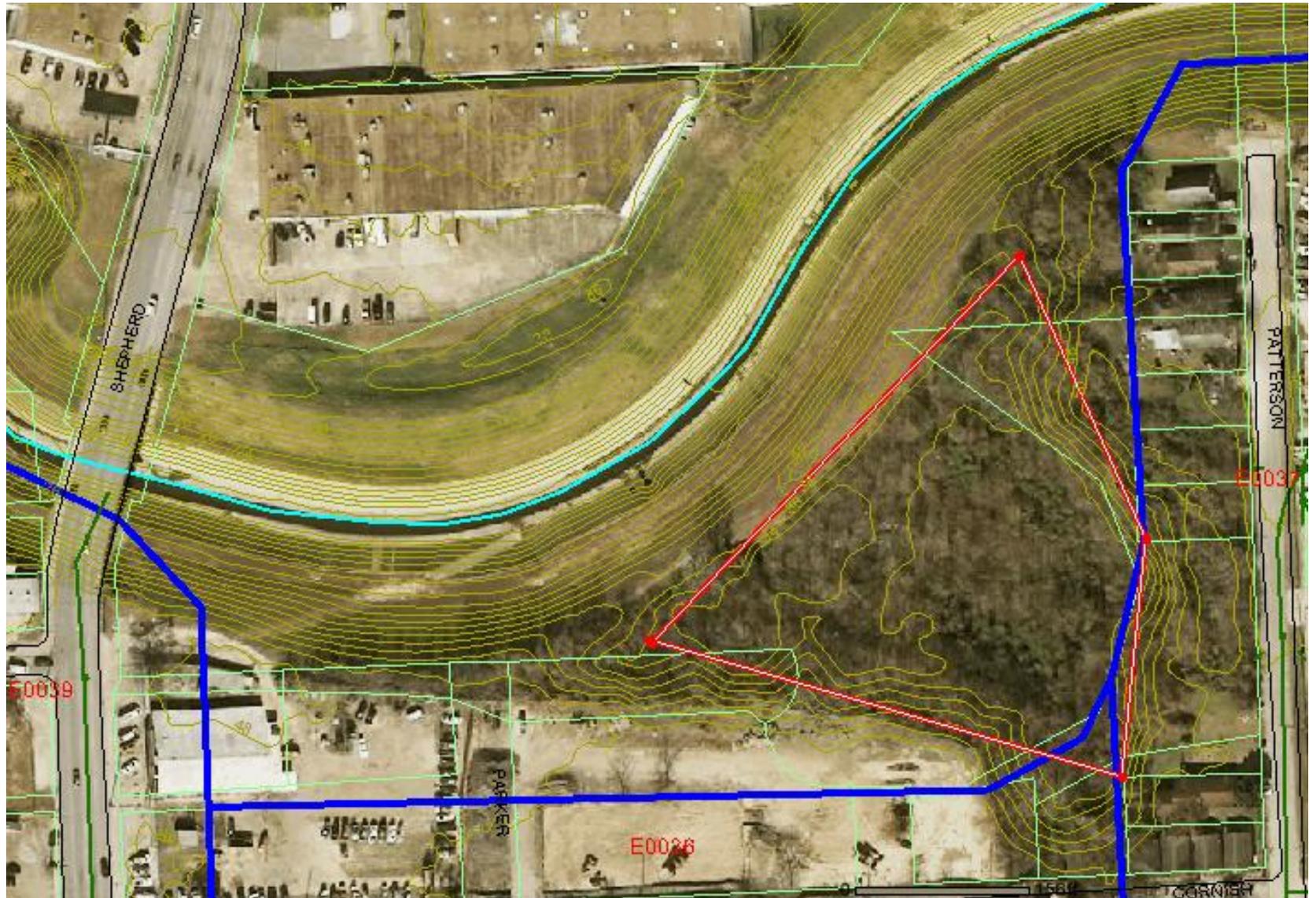
E0037

E0039

0 156ft

CORNISH

Existing Detention Inside Proposed A5 Site





2010 February 7

I-10 Detention Basins

Design Recommendations for Multi-use Basins

White Oak Bayou Association



- MKT TRAIL
- SHEET FLOW WIER
- REFORESTATION
- TOP OF BERM TRAIL
- FOOTBRIDGE
- WET BOTTOM POND
- BOTTOM TRAIL
- INLET WIER AND OUTFALL
- FUTURE WHITE OAK BAYOU TRAIL

A7/A9/A10 Detention Basin

Revised: 2010 February 7

White Oak Bayou Association

I-10 Detention Basin - Design Recommendations

1. General:

- a. These notes and sketches recommend changes to the schematic plans for detention basins proposed by TxDOT for work associated with improvements to I-10 between Washington and Taylor Streets. The recommended changes leave the basic detention basin design as proposed by TxDOT, but with design modifications that will permit the basins to function as meaningful and useful open space for the surrounding communities, and will provide water quality, recreational and aesthetic benefits to the community at large.
- b. The design change recommendations assume that TxDOT will do the work that is well within its financial restrictions (and obligations) to perform, and that work that is clearly not a part of TxDOT's mandate will be done by others at a later date. (See the Partnering suggestions at end of Recommendations).
- c. The drainage work should be coordinated with:
 - i. The City Stormwater Master Plan (SWMP) to be sure that drainage work being installed as part of this project will match up with the proposed improvements shown in the SWMP. The City should be contributing to this project to increase the detention to a point where the mitigation for these future improvement is already in place when they are implemented. There are significant improvements near the east side of the North Bank Basin, and there are proposed storm drain lines connecting to the existing storm drain under the main lanes of I-10 and ultimately draining through the new TxDOT pump station. It is to mitigate the effects of these proposed improvements that we are recommending that the City contribute the land for Basin area A4-1 and that the City purchase the land for Basin area A6-1;
 - ii. The HCFCD Federal Flood Damage Reduction Project for Buffalo Bayou and Lower White Oak Bayou to be sure the detention basin containment berms are constructed such that they will not limit or constrain possible channel adjustment or widening components at a later date. The project planning is under way and is currently evaluating Project Components.
- d. Mitigation earthwork volumes need to be carefully calculated using existing topography and the proposed designs as a basis of volume calculations. Areas or volumes currently connected to the floodplain cannot be counted towards mitigation.
- e. Design recommendations common to all Basins:
 - i. 20' Top berm should be graded to provide a minimum 8' wide maintenance bench (on basin side of berm) with 2% cross-slope designed to accommodate future trails.

1. Backslope swales shall be no wider than 12' and no deeper than 3' (4:1 side slopes);
 2. Backslope drain inlets shall be located to best utilize surrounding natural grades and shall be located frequently enough to limit swale depth to 3' (allows for a 500' run at 0.5% slope, or 1000' between inlets maximum);
 3. Backslope drain outfalls shall be extended to discharge below bottom feature pool elevation (for safety, for ease of mowing, and to provide aquatic habitat shelter).
- ii. Basins shall be provided with maintenance and access ramps to the bottom area. These ramps will provide equipment access to the basin bottom, security vehicle access to the bottoms, and will provide a base on which to build future trails by others. Ramps should be a graded bench of 10' wide in the 3:1 side slope with a gradient less than 5% (or a 20:1 slope).
 - iii. Top 12" of detention basin grade (berms, slopes and planted bottom area) should be clayey topsoil with high organic content to support a strong stand of grass and to support reforestation plantings.
 - iv. Reforestation plantings should be installed as shown on the sketches per normal TxDOT Green Ribbon planting specifications, with the exception that the heavy mulching should be immediately top-seeded so that grass cover and roots will bind the mulch to the soil and prevent it from floating away during a storm event.
 - v. Basins should not be fenced. However, vehicular control is important for safety and in order to restrict motorized vehicles that could damage the basin vegetation. Vehicular control could consist of wooden dome top posts (a TxDOT standard detail) where the basin is open to streets running parallel to the basin. Low, heavy metal pipe guardrails should be used where streets terminate into the basin (for instance at 6th Street).
2. North Bank Basin (A7, A9, A10):
 - a. Add a wet bottom pond to the basin in order to get better utilization of the basin bottom capacity, allowing for a flat bottom profile. Adding the wet bottom provides an additional 94,000cf of storage which allows for the creation of the access ramps into the basins.
 - i. Bottom feature pond to have 5:1 banks to 8' depth;
 - ii. Bottom feature channel to have 5:1 wetland shelves (variable width) and 3:1 lower slopes to minimum 15' wide bottom at 6' depth;
 - iii. Provide a bottom feature crossing point for maintenance and future trail access as shown on sketch plan:
 1. 3- 6'x10' box culverts (16' long) side by side.
 - b. Inflow weir:
 - i. Should be designed to accommodate a future trail crossing to meet ADA standards.
 - ii. Solid concrete slope protection, if deemed necessary, should be used only directly in line with bottom of weir;

- iii. Flanking areas on either side of solid concrete should have open cell cable linked concrete matting with topsoil and grass cover;
 - iv. Minimize visible solid concrete, maximize use of grass-covered erosion control methods wherever possible;
 - v. Outfall headwall and pipe should be relocated to occur at base of inflow weir spillway (incoming flows will clear the outfall area);
 - vi. Toe of spillway and outfall headwall to be designed to allow low-water pedestrian access.
- c. Add two access ramps to bottom of basin, diagonally opposite from northeast corner towards center of basin and from southwest corner towards center of basin.
- d. Add one access ramp on south face of south berm to provide access from future White Oak North Bank Trail to future top of berm trail.
- e. Regrade 7th Street right of way (south side of MKT Trail) to direct sheet flows coming south on Tulane Street into basin (Refer to COH SWMP).
 - i. Construct new culvert under MKT Trail;
 - ii. Construct wide swale along south side of right of way (30' wide, 5:1 side slopes, 4' deep at center);
 - iii. Construct backslope inlet and drain at north end of basin and discharging into bottom feature channel;
 - iv. Extreme event spillway from swale to basin shall have open cell cable linked concrete matting with topsoil and grass cover.
- f. Backslope drain running parallel to Rutland Place shall be combined with street drainage swale so there are not two swales running parallel to one another.
- g. Backslope drains should not be necessary on top of the south berm (since there is no contributing drainage area to the top of this separation berm).
- h. Current plans call for filling up to 24' of fill at southwest corner of the Rutland Place and 5th Street rights of way where the natural ground currently falls off into the bayou. It does not seem appropriate to be filling this area when the project is generally intended to offset other losses of floodplain storage capacity. See sketch plan for recommended alternative geometry and grading, which conforms to existing ground within public rights of way.
- i. Was the option of leaving the existing north bank of the bayou in place considered?:
 - i. This would entail leaving the existing north bank of the bayou in place as the south detention basin berm;
 - ii. Closing off the berm along the southeast corner at the existing stormwater outfall,
 - iii. Grading the top of the berm to the correct elevation and adding the inlet weir structure;
 - iv. Closing off the berm along the southeast corner at the existing stormwater outfall;
 - v. This would significantly increase the amount of storage in the off-line portion of the basin and it would reduce the amount of grading required.
- j. North Basin by the Numbers (all approximate):

- i. Overall land area: 20.4 acres
 - ii. Storage: approx 300 acre/feet (to berm elevation)
 - iii. Bottom area: 5.7 acres
 - iv. Bottom feature wetted area: xx acres
 - v. Trails (inside ROW): xx lf
- k. Consider COH participation in enlargement of basin through acquisition of additional properties, as highlighted on the attached maps and designated A6-1. Part of this area was studied by TxDOT at an earlier date and designated A6; we are recommending that all of the highlighted area be acquired before the land is redeveloped.
- i. The COH SWMP website shows a planned 120" storm drain line replacing an older 84" line running down Yale Street to the bayou. This line could be diverted to the west at the MKT Trail (7th Street) and run into the north end of the detention basin, providing a good base flow for the wet bottom feature and at the same time the detention basin would provide detention to mitigate increases in discharge rates in the larger pipes and will provide important water quality benefits for the base flow.
 - ii. The City could participate in the project by acquiring undeveloped or abandoned industrial land on the east side of the basin (to the alley east of Allston) as well as the A6 property to provide for its share of the basin capacity.
 - iii. This could add about 90 acre/feet to the basin.

3. South Bank Basin (A5):

- a. Design A5 basin as if the A4-1 basin will be built at a future date.
- b. Remove west inflow weir to save cost and reduce amount of concrete visible from North Shepherd. This small basin will be adequately served by one weir.
- c. East Inflow weir to be designed to accommodate trail and maintenance crossing to meet ADA standards.
 - i. Solid concrete slope protection only in line with bottom of weir;
 - ii. Flanking areas to have open cell cable linked concrete matting with topsoil and grass cover;
 - iii. Minimize visible solid concrete, maximize use of grass-covered erosion control methods;
 - iv. Outfall headwall and pipe relocated to occur at base of inflow weir spillway (incoming flows will clear the outfall area);
 - v. Toe of spillway and outfall headwall to be designed to allow low-water pedestrian access.
- d. Backslope drain running parallel to Cornish Street and Patterson shall be combined with street drainage swale so there are not two swales running parallel to one another.
- e. Backslope drains should not be necessary on top of the north berm, nor along North Shepherd (since there is no contributing drainage area to the top of this separation berm).

- f. An accessible maintenance ramp shall be installed in the east end of the basin.
 - g. Did TxDOT consider buying some or all of the un-built lots (11) at the “Villas on Shepherd” development to increase the capacity of the project?
 - h. South Basin by the Numbers (all approximate):
 - i. Overall land area: xx acres
 - ii. Storage: approx xx acre/feet (to berm elevation)
 - iii. Bottom area: xx acres
 - iv. Bottom feature wetted area: xx acres
 - v. Trails (inside ROW): xx lf
 - i. Consider COH participation in enlargement of basin through donation of additional properties, as highlighted on the attached maps and designated A4-1. Part of this area was studied by TxDOT at an earlier date and designated A4; we are recommending that all of the highlighted area be used for detention.
 - i. The City of Houston SWMP shows planned improvements to a number of storm drain lines on the south side of I-10 that run into the I-10 storm drain system. This added capacity will mitigate the impact of those improvements;
 - ii. The enlarged basin and wet bottom will also allow for treatment of the stormwater running off the main lanes before it runs into White Oak Bayou.
4. Capital and Construction Partnering (for All the Basins):
- a. TxDOT will perform or construct the following as a part of the basic project:
 - i. Land acquisition, remediation, demolition, etc.
 - ii. Grading and earthwork related to creating the berms, the basins, the access ramps and the wet bottom ponds;
 - iii. Drain inlets and discharge piping and culverts;
 - iv. Vehicular access controls;
 - v. Grass establishment;
 - vi. Reforestation as shown on sketches.
 - b. The City of Houston will have need for mitigation detention as part of its SWMP in this part of the City and the community will be advocating for the City to participate in potential enlargements of these basins. This is not intended to delay the design or construction of the basins in any way and the design recommendations are made with possible future enlargements in mind. Most likely places for expansion are:
 - i. Property between Rutland and Rutland Place that would allow the A9/A10 basin to expand eastward and properties between the Bayou and Yale Street identified as “Danburg Estates” on HCAD maps which would allow the further enlargement of the A9/A10 basin eastward and connect to the TxDOT designated area A6. We recommend combining the A9/A10 basin to the A6 basin by acquisition of the “Danburg Estates” properties;
 - ii. The City of Houston Traffic Operations Patterson Street facility is located ideally to be an expansion of the off-line storage and was early identified as a qualified detention basin site (Basin A4). There have been discussions about the City wanting to move this facility to another location. A basin constructed on this land

could provide excellent water quality treatment to discharges from the main lanes and feeder roads of I-10 in addition to buffering the pumped discharges during a major event from the proposed I-10 pump station.

- c. TIRZ #5 recently extended its boundaries to run up White Oak Bayou and is a likely partner for recreational improvements to the basins. A TIRZ can only fund capital projects and cannot take on maintenance obligations since TIRZ's all have sunset dates when they cease to exist. Features that could be constructed or installed by TIRZ #5 include:
 - i. Trail surfaces;
 - ii. Supplemental special tree and wetlands plantings;
 - iii. Benches;
 - iv. Signage.
5. Long Term Maintenance Partnering:
- a. TxDOT: will own and operate the storm drain lines under its roadways and the pump station that serves the depressed freeway section.
 - b. HCFCD: It is our understanding that TxDOT has asked the HCFCD to pick up the long term maintenance of the detention basins and mitigation areas along the main White Oak Bayou channel. This would involve mowing several times per year and maintenance of the backslope drains and other pipes and culverts.
 - c. HPARD: The City of Houston Parks and Recreation Department maintains trails in many places in the City, including the White Oak Bayou and MKT Trails, and would be the logical entity to adopt and maintain any permanent trail improvements constructed in and around the basins. HPARD might also be responsible for maintaining benches and trash receptacles (if any). HPARD usually supplements the HCFCD mowing cycles with more frequent mowing along the edges of the trail systems where trails are located on HCFCD right of way.
 - d. Heights Association (or other community organization): might formally or informally adopt one or both detention basins. Informal adoption might mean that members of the association would provide 'eyes on the trail' for security and maintenance issues, and might provide volunteer labor for special tree or wildflower plantings, trash pickup or other special events. Formal adoption might mean assuming responsibility for special vegetation management of the reforestation areas (such as invasive plant control) and for supplemental mowing, within the basins themselves, above and beyond the infrequent HCFCD mowing.

Notes and Sketches prepared on behalf of the White Oak Bayou Association

By:

Kevin Shanley, Chairman of the Board, The Bayou Preservation Association

713-869-9906 cell

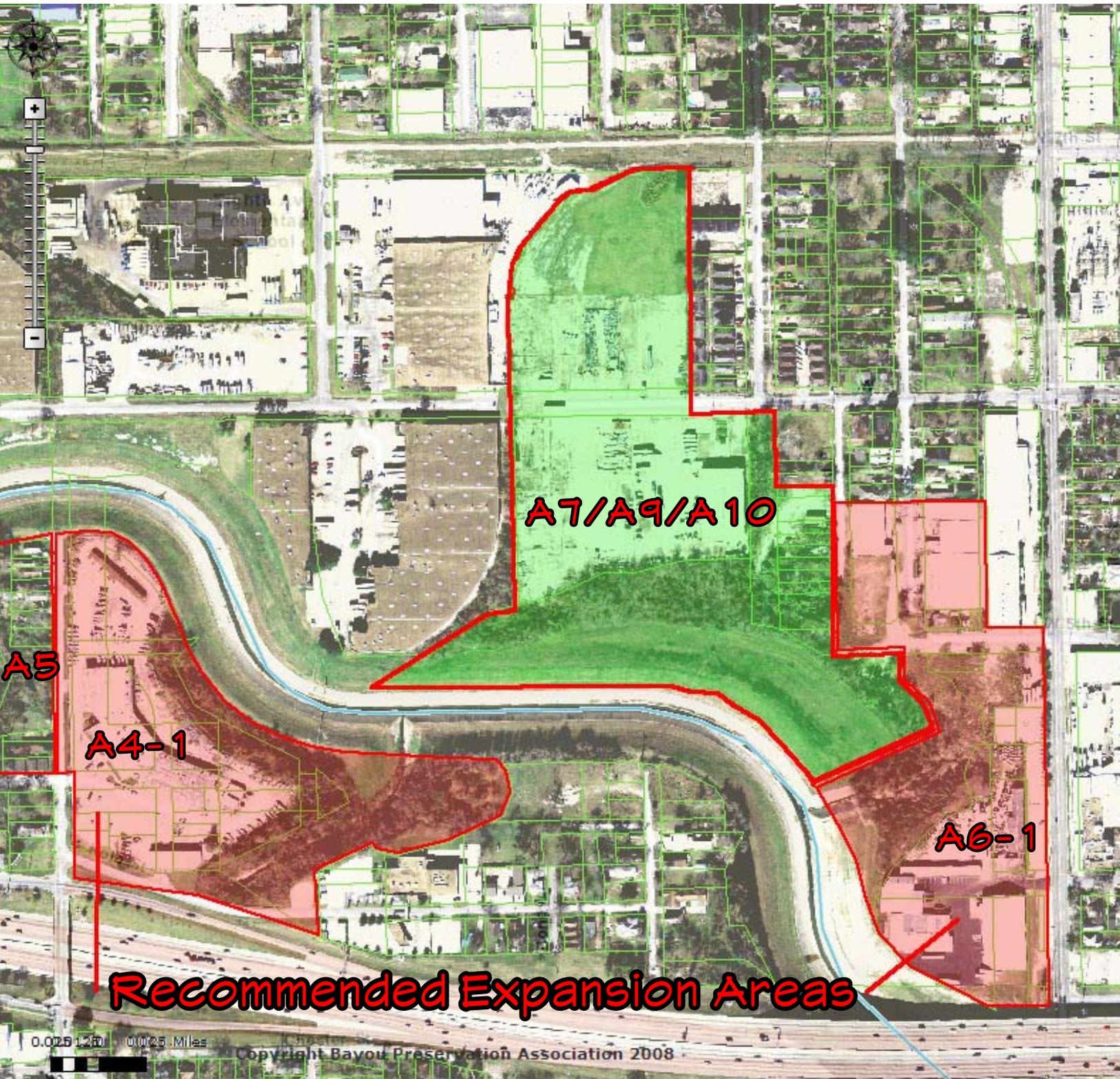
kshanley@swagroup.com



**TxDOT
A5**

**COH
A4-1**

Recommended Expansion Area



A7/A9/A10

A5

A4-1

A6-1

Recommended Expansion Areas

0.025 Miles

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**Public Hearing
IH 10 at White Oak Bayou Detention Basins
Houston, Texas**

COMMENT FORM (FORMA DE COMENTARIO)

February 18, 2010

Thank you for attending the IH 10 at White Oak Bayou Detention Basins Public Hearing. Your comments are important and will be taken into consideration regarding the proposed project.

Gracias por asistir la Audiencia Pública de IH 10 y White Oak Bayou para Palanganas de Detención. Sus comentarios son importantes y serán tomados en consideración con respecto al proyecto propuesto.

Name (Nombre): MAT WILCOX Phone (Teléfono): 713-869-8412

Address (Dirección): 1515 W. 21ST, HOUSTON, TX 77008

PLEASE COMPLETE THE APPROPRIATE ITEMS BELOW:
POR FAVOR COMPLETE LOS ARTICULOS APROPIADOS ABAJO:

1. How did you hear about this hearing? / Como se informo de esta audiencia?

- | | |
|---|---|
| <input type="checkbox"/> Newspaper / Periodico | <input type="checkbox"/> Received Notice by Mail / Aviso por Correo |
| <input checked="" type="checkbox"/> Told by Friend or Neighbor / Amigo o vecino | <input type="checkbox"/> Other (please explain) / Otro (por favor explique) |

2. I am primarily interested in the project from the standpoint of a:
Estoy interesado principalmente en el proyecto del punto de vista de:

- | | |
|--|--|
| <input checked="" type="checkbox"/> Residential property owner or renter
Dueño de propiedad residencial o inquilino | <input type="checkbox"/> Highway User
Usuario de Carretera |
| <input type="checkbox"/> Business property owner or lessee
Dueño de propiedad comercial o inquilino | <input type="checkbox"/> Other (please explain)
Otro (por favor explique) |

COMMENTS (COMENTARIOS):

SIMPLY:
DETENTION GOOD
LOW BRIDGES BAD
RAISE OR OMIT THE BRIDGES AND START
THE WORK ON THE BASINS AND ROAD

CSJ: 0271-07-292

**PLEASE RETURN THIS FORM DURING THE MEETING OR
SUBMIT BY MAIL BY March 4, 2010**

**POR FAVOR DEVUELVA ESTA FORMA DURANTE LA REUNIÓN O
ENVIAR POR CORREO EL 4 DE MARZO DE 2010**

: TXDOT
DIRECTOR OF PROJECT
DEVELOPMENT
P.O. BOX 1386
HOUSTON, TX
77251

6002 De Lange
Houston, Texas 77092

March 2, 2010

Texas Department of Transportation
Director of Project Development
P.O. Box 1386
Houston, Texas 77251-1386



Dear sir:

RE: TxDOT Public Hearing – IH-10 at White Oak Bayou Detention Basins

As an IH-10 user and White Oak Bayou (WOB) watershed resident, I welcome and appreciate the opportunity to once again offer comments regarding the proposed TxDOT IH-10 Detention Pond Project – CSJ 0271-07-292. I support, in concept, the construction of the detention basins, but given the need to spend federal Stimulus Funding dollars on The IH-10 Feeder Road/Ramp Project – CSJ:0271-07-242, I feel that TxDOT is moving forward with both the road and detention projects without full coordination with the community and local government entities. While TxDOT must move quickly, updated coordinated designs and planning would result in improved local traffic flow, increased traffic safety, reduced flood risks, an overall improvement in quality of life and over the long haul, substantial savings in taxpayer dollars. As such, I join with my fellow citizens in requesting TxDOT act on the following points related to the basins and associated flood risks:

- I urge TxDOT to finalize required environmental studies, secure funding and proceed quickly with the acquisition of the detention basin sites. At the Feb. 18th Public Hearing, it was disappointing to hear a property owner from inside the project confines who had yet to be contacted by TxDOT.
- I strongly request that all detention and mitigation measures be constructed concurrently if not before the feeder road construction that will impact WOB and the drainage of the below grade section of I-10.
- TxDOT to address and resolve the hazardous conditions created by having sections of the feeder roads and bridges constructed well below the Base Flood Elevation (BFE). Specifically, there is concern the new eastbound feeder road and Yale/Heights exit ramp will take exiting drivers from the freeway over a rise and then drop them on the bayou feeder road bridge at Yale several feet below the BFE.
- The detention basins should be designed and built in such a way as to support the future development of the sites as parks complete with “wet bottoms,” native plantings and trails. The White Oak Bayou Assoc. renditions for such a design are a good starting point for this discussion.

In order to make more fully informed comments and to gain a broader understanding of potential and possible flood risks related to both the Detention Pond Project – CSJ: 0271-07-292 and the Road Project – CSJ: 0271-07-242, I reviewed TxDOT's 2/09 Drainage Mitigation Study for the I-10 Project, the 2/10 detention pond renditions, the road construction plan sets and the Federal Highway Administration (FHWA) Policy Guide 23.650 pertaining to encroachments in the floodplain. Upon considering the following comments, questions and recommendations; my hope is TxDOT will see the need to revisit the drainage study and make modifications to their plans as they relate to detention, bridge design and mitigation of flows on lower WOB. I make the following points acknowledging that TXDOT is not a flood control/reduction agency and that several of the below recommendations will have to be carried out in partnership with others.

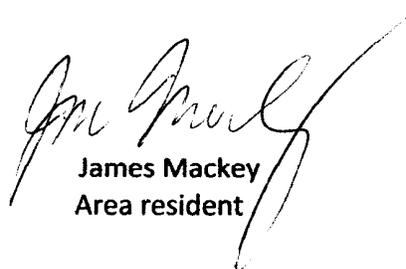
- Regarding TxDOT's 2/09 Drainage Study, the road construction plan sets and specifically the proposed bridge elevations, I find it difficult to understand how TxDOT is compliant with FHWA Floodplain Policy Guide. From the outset in **Sec. 650.103 Policy** - there are statements that read, "... a broad and unified approach to prevent uneconomic, hazardous or incompatible use of the Nation's floodplains...Too avoid significant encroachments, where practicable...To restore and preserve the natural and beneficial floodplain values that are adversely impacted by highway agency actions." Another key point is made in **Sec. 650.115 Design Standards** - stating that, "The design selected for an encroachment shall be supported by analyses of design alternatives with consideration given to capital costs and risks, and to other economic, engineering, social and environmental concerns...The design of encroachments shall be consistent with standards established by the FEMA, State and local governmental agencies..." As a private citizen, I find it difficult to fathom a number of TxDOT's design decisions after reading the FHWA words. Given the "social concerns" alone over flooding that is part of the culture of the WOB citizenry, TxDOT needed to avoid designs placing the bridges low in the floodplain.
- While utilizing best practices and technically sound, TxDOT's 2/09 Drainage Study is flawed by the fact that TxDOT didn't consider running simulations with the proposed new and replacement bayou feeder road bridges raised to feasible and practical levels approaching or above the BFE. If run, such modeling simulations would have given TxDOT and the community a solid indication if additional mitigation measures would be needed to offset any additional flows passing under the raised bridges. **Sec. 650.113** of the FHWA Policy Guide states that alternatives must be considered and analyzed. Further, the FHWA states in **Sec. 650.109** that the public should be afforded the opportunity to comment on alternatives.
- I must ask TxDOT to more fully explain how they calculated the capacities of the proposed detention pond facilities. My concern is TxDOT may be unknowingly claiming existing stormwater storage capacity as their own at site A-5. The table at the bottom of page 5-1 in the Drainage Mitigation Study makes no mention of the existing 25ac/ft of stormwater storage at that location. See attached images.
- I must encourage TxDOT to secure an agreement with the City Of Houston (COH) to abandon Patterson St north of Cornish and to include that street ROW in an expansion of the A-5 Detention Site. In addition to the benefit of increased detention, removing the Patterson bayou deadend reduces the likelihood this area will become an illegal dumping ground as so often occurs.

Additionally, I suggest TxDOT and/or the COH purchase the undeveloped townhome lots along the north side of Cornish for further expansion of the A-5 site.

- In conjunction with proposals for “park ready” wet bottom detention sites, I encourage TxDOT and the COH to coordinate on the A9-10 design and construction. If city drainage were to be rerouted to provide semi-permanent flows into the site, additional detention capacity may be required. The adjoining city owned MKT ROW south of the hike-n-bike trail could be sculpted to provide additional stormwater storage.
- There is more than 200’ of open land between Yale and the bayou on the westbound feeder ROW that could be utilized to raise the approach for the westbound bayou feeder road bridge at Yale. By bringing the elevation of the approach up, the bridge itself could be raised 2-3’.
- Yale St at I-10 and the nearby Yale bayou bridge lay at 42’ which is 2-7’ below the BFE at this location. The lower chord of the planned eastbound bayou feeder road bridge at this location is at 37’, again several feet below the BFE. We encourage TxDOT, the COH and Harris County Flood Control District (HCFCD) to coordinate and address flood risks at this location. If TxDOT proceeds as planned with the eastbound feeder connection to Yale, they will make it extremely difficult and expensive in the future to raise the grade at Yale and/or replace the 80 year old Yale bayou bridge at a higher elevation. Additionally, TxDOT’s feeder roads and bridges will pull increased traffic into this high risk flood area. This is a public safety concern as noted earlier in this letter.
- I ask TxDOT to study the impacts of raising the replacement bayou feeder road bridges just west of Studemont to an elevation above the current low lying flow obstructing bridges. While probably not practical from a traffic engineering perspective to raise the new bridges above the BFE, it may be feasible to raise the bridges 1-2’, especially in the case of the westbound bridge where there appears to be adequate room west of the Studemont intersection to raise the approaches for the westbound bridge. I realize that raising the bridges at this location may require additional mitigation measures. Again, I encourage TxDOT to coordinate with the COH and HCFCD in developing long lasting cost effective flood reduction strategies.

I appreciated the opportunity to once again offer comments and thoughts regarding the proposed TxDOT I-10 Detention Pond Project at the Public Hearing. I join with my fellow citizens in urging TxDOT to truly consider our input and to refine their plans for the benefit of their department, IH-10 drivers and the community as a whole. The Public Hearing was another step in working towards that goal as we move forward on formulating a project we all can live with and be proud of.

Sincerely,



James Mackey
Area resident

WASHINGTON AVENUE COALITION - MEMORIAL PARK

March 4, 2010

Re: IH 10- at White Oak Bayou Detention Basins, Houston, Texas

Dear Mr. Henry,

On behalf of the Washington Avenue Coalition/Memorial Park Super Neighborhood (SN 22) community, I urge you and your agency, please, to ensure (1) that the IH 10 at White Oak Bayou Detention Basins are constructed as "park-ready," multi-purpose assets built prior to or contemporaneously with the construction of the IH 10 frontage road projects, and (2) that any and all bridges built as part of either the IH 10 frontage road project or the detention basins project be designed and built above the base flood elevation.

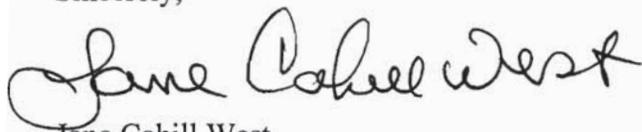
Because, together, these two projects, i.e., the frontage road project and the detention basins project, are intended to displace storm water holding capacity of the main lanes of IH 10 between the IH 610 loop on the west and IH 45 on the east, property owners (residents and businesses, alike) in the adjacent SN 22 neighborhoods are likely to be severely and adversely impacted if the detention basins are not constructed and operational **BEFORE** the frontage road construction is completed and operational.

In addition, SN 22 joins in and supports the requests already made by other community organizations (e.g., White Oak Bayou Association, Citizens Transportation Coalition, Bayou Preservation Alliance, and Greater Heights Super Neighborhood (SN 15)), urging you and your agency to work closely and cooperatively with the surrounding communities and with other effected governmental entities including, especially, the City of Houston and the Harris County Flood Control District, to ensure that the detention basins are designed and built in such a way that they can be readily up-graded for use as public park and green space akin to the Willow Waterhole in southwest Houston.

The SN 22 Council, along with its constituent neighborhoods and businesses, looks forward to developing a close working relationship with you and your agency to achieve the goals outlined above with respect to the IH-10 at White Oak Bayou Detention Basins.

Thank you for your time and attention to this matter of great community concern.

Sincerely,



Jane Cahill West
President, 832-721-9080

Cc: Congress Member Sheila Jackson Lee, State Senators John Whitmire and Mario Gallegos, State Representative Jessica Farrar, Harris County Judge Ed Emmett, Harris County Commissioners Jerry Eversole and Sylvia Garcia, City of Houston Mayor Annise Parker, City of Houston Council Members Stephen Costello, Brenda Stardig, and Edward Gonzalez.

P. O. BOX 70886 HOUSTON, TEXAS 77270

Emailed IH 10 at White Oak Bayou Detention Basins Public Hearing Comments

From: "anne culotta" <anneculotta@usa.net>
To: "Mayor Annise Parker" <mayor@cityofhouston.net>
CC: <districth@cityofhouston.net>, <atlarge1@cityofhouston.net>, <atlarge2@cityofhouston.net>, <Minnette.Boesel@cityofhouston.net>, <HOU-PIOWebmail@DOT.state.tx.us>, <JSmulian@wrtdesign.com>, <bikin7@aol.com>, <rholzer@ctchouston.org>, "Mark R. Williamson" <markwmsn@markwmsn.com>

Date: 2/25/2010 10:45 PM

Subject: Letter from the President of the Houston Heights Association re TxDotProjects

Re: TxDot's current I-10 frontage road and flood detention projects on White Oak Bayou

Dear Mayor Parker,

I attach for your attention a resolution passed by the Houston Heights Association and presented last week at a statutory public hearing organized by TxDOT at Reagan High in our neighborhood. The meeting was well attended by local citizens and included two Council members, State and Federal representatives or their aides and a number of groups, such as the White Oak Bayou Association and the Citizens Transportation Coalition.

A number of those attending spoke for the record. All supported and requested greater participation in major infrastructure projects affecting both our city and our neighborhood, and much closer and effective coordination between the TxDOT and the City. With respect to the project under discussion, such coordination clearly has been lacking, and there are reasons to question whether the plans approved in 2004 (without effective community input, at least in the Heights) are a constructive addition to our neighborhood.

In order to lay claim to federal stimulus dollars, TxDOT has effectively attempted an end-run around the communities affected, declaring the project "let and underway". In the face of these circumstances, our concern for the future of our neighborhood has led the Heights Association to take a position on this matter.

Among other issues :

We are concerned that the planned Yale Street on/off ramps will engender unacceptable additional cross-town traffic, flowing through this historic neighborhood. The most recent traffic analysis carried out by the City does not take into account the addition of these ramps and is predicated on Yale being a divided four-lane thoroughfare. The City's current rehabilitation project for Yale, for which we had your support, does not envision Yale as a four-lane divided road and there is no doubt that the ramps will affect traffic.

We understand that new bridges being constructed over White Oak Bayou are well below the Base Flood Elevation although a City code expressly forbids this. We are concerned that these bridges will be a future hazard in times of flood and may well have to be reconstructed in the foreseeable future at great

expense. We also do not understand why the State (TxDOT) is allowed to provide lower standards than those mandated by the City.

The contract to build I-10 frontage road and its bridges and ramps from Yale Street has been let and “stimulus funds” have been made available for this “shovel ready” project, but the detention ponds (which TxDOT has acknowledged are a critical element in mitigating the effect of flooding associated with the road project), have not been funded at this time. In fact, none of the property needed for those ponds has yet been acquired. We are informed by TxDOT that when an Environmental Impact Statement for one of the ponds is completed and approved, TxDOT will be able to commence the purchase of properties required for the ponds. It appears there is no guarantee of concurrency in building both these projects and we are concerned that without the ponds (even for a short period of time) water, which in the past has flooded I-10, would back up, seriously affecting adjacent areas in the Heights.

In short we feel that these projects , conceived over 10 years ago and approved 7 years ago, do not reflect the best possible engineering design, do not satisfactorily address our neighborhood’s concerns and may well be detrimental to the future of what has been described by a previous Mayor as “one of the city’s greatest gems and assets”.

We, as citizens, respectfully request the City’s timely assistance in working with us, coordinating with TxDOT and seeking to find the most cost and functionally effective solutions that will be of maximum benefit to all affected parties.

Sincerely,

Anne O. Culotta
President, Houston Heights Association

Cc:

Council Member Ed Gonzalez

Council Member Stephen Costello

Council Member Sue Lovell

Minnette Boesel, Mayor’s Office

Pat Henry, Director of Project Development TxDot Houston District Jonathan Smulian, Chairman HHA

Land Use Committee Jim Mackey, White Oak Bayou Assoc.

Robin Holzer, Citizens Transportation Coalition Mark Williamson, President Greater Heights Super Neighborhood Council

From: "Jonathan Smulian" <JSmulian@wrtdesign.com>
To: <HOU-PIOWEBMAIL@DOT.STATE.TX.US>
Date: 3/4/2010 7:43 AM
Subject: TxDOT I-10 Drainage/ detention project

Comment:

Land has not been purchased or funding allocated for the detention ponds. No work on the I -10 frontage roads, ramps to Yale Street or new bridges across White Oak Bayou should commence until the construction of the detention ponds is assured. TxDOT has informed us that without the detention ponds the Houston Heights neighborhood will be vulnerable to serious flooding. The public needs to participate in the design of the detention ponds if they are going to be an asset to the neighborhood. TxDOT has only complied with statutory public information requirements and not in any serious public participation. This is unacceptable!!!

Jonathan Smulian
1231 Arlington Street
Houston Tx 77008

From: Mike and Elisa Goodwin <goodwins2005@yahoo.com>
To: <HOU-PIOWEBMAIL@DOT.STATE.TX.US>
Date: 3/4/2010 8:35 AM
Subject: TxDOT I-10 Detention/Drainage Project

Re: TxDOT I-10 Detention/Drainage Project

My husband and I live on the 700 block of Rutland, an area which could easily be compromised by the new Katy Freeway access roads, if the detention/drainage issues are postponed or neglected. I'd like to make two points:

- 1) We feel very strongly that TxDOT has a responsibility to address drainage issues in a timely fashion. The detention pond project should be undertaken at the same time as the access road. Waiting a year, or two, or until funds are available, will endanger our property.
- 2) We support the White Oak Bayou Association and their efforts to ameliorate the detention ponds, so that they become an asset to our community. As well as being very important functionally, the detention pond areas have the potential to become a focal point for wildlife, a link in the greenspace chain leading towards downtown, and a restful, attractive place for Heights residents to walk, bike, picnic, or birdwatch. We ask that TxDOT take the extra steps necessary to make the detention pond areas park-ready.

Thank you,
Elisa Goodwin
Houston Heights Association member
White Oak Bayou Association member
718 Rutland, Houston, TX 77007
713-861-6151

From: <valhatley@yahoo.com>
To: <HOU-PIOWEBMAIL@dot.state.tx.us>
Date: 3/7/2010 2:27 PM
Subject: TxDOT Internet E-Mail

Name: Val K. Hatley<valhatley@yahoo.com>
Address:
2368 Rue Bienville Way
Henderson, Nevada 89044
702-449-0273

Comment: Greetings,

RE: Property at 4136 Bonner View Lane, Houston, Texas, 77007

I am currently living in Nevada and selling my home in Texas. I am seeking a summary report from the Feb 18th public hearing on the I-10 White Oak Bayou project. Specifically, I need to know whether my home is impacted by the outcome before I enter into escrow.

Please let me know where I can find the summary report from the hearing.
Thank you.
Val K. Hatley
valhatley@yahoo.com

RESPONSE TO PUBLIC COMMENTS

No.	Name	Comment	Response
Oral Comments			
1.	Ed Gonzalez, Houston City Council Member, District H	<p>Good evening. My name is Council Member Ed Gonzalez; and I represent District H, which covers the location where the detention basins would be placed.</p> <p>And I just want to make sure that TxDOT communicates with the community and updates them and provides an avenue to where they can voice their concerns beyond just this meeting, perhaps, if we need additional, you know, follow-up meetings and be willing to host them.</p> <p>And also, if we could make sure that we're trying to include within the detention basins different features that, perhaps, the community could use as well, that, you know, maybe some trails or at least create the basins to where these features could be added at some point at least to where the City could perhaps participate at some point. So I just want to comment, listen to the comments and hopefully be able to follow up after that. Thank you.</p>	<p>A Public Hearing Summary Report will be prepared and available for public review on TxDOT's website, www.txdot.gov. Attendees of tonight's meeting, as well as property owners within or adjacent to the proposed project, will receive notice with the report is available. At any time, comments or questions regarding the proposed project can be sent to HOU-PIOWEBMAIL@DOT.STATE.TX.US or by calling Dr. Stanley Cooper at 713-802-5244. Since the public hearing, TxDOT has continued to meet with local agencies and officials to discuss the proposed project.</p> <p>TxDOT is currently in the process of working with local agencies to determine construction and design methods which will allow for future recreational uses of the basins. TxDOT funding cannot be used to construct the parks; however, TxDOT is open to allow for the future development of parks and/or recreational use of the detention basins. The final design is still pending based on these discussions with local agencies.</p>
2.	Jessica Farrar, State Representative, District 148	<p>Really I want to enter into the record what resulted from our meeting or what was discussed at our meeting with you last Friday -- well, with Jim Koch and Pat Henry and I appreciate the opportunity that they gave us to present an idea to them; and it was a meeting with Kevin Shanley from the White Oak Bayou Association.</p>	

	<p>So I don't know if he's going to be presenting it or not, but I wanted to make sure it gets into the record. And the request that we made was that in the design work of these projects that the -- especially the earthwork that's done, it be left in a condition to where a future project, whether it's a public or a private entity, could come back and make a -- different ideas have been discussed: Dog parks, trails, connections to bike trails that are already planned and so on.</p> <p>But basically the idea is that the costs to the state agency in terms of earthwork is minimal to the needs of a future park; but if a group comes in subsequently to build that park, the cost for earthwork would be substantial. So we're just asking if you would, so to speak, leave the beds ready; and we'll come back and plant the flowers later.</p> <p>So that is my request, is that the agency work with the groups that are interested and it's -- Council Member Gonzalez has asked the Houston Parks Department be involved. It's questionable at this moment whether there's funds to do so, but there are private groups that are interested as well. But the interest is there and that's what I would like to express is that if you could help these groups in your -- and the work that is being done in the near future to the work that these groups can do later on in terms of making these spaces, expanding the green space that we know that we're in such need of in the Houston area. So that's what I would like to contribute, and I look forward to working with you more on this. Thank you.</p>	<p>TxDOT is currently in the process of working with local agencies to determine construction and design methods which will allow for future recreational uses of the basins. TxDOT funding cannot be used to construct the parks; however, TxDOT is open to allow for the future development of parks and/or recreational use of the detention basins and is willing to design the basins in a manner that will allow for future recreation. The final design is still pending based on these discussions with local agencies.</p> <p>Since the public hearing, meetings have continued with the White Oak Bayou Association and local officials to discuss the project. The proposed project will result in additional green space.</p>
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<p>3.</p>	<p>Michael Halpin for Sheila Jackson Lee, U.S. Congresswoman, 18th District, Texas</p>	<p>Michael Halpin with the office of Congresswoman Jackson Lee, who asked me to bring a letter tonight that I'll leave with you, and I'll just read a portion of it.</p> <p>"I joined some very concerned neighbors this past weekend on the banks of White Oak Bayou. I was impressed by the cohesiveness of the neighbors and of the massive greenbelt of which our bayous are part. However, I was disappointed at the disregard of the greenbelt and lack of development.</p> <p>"Let us agree on one thing tonight. We will all work together to make White Oak Bayou and the detention ponds into park space and complimentary hike and bike trails. Let's work together to connect our bikeways and make them better.</p> <p>"I strongly support recommendations by White Oak Bayou Association and Greater Heights Super Neighborhood Council encouraging TxDOT to include provisions for park features to be added in the areas identified as detention ponds for mitigation on the I-10 frontage road project.</p> <p>"The cost to acquire the land alone will provide multiple benefits to surrounding communities by TxDOT working with the Flood Control District and the City of Houston. We encourage a collaborative effort for the inclusion of the park option. We want to encourage, wherever possible, the pursuit of shared goals and effective communication between the different representative bodies. We look forward to working with TxDOT to deliver this valuable benefit to the community.</p>	<p>Comment noted.</p> <p>TxDOT is currently in the process of working with local agencies to determine construction and design methods which will allow for future recreational uses of the basins. TxDOT funding cannot be used to construct the parks; however, TxDOT is open to allow for the future development of parks and/or recreational use of the detention basins. The final design is still pending based on these discussions with local agencies.</p> <p>Comment noted.</p> <p>Since the public hearing, meetings have continued with the White Oak Bayou Association and local officials to discuss the proposed project and how to design the basins to allow for future recreation development.</p>
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		<p>"Thank you for your consideration.</p> <p>Sincerely, Sheila Jackson Lee, member of Congress."</p>	
4.	Dwayne Bohac, State Representative, District 138	<p>Good evening. My name is Representative Bohac, and I represent Houston and West Houston in the Texas legislature. I've also been a member of the White Oak Bayou Association for more years than I can count. I know they talked with you-all last week, and there's some plans that they put on the table. I just wanted to let you-all know that I strongly support their plans. I would like for you-all to take them very seriously and look at those plans and adopting them on a go-forward basis.</p>	<p>Comment noted.</p> <p>TxDOT is currently in the process of working with the White Oak Bayou Association to determine construction and design methods which will allow for future recreational uses of the basins. TxDOT funding cannot be used to construct the parks; however, TxDOT is open to allow for the future development of parks and/or recreational use of the detention basins. The final design is still pending based on these discussions.</p>
5.	Anne Culotta, President, Houston Heights Association	<p>Because of this mission to preserve and enhance the quality of life in the Heights, the Houston Heights Association is opposed in principle to road construction or other infrastructure projects that could generate increased congestion or risk of flooding in our historic neighborhood.</p> <p>In the case of the proposed TxDOT project to construct access ramps at Yale Street to and from Interstate 10, based on information brought before us, we consider that the project may, one, lead to a significant increase in traffic volumes for through traffic between the 610 Loop and Interstate 10 that does not have its origin or destination within the Heights, resulting in an increase in traffic congestion in our historic neighborhood and, two, as currently planned, we consider that the project may pose a risk of increased flooding in our historic neighborhood and, three, the Houston Heights Association specifically requests that TxDOT apply its new policy of context sensitive design for this project and coordinate with the City of Houston, the Harris County Flood Control District and</p>	<p>Comment noted.</p> <p>The IH 10 frontage roads project was not the subject of the public hearing. The IH 10 frontage roads were included as part of IH 10 roadway widening project. An Environmental Impact Statement was prepared and approved by FHWA for this project. In 2009 and 2010, neighborhood and civic association meetings were held to discuss the frontage road project. Modifications were made to the design based on public comments from these meetings.</p> <p>The detention basins project is proposed to mitigate floodplain impacts from the frontage road project.</p> <p>TxDOT has coordinated with the City of Houston and the</p>

		<p>the community. And the Houston Heights Association requests that all floodwater detention and mitigation issues be addressed and implemented concurrently with road construction.</p> <p>The Houston Heights Association welcomes the opportunity to work with TxDOT and our elected officials to address these issues. However, without further study and any necessary actions to alleviate these community concerns, the Heights Association is opposed to the current execution of the project.</p>	<p>HCFCFCD regarding the proposed project. Both entities have reviewed the report and drainage study and stated no objections to the findings.</p> <p>The detention basins will be constructed concurrently with the frontage road project. The frontage road project construction schedule is longer than the detention basin construction schedule and therefore, starting first. IH 10 will continue to serve as floodplain storage until the detention pond construction is complete.</p> <p>Coordination with local groups has continued since the public hearing. Comments or questions regarding the proposed project can be sent to HOU-PIOWEBMAIL@DOT.STATE.TX.US or by calling Dr. Stanley Cooper at 713-802-5244.</p>
<p>6.</p>	<p>Tom Dornbusch, Super neighborhood 22 Council VP, Board Member, White Oak Bayou Association, Citizen's Transportation Coalition</p>	<p>My name is Tom Dornbusch. I live four blocks from Interstate 10, so I'm aware of the flooding issues there. I am the vice-president of Super Neighborhood 22, Washington Avenue/Memorial Park Super Neighborhood, which I-10 moves through.</p> <p>And as much as I feel compelled to use my time to address concerns over TxDOT's lack of community involvement with regard to the IH 10 frontage lanes project that necessitates these detention ponds; given the seven-year lag between the only public hearing, which was held outside the area of immediate impact; the hundreds, possibly thousands, of new residents new to the impact area since that hearing who have had no opportunity to comment on that project; TxDOT's recent severe reprimanding from its Sunset Review Commission for lack of transparency and the disingenuousness of agreeing to a formal meeting about the project, then</p>	<p>The IH 10 frontage roads project was not the subject of the public hearing. The IH 10 frontage roads were included as part of IH 10 roadway widening project. An Environmental Impact Statement was prepared and approved by FHWA for this project. In 2009 and 2010, neighborhood and civic association meetings were held to discuss the frontage road project. Modifications were made to the design based on public comments from these meetings. The public hearing was for the detention basins project, which is planned for construction in Fall 2010.</p>

		<p>announcing at that meeting the letting of construction contracts earlier in the same day, after having, a few weeks earlier -- after having, a few weeks earlier, touted the adoption of and commitment to context sensitive solutions -- well, I don't need to address that.</p> <p>Instead, I'll thank you for scheduling this additional hearing for the detention facilities project and I urge you to exercise that stated commitment to context sensitive solutions by taking the steps necessary to assure residents of the area, who are concerned about these projects' impact on their property, that this detention will be provided concurrently with the roadway construction so that there will be no opportunity for neighborhood flooding resulting from that construction before detention is provided and that you will act on the suggestions presented and assist with making these facilities multipurpose community assets rather than fenced off pits.</p>	<p>The detention basins will be constructed concurrently with the frontage road project. The frontage road project construction schedule is longer than the detention basin construction schedule and therefore, starting first. IH 10 will continue to serve as floodplain storage until the detention pond construction is complete.</p> <p>TxDOT is currently in the process of working with local agencies to determine construction and design methods which will allow for future recreational uses of the basins. TxDOT funding cannot be used to construct the parks; however, TxDOT is open to allow for the future development of parks and/or recreational use of the detention basins. The final design is still pending based on these discussions with local agencies. There will be no fencing around the detention basins.</p>
<p>7.</p>	<p>Jim Mackey, Vice President, White Oak Bayou Association</p>	<p>I'm Jim Mackey, vice-president of the White Oak Bayou Association; and our organization goes by the acronym WOBA. We join with the other community groups in the Heights area on the following points: We believe that the detention ponds should be designed and built in such a way to support the future development of the site's parks, complete with wet bottoms, native plants and trails.</p>	<p>Since the public hearing, TxDOT has met with the White Oak Bayou Association and local officials to determine construction and design methods which will allow for future recreational uses of the basins. TxDOT funding cannot be used to construct the parks; however, TxDOT is open to allow for the future development of parks and/or recreational use of the detention basins. The final design is still pending based on these discussions. However, landscaping would be included as part of the proposed project and will include seeding and replanting the right-of-way with native species of grasses, shrubs, or trees where applicable. The access ramp to the basins for maintenance</p>

		<p>Additionally, we urge TxDOT to secure funding and proceed quickly with the acquisition of the detention pond sites. On this point we fully support TxDOT's efforts and offer to advocate for funding.</p> <p>It's somewhat of a disappointment this evening that in the formal presentation we didn't hear more about the proposed funding of the acquisition of the detention sites.</p> <p>Additionally, we strongly request, as did some of the other speakers already, that all the detention and mitigation measures be constructed concurrently, if not before the feeder road construction, that will impact White Oak Bayou and the drainage of the below-grade section of I-10.</p> <p>Our organization took the time to review TxDOT's drainage study, the detention pond renditions and the road plans; and we have several questions in that regard.</p> <p>We still would like to meet with TxDOT and discuss the detention pond capacities. We feel there is a little bit of discrepancy between our numbers and TxDOT's, that we may be able to resolve that; but we would like to meet on that matter.</p> <p>While technically sound and the drainage study used what are considered best practices, we're somewhat troubled in the study in that TxDOT didn't run any simulations that showed the bridges, the feeder road bridges at different levels. We would have liked to have seen a simulation run with the ponds -- or with the bridges, excuse me, raised to see if any additional</p>	<p>will be designed as a natural ramp, suitable for future trails and access to the ponds.</p> <p>Funding has been secured for the proposed project.</p> <p>Comment noted.</p> <p>The detention basins will be constructed concurrently with the frontage road project. The frontage road project construction schedule is longer than the detention basin construction schedule and therefore, starting first. IH 10 will continue to serve as floodplain storage until the detention pond construction is complete.</p> <p>Since the public hearing, meetings were held with the White Oak Bayou Association and local officials to further discuss the project. Coordination with this group will continue through final project development.</p> <p>The proposed bridges were designed to avoid any increase in the Base Flood Elevation, provide the grades necessary for safe tie-ins to existing roadways, and are above the 10-year floodplain. These design features are consistent with standard TxDOT Houston District procedures. The frontage road project has been approved by FHWA and additional</p>
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		<p>mitigation may be required. We think that we would encourage TxDOT to undertake that study.</p> <p>And finally, some concern -- as time is running out here -- we're very concerned about the flooding and drainage situation of Yale at I-10 because the bridges at that location are a problem.</p>	<p>studies are not proposed at this time.</p> <p>The bridges at Yale and White Oak Bayou were designed with the thinnest beam available to meet the span and skew requirements to minimize impacts to the floodplain. Any impacts to White Oak Bayou from the bridges at Yale Street and IH 10 will be mitigated by removal of the timber approach spans of the abandoned railroad trestle bridge over White Oak Bayou, upstream of Yale Street.</p>
<p>8.</p>	<p>Ken Culotta</p>	<p>I'm a resident of the Heights for over 20 years. I want to express my appreciation, first, to TxDOT. You-all have fielded a lot of citizens' concerns, sometimes very passionately expressed and often, I think, outside of your authority to really change things and put in motion. For that reason my comments are really directed to the elected officials present tonight as much as they are to TxDOT.</p> <p>We have been told that this is a meeting about detention ponds and flood control and indeed to the extent that we are only talking about flood control, and we're talking about flood control for this neighborhood, I am sure that compromises can be arrived at. And, in fact, I'm grateful for some of the positive suggestions that have already been made tonight.</p> <p>But integral to the flood control project is a roadway project, that based on information that I and others have reviewed, could make the detention ponds less effective and, perhaps, from the neighborhood perspective, ineffective or worse in controlling of floods in our neighborhood, not to mention other issues to which this roadway work may give rise to in the neighborhood, which I won't go into here tonight because that is not the subject of this meeting.</p>	<p>A drainage study was conducted for the frontage road project, and adequate flood mitigation is being proposed. Additional residential flooding from the frontage road project is not anticipated.</p>

		<p>But again, for our elected officials present, I want to make it clear that those are our concerns, and we would like to continue to take this up with them. So for the record, for the neighborhood I ask that our elected officials review the project in its entirety carefully in cooperation with the Houston Heights Association and the White Oak Bayou Association to ensure that it improves the neighborhood and not just the freeway.</p>	<p>Since the public hearing, TxDOT has continued to meet with local agencies and officials.</p>
<p>9.</p>	<p>Mark Williamson, President, Greater Super Heights Neighborhood Council</p>	<p>I'm the president of the Greater Heights Super Neighborhood Council, and I would like to read a letter that we approved and mailed to you. And I will provide a hard copy also to you and various elected officials.</p> <p>The Greater Heights Super Neighborhood Council strongly supports the inclusion of park features and/or park convertibility on both of the areas previously identified as detention ponds for use as mitigation on the I-10 frontage road project. We ask that formal plans be developed to convert the detention pond sites as public parks or sites with park features and amenities.</p> <p>Our super neighborhood council feels that a public park will be of great value to the surrounding community and area as a whole. This letter follows a formal adoption of a resolution in support of the inclusion of the public park as part of the detention ponds that are to be designed as part of the above-referenced project.</p> <p>We understand that TxDOT is willing to support and facilitate a design that would allow for, accommodate the use of one or both of the proposed detention ponds as a public park. We strongly urge all parties to come together and reach an agreement regarding the direction of design for this option and</p>	<p>TxDOT is currently in the process of working with local agencies to determine construction and design methods which will allow for future recreational uses of the basins. The final design is still pending based on these discussions with local agencies. TxDOT funding cannot be used to design or construct the parks.</p> <p>Future maintenance of any recreational facilities will be the responsibility of the park developer. If a park is not developed (or until one is developed), TxDOT will maintain the vegetation and detention basins.</p>

	<p>to determine an amenable solution for future acceptance, maintenance and upkeep of this park. We specifically ask that one of our representative bodies volunteer to become a sponsor for the park and to agree to future maintenance in a manner acceptable to TxDOT so that final design of the detention ponds may incorporate these aspects.</p> <p>Lastly, Greater Heights Super Neighborhood Council requests that the necessary properties for both detention ponds required for mitigation of the frontage road projects be acquired and constructed prior to or contemporaneously with the construction of the frontage road projects.</p> <p>Furthermore, we understand that a prompt decision must be made if the park option is to be included as part of the detention pond projects.</p> <p>The Greater Heights Super Neighborhood Council requests that a resolution on this matter be made prior to any, quote, point of no return, unquote, decisions from TxDOT on the direction of the detention pond design. We humbly ask that the involved parties coordinate with TxDOT to discuss a date by which a decision can be made.</p> <p>The Greater Heights Super Neighborhood Council understands this must be a collaborative effort on the part of several different entities if the inclusion of the park option to be successful. We want to encourage, wherever possible, the pursuit of shared goals and effective communication between the different representative bodies. We look forward to working with the involved parties to provide the valuable benefits to the community. Thank you for your time and consideration on this matter.</p>	<p>The detention basins will be constructed concurrently with the frontage road project. The frontage road project construction schedule is longer than the detention basin construction schedule and therefore, starting first. IH 10 will continue to serve as floodplain storage until the detention pond construction is complete.</p> <p>Since the public hearing, coordination with local agencies regarding the proposed project has continued.</p> <p>Comment noted.</p>
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		<p>And on a personal note, I support the proposal of the White Oak Bayou Association. They look very nice.</p>	<p>Comment noted.</p>
<p>10.</p>	<p>Robin Holzer, Chair, Citizens Transportation Coalition</p>	<p>My name is Robin Holzer, and I chair the board of the Citizens' Transportation Coalition, which is an all-volunteer grassroots group that works to engage neighborhoods in the transportation pattern.</p> <p>I want to start by saying that we support the extensive work and recommendations of the White Oak Bayou Association, whose volunteers have done a heroic effort to figure out how to make sure these detention facilities are a neighborhood amenity and not merely a flood mitigation element.</p> <p>We also share the concerns of the Houston Heights Association and other neighborhoods along I-10 who still have unaddressed issues with the roadway project that is in concert with the detention facilities tonight's hearing is about.</p> <p>To that end, I want to reiterate something you have heard tonight already, which is that given the concerns about flooding in the neighborhood, it's essential that the roadway project and the flood detention project be synchronized, such that the new flood mitigation measures come on-line in conjunction with the roadway. You can't rush forward with the road and get to the road so quickly. It's key, essentially.</p> <p>I also want to support the recommendation you've heard several times tonight to employ context sensitive design. In June of 2009, the Texas Transportation Commission at the</p>	<p>Comment noted.</p> <p>The IH 10 frontage roads project was not the subject of the public hearing. The IH 10 frontage roads were included as part of IH 10 roadway widening project. An Environmental Impact Statement was prepared and approved by FHWA for this project. In 2009 and 2010, neighborhood and civic association meetings were held to discuss the frontage road project. Modifications were made to the design based on public comments from these meetings.</p> <p>The detention basins will be constructed concurrently with the frontage road project. The frontage road project construction schedule is longer than the detention basin construction schedule and therefore, starting first. IH 10 will continue to serve as floodplain storage until the detention pond construction is complete.</p> <p>TxDOT is currently in the process of working with local agencies and officials to determine needs of the community and incorporate them into the final design of the detention</p>

		<p>highest level said context matters and it's essential on all projects that TxDOT develops to work with the neighborhood to figure out what they value and how to make sure the new projects meet their needs as much as possible; and this project is a great opportunity to do that. So I want to exhort you to embrace context sensitivity as you move forward with the design of this project.</p> <p>Finally, I want to wrap up by saying tonight's hearing, this formal public hearing, is a required piece of an extensive Federal process, and you guys know that. We know that. And it includes the environmental assessment that has been done and so on. It is a step of many steps, a long process to ultimately lead to the funding of the project with Federal funds.</p> <p>However, I urge both TxDOT and the elected officials and the neighbors in this room not to let this hearing be the extent -- or the final extent of the engagement on this project. While this is one opportunity for you to hear from us, please don't let it be the last, and please don't let the communication be one direction.</p> <p>With that, I would conclude by saying public participation matters, and we strongly believe that the input you get from the community will make your project better. Work with us.</p>	<p>basins.</p> <p>At any time, comments or questions regarding the proposed project can be sent to HOU-PIOWEBMAIL@DOT.STATE.TX.US or by calling Dr. Stanley Cooper at 713-802-5244.</p>
11.	Nancy Wilcox	<p>Thank you. I appreciate the opportunity to speak. I would like to reiterate the concern of concurrent construction of the detention basins and the project.</p> <p>I am concerned that the land has yet to be acquired; nor has there been much discussion, if any discussion, as to where the funding is going to come from for the acquisition of these detention basins. At this point, as I understand, the design work</p>	<p>Land acquisition of certain parcels has already begun. The remaining land acquisition will occur after a Finding of No Significant Impacts is received from the Federal Highway Administration. Construction is anticipated to begin in the</p>

		<p>is not even completed because the land has yet to be acquired.</p> <p>So I am concerned about how a project that has already been let is going to be constructed concurrent to the construction of the detention basins when the property has yet to be acquired.</p> <p>Given that situation, I would propose that there would be no grave raising and no pump station installed in the area where the water leaves White Oak Bayou and then floods into I-10, so that I-10 continues as the default detention basin until -- until the detention basins are, in fact, functioning and on-line.</p> <p>There is another safety concern that I would like to articulate. At the westbound feeder, as it goes over White Oak Bayou at Yale, drops down to the level -- or to below the BFE so that, in fact, when traffic exits on a dark rainy night, it is conceivable that the driver would not see the water flowing over the bridge because it is well below BFE and, in fact, run into several feet of water and could conceivably be whisked off the bridge because that bridge is well below the BFE and I'm concerned that we would be creating a very serious safety issue there.</p> <p>I think that would conclude my comments. Thank you.</p>	<p>fall of 2010.</p> <p>Funding of the proposed project will be from Federal funds.</p> <p>The final design of the detention basins is currently being developed. It is on schedule with the proposed construction date.</p> <p>The detention basins will be constructed concurrently with the frontage road project. The frontage road project construction schedule is longer than the detention basin construction schedule and therefore, starting first.</p> <p>IH 10 will continue to serve as floodplain storage until the detention pond construction is complete.</p> <p>Roadway lighting is included as part of the IH 10 frontage road project. Additionally, lights are located along Yale Street.</p>
12.	David Danburg	<p>I'm David Danburg and I own the majority of the property between 4th, 5th and 6th Street, to the west of Yale and this is where the detention pond is going to be dug out. And at the same time the Yale Street from I-10 to 610 is going to be under</p>	<p>Access to property will be maintained during and after construction. Construction of the detention basins will not block access of any roadways.</p>

	<p>construction, I have no access or egress from my property if those projects are simultaneously being done -- worked on.</p> <p>On a more personal note, the maintenance of 5th Street and 6th Street between Yale and that detention pond has been left solely to me for the last 40 years. I have permitted, built and paid for the sanitary sewer system back there and the street repairs on both 5th and 6th Street and I would respectfully submit that you-all pay a little bit of homage to me when you are tearing them up with trucks going in and out of the street.</p> <p>But particularly I'm concerned about the fact that Yale would be built and torn up at the same time that the detention pond work is being done, and it's going to create a great hardship for us. We've been there for 60-something years, and we don't want the discomfort of having to tell people we can't get to our property because of the work on Yale Street and the work back on the detention pond.</p> <p>Also, on another personal note, Debbie Boreman is here tonight. She talked me into, 20-something years ago, planting those many oak trees along Yale Street and when I built the building on Yale between 5th and 6th, all of that landscaping has just been torn up last year.</p> <p>I don't want my trees destroyed. I don't know how to prevent it but between me crying to Dale Garchintzky (phonetically), Adrian Garcia, Felix Fraga, and I don't want to go to Councilman Gonzalez and start crying about all the work that's being done to tear down all of the money that I've spent over the last 40 or 50 years.</p> <p>I would respectfully submit that someone give me some consideration when all of this work goes on.</p>	<p>Comment noted.</p> <p>Any proposed road closures for the Yale Street project will be the responsibility of the City of Houston. As stated, the detention basin project would not close any roads during construction.</p> <p>TxDOT is only proposing to impact vegetation, including trees, within the footprints of the detention basins. Complete vegetation removal within project area would be minimized where possible.</p> <p>Comment noted.</p>
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13.	John Kleiber	<p>My name is John Kleiber with the Floodway Coalition of Houston. One of the primary concerns that I have is the feeder road bridges at Yale. The feeder road bridges at Yale are designed to be 4 feet below the base flood elevation. City of Houston Ordinance, 1943, dictates that no structure shall be built unless it is 18 inches above the base flood elevation. State of Texas, TxDOT should not be above the laws that us, as the citizens of Houston, have got to abide by.</p> <p>Secondly, it's a hazard because, effectively, if you put a bridge 4 feet below the base flood elevation, you are creating a dam. Where is the water going to go? It has but one place, and that's into the neighborhood.</p> <p>In addition, there are several spillways, more specifically the one in front of the Firefighters' Credit Union. During major flooding events, that water is as much as 10 feet high. If the ramp that you guys are proposing to put in is put in place, that is going to create yet another wall if you do not have detention in place to hold the water. Again, effectively, what you will have is a 10-foot dam. You'll have a nice dry I-10, but your neighborhoods are going to be flooding.</p> <p>I look around this room and I see a lot of signs and it looks interesting when I see these icons of flooding on I-10, but I dare say if you ask anyone here, they would rather see a dozen 18 wheelers with some water than hundreds upon hundreds of their homes.</p>	<p>The City of Houston and the Harris County Flood Control District reviewed the drainage report and the proposed design and stated no objections to the findings.</p> <p>The proposed project will not create a roadway elevation lower than the existing frontage roads in the area or Yale Street. The frontage road bridge will not act as a dam, but allow water to flow under the bridge.</p> <p>The purpose of the detention basins is to store the floodwater that previously flowed into IH 10 near the Firefighters' Credit Union. IH 10 will continue to serve as floodwater storage until the detention basin construction is complete.</p> <p>The flooding of IH 10 presents a safety concern to many residents. This highway serves as the only major east-west route into and out of Houston. Removing IH 10 from the floodplain and providing detention basins for the floodwater will have a positive effect on the community.</p>
14.	Darryl Heine	<p>Thank you, Mr. Gonzalez, for coming to this hearing. You-all have got a good councilman out here.</p> <p>I just found out about this project yesterday. You would think that a person who is a native Houstonian, that grew up in this</p>	<p>After receiving this comment, a staff member met with Mr. Heine during the public hearing. It was determined that he</p>

		<p>neighborhood would have gotten some kind of notice from TxDOT, when I own a piece of property that I just saw on your map up here that's going to be impacted by this flood -- flood pond. And I'm wondering how in the world that my house won't be in it if the streets are gone and my house. That is kind of interesting.</p> <p>Even I know that the Federal Government, we had to give them notice a year in advance when we had to get rid of the HUD people that we had up in Brenham; and I'm wondering why in the world TxDOT didn't give me any kind of notice that my property is going to be impacted.</p> <p>What's interesting also is I'm wondering if whether or not the City of Houston and TxDOT have been in collaboration with this whole program for who knows how far back. It seems that this floodway was put into fruition about two or three years ago to affect 9900 properties and the people got no compensation for their land but their tax values are somewhat still in place. I'm dumbfounded, dumbfounded by all of this; and I would like to know why I haven't gotten any information. That's all I've got to say.</p>	<p>does not live within or adjacent to the proposed project. He lives near an alternative, south of IH 10, which was dismissed and his property will not be impacted by the proposed project.</p> <p>Per TxDOT requirements, notices were mailed on January 19, 2010 to property owners within or adjacent to the project area. Attendees of the previous public meetings were also added to this distribution list. Additionally, notice of the public hearing was published in the following:</p> <ul style="list-style-type: none"> o Houston Chronicle on January 19 and February 8, 2010; o Heights Leader on January 21 and February 11, 2010; o La Voz on January 24 and February 7, 2010. <p>All attendees of the public hearing and those that submitted comments will be added to the distribution list to receive any mailed information regarding the project.</p> <p>Floodplain boundaries are approved by FEMA and not TxDOT.</p>
Written Comments			
15.	Senthil Alagarsamy	I have concerns about mosquitoes, whether the land will have any kind of park/greenspace use and land settling/foundation	Mosquitoes may be anticipated within the project area, as they are currently located within the region. The Harris

		<p>issues that may arise from having this retention pond place.</p> <p>I would be fully in support of this project if the mosquitoes and park issues were addressed.</p>	<p>County mosquito control division works to prevent and control mosquito-borne diseases. The detention basins will provide greenspace after construction. Any park facilities would be dependent on another developer; however, TxDOT is in coordination with local agencies regarding potential park use. Land settling is not anticipated from construction of the proposed project.</p>
16.	J. Michael Goodwin	<p>I would strongly support TxDOT working with White Oak Bayou Association to ensure initial construction of the detention ponds makes them amenable to development as a multi-use detention facility. The intent would be to maximize the benefits that accrue from the expenditure from both public safety and utility.</p>	<p>TxDOT is currently in the process of working with the White Oak Bayou Association to determine construction and design methods which will allow for future recreational uses of the basins. TxDOT funding cannot be used to construct the parks; however, TxDOT is open to allow for the future development of parks and/or recreational use of the detention basins. The final design is still pending based on these discussions.</p>
17.	Jim Ohmart	<p>My tenant is a retired school teacher who has lived there since 1980. He is disabled and I have kept his rent well below market. I am concerned about his ability to find affordable housing.</p>	<p>The TxDOT ROW Acquisition and Relocation Assistance Program is conducted in accordance with the Uniform Relocation Assistance and Real Property Acquisition Policy Act of 1970 (Public Law 91-6) and is available to all displaced residents. This includes tenants on affected property. TxDOT will assist persons in finding decent, safe, and sanitary replacement housing in the project vicinity. The available replacement housing must also be within the financial means of those individuals affected.</p>
18.	Deborah Oliver	<p>The right to ownership is in court.</p>	<p>Comment noted.</p>
19.	Donna Bennett	<p>Request construction of detention ponds in conjunction with construction of IH 10 feeder project.</p>	<p>The detention basins will be constructed concurrently with the frontage road project. The frontage road project construction schedule is longer than the detention basin construction schedule and therefore, starting first. IH 10 will continue to serve as floodplain storage until the detention pond construction is complete.</p>

		Request detention ponds be constructed such that they can be used by the community as much needed green space.	TxDOT is currently in the process of working with local agencies to determine construction and design methods which will allow for future recreational uses of the basins. TxDOT funding cannot be used to construct the parks; however, TxDOT is open to allow for the future development of parks and/or recreational use of the detention basins. The final design is still pending based on these discussions with local agencies.
20.	Jim Bennett	If TxDOT will use tax dollars for this project, maximum public benefit must be sought. Please work with City of Houston, Harris County Flood Control District, Houston Heights Association, White Oak Bayou Association, and other interested groups to maximize public use/benefit of this project. This must include leaving the project as "park ready."	TxDOT is currently in the process of working with local agencies to determine construction and design methods which will allow for future recreational uses of the basins. TxDOT funding cannot be used to construct the parks; however, TxDOT is open to allow for the future development of parks and/or recreational use of the detention basins. The final design is still pending based on these discussions with local agencies.
21.	Christine Spin	Because of the proximity of IH 10 and the detention basins to a vibrant, active neighborhood, TxDOT would be considerate and cooperative to create a visually appealing basin area. The neighborhood strongly urges TxDOT to assess and design an area basin that can expand its use as a detention pond to a park that is pedestrian friendly. By making the additional investment of time and money to create an environment that serves and incorporates the neighborhood, TxDOT would improve the quality of life beyond the intended flood mitigation.	TxDOT is currently in the process of working with local agencies to determine construction and design methods which will allow for future recreational uses of the basins. TxDOT funding cannot be used to construct the parks; however, TxDOT is open to allow for the future development of parks and/or recreational use of the detention basins. The final design is still pending based on these discussions with local agencies.
22.	Ken Hoge	My house at 505 Heights Blvd. is just adjacent to the 100 year floodplain of White Oak Bayou. I also reside just 3 blocks from I-10. I am concerned about the increase in traffic and traffic noise on the access road for I-10 on the north and south sides which will be caused by the Yale access ramps. I am also	The IH 10 frontage roads project was not the subject of the public hearing. The IH 10 frontage roads were included as part of IH 10 roadway widening project. An Environmental Impact Statement which included a noise analysis was prepared and approved by FHWA for this project. In 2009

		<p>very concerned about the impact of this project on the floodplain. It is very difficult to see either the benefit to our neighborhood of this project or the possibility that it will not have an adverse impact on our quality of life and property. I strongly oppose this project as described and request it not proceed.</p>	<p>and 2010, neighborhood and civic association meetings were held to discuss the frontage road project. Modifications were made to the design based on public comments from these meetings. The detention basins project is proposed to mitigate for floodplain impacts from the frontage road project.</p>
23.	Scott Johnson	<p>I strongly support modifications of detention pond plans to accommodate park uses as proposed by the White Oak Bayou Association.</p>	<p>TxDOT is currently in the process of working with local agencies to determine construction and design methods which will allow for future recreational uses of the basins. TxDOT funding cannot be used to construct the parks; however, TxDOT is open to allow for the future development of parks and/or recreational use of the detention basins. The final design is still pending based on these discussions with local agencies.</p>
24.	Greater Super Heights Neighborhood Council	<p>The Greater Heights Super Neighborhood Council strongly supports the inclusion of park features and/or park convertibility on both of the areas previously identified as detention ponds for use as mitigation on the I-10 frontage road project. We ask that formal plans be developed to convert the detention pond sites into public parks or sites with park features and amenities. Our Super Neighborhood Council feels that a public park will be of great value to the surrounding community and area as a whole. This letter follows a formal adoption of a resolution in support of the inclusion of a public park as part of the detention ponds that are to be designed as part of the above referenced projects.</p> <p>We understand that the TxDOT is willing to support and facilitate a design that would allow for and accommodate the use of one or both of the proposed detention ponds as a public park. We strongly urge all parties to come together and reach an agreement regarding the direction of design for this option</p>	<p>TxDOT is currently in the process of working with local agencies to determine construction and design methods which will allow for future recreational uses of the basins. The final design is still pending based on these discussions with local agencies.</p> <p>TxDOT funding cannot be used to design, construct or maintain the parks. This will be the responsibility of the park developer.</p>

		<p>and to determine an amenable solution for future acceptance, maintenance, and upkeep of this park. We specifically ask that one of our representative bodies volunteer to become the sponsor for the park and to agree to future maintenance in a manner acceptable to TxDOT so that final design of the detention ponds may incorporate these aspects.</p> <p>Lastly, Greater Heights Super Neighborhood Council requests that the necessary property for both detention ponds required for mitigation of the frontage roads projects be acquired and constructed prior to or contemporaneously with the construction of the frontage road projects. Furthermore, we understand that a prompt decision must be made if the park option is to be included as part of the detention pond projects. The Greater Heights Super Neighborhood Council requests that a resolution on this matter be made prior to any "point of no return" decisions from TxDOT on the direction of the detention pond design. We humbly ask that the involved parties coordinate with TxDOT to discuss a date by which a decision must be made.</p> <p>The Greater Heights Super Neighborhood Council understands that this must be a collaborative effort on the part of several different entities if the inclusion of the park option is to be successful. We want to encourage, wherever possible, the pursuit of shared goals and effective communication between the different representative bodies. We look forward to working with the involved parties to provide a valuable benefit to the community. Thank you for your time and consideration of this matter.</p>	<p>Land acquisition of certain parcels has already begun. The remaining land acquisition will occur after a Finding of No Significant Impacts is received from the Federal Highway Administration. The detention basins will be constructed concurrently with the frontage road project. The frontage road project construction schedule is longer than the detention basin construction schedule and therefore, starting first.</p> <p>Since the public hearing, coordination has continued with local agencies and officials to complete a final design.</p>
25.	Mark Williamson	Personally, I support the proposals of the White Oak Bayou Association.	Comment noted.

<p>26.</p>	<p>Sheila Jackson Lee, U.S. Congress-woman, 18th District, Texas</p>	<p>I joined some very concerned neighbors this past weekend on the banks of White Oak Bayou. I was impressed by the cohesiveness of the neighbors and of the massive green belt of which our bayous are part. However, I was disappointed at the disregard of the greenbelt and lack of development. Let us agree on one thing tonight. We will all work together to make White Oak Bayou (and the detention ponds) into park space with complimentary hike and bike trails. Let's work together to connect our bikeways and make them better.</p> <p>I strongly support recommendations by White Oak Bayou Association and Greater Heights Super Neighborhood Council encouraging Texas Department of Transportation to include provisions for park features to be added in the areas identified as detention ponds for mitigation on the I-10 frontage road project.</p> <p>The \$40 million that it will cost to acquire the land alone will provide multiple benefits to the surrounding communities by TxDOT working with Harris County Flood Control District and the City of Houston to develop park amenities. We encourage a collaborative effort for the inclusion of the park option.</p> <p>The recommendations also make reference to potential trails and other park improvements. We understand that construction and maintenance of the park improvements would be funded through this collaborative effort with community support.</p> <p>We want to encourage, wherever possible, the pursuit of shared goals and effective communication between the different representative bodies. We look forward to working with TxDOT to deliver this valuable benefit to the community.</p>	<p>TxDOT is currently in the process of working with local agencies to determine construction and design methods which will allow for future recreational uses of the basins. The final design is still pending based on these discussions with local agencies.</p> <p>Comment noted.</p> <p>Since the public hearing, meetings have continued with the White Oak Bayou Association and local officials to discuss the proposed project and how to design the basins to allow for future recreation development.</p> <p>TxDOT funding cannot be used to design, construct or maintain the parks; however, TxDOT is open to allow for the future development of parks and/or recreational use of the detention basins.</p> <p>At any time, comments or questions regarding the proposed project can be sent to HOU-PIOWEBMAIL@DOT.STATE.TX.US or by calling Dr. Stanley Cooper at 713-802-5244.</p>
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27.	Carole C. Johnston	<p>I am delighted at the prospect of these detention ponds, not only for the floodwater mitigation but especially for the additional greenspace it affords Houstonians, enhancing our quality of life.</p> <p>The addition of an exit ramp at Yale from I-10, on the other hand, does not meet my approval. It is completely unnecessary and unwanted.</p>	<p>Comment noted.</p> <p>The IH 10 frontage roads project was not the subject of the public hearing. The IH 10 frontage roads were included as part of IH 10 roadway widening project. An Environmental Impact Statement was prepared and approved by FHWA for this project. In 2009 and 2010, neighborhood and civic association meetings were held to discuss the frontage road project. Modifications were made to the design based on public comments from these meetings.</p>
28.	Anne O. Culotta, President, Houston Heights Association	<p>Dear Mayor Parker,</p> <p>I attach for your attention a resolution passed by the Houston Heights Association and presented last week at a statutory public hearing organized by TxDOT at Reagan High in our neighborhood. The meeting was well attended by local citizens and included two Council members, State and Federal representatives or their aides and a number of groups, such as the White Oak Bayou Association and the Citizens Transportation Coalition.</p> <p>A number of those attending spoke for the record. All supported and requested greater participation in major infrastructure projects affecting both our city and our neighborhood, and much closer and effective coordination between the TxDOT and the City. With respect to the project under discussion, such coordination clearly has been lacking,</p>	<p>The IH 10 frontage roads project was not the subject of the public hearing. The IH 10 frontage roads were included as part of IH 10 roadway widening project. An Environmental Impact Statement was prepared and approved by FHWA for this project. In 2009 and 2010, neighborhood and civic association meetings were held to discuss the frontage road</p>

	<p>and there are reasons to question whether the plans approved in 2004 (without effective community input, at least in the Heights) are a constructive addition to our neighborhood. In order to lay claim to federal stimulus dollars, TxDOT has effectively attempted an end-run around the communities affected, declaring the project “let and underway”. In the face of these circumstances, our concern for the future of our neighborhood has led the Heights Association to take a position on this matter.</p> <p>Among other issues:</p> <p>We are concerned that the planned Yale Street on/off ramps will engender unacceptable additional cross-town traffic, flowing through this historic neighborhood. The most recent traffic analysis carried out by the City does not take into account the addition of these ramps and is predicated on Yale being a divided four- lane thoroughfare. The City’s current rehabilitation project for Yale, for which we had your support, does not envision Yale as a four-lane divided road and there is no doubt that the ramps will affect traffic.</p> <p>We understand that new bridges being constructed over White Oak Bayou are well below the Base Flood Elevation although a City code expressly forbids this. We are concerned that these bridges will be a future hazard in times of flood and may well have to be reconstructed in the foreseeable future at great expense. We also do not understand why the State (TxDOT) is allowed to provide lower standards than those mandated by the City.</p> <p>The contract to build I-10 frontage road and its bridges and ramps from Yale Street has been let and “stimulus funds” have been made available for this “shovel ready” project, but the</p>	<p>project. Modifications were made to the design based on public comments from these meetings.</p> <p>The Yale Street access ramps are to provide access to IH 10. Yale Street is not anticipated to be adversely impacted.</p> <p>The City of Houston and the Harris County Flood Control District reviewed the drainage report and the proposed design and stated no objections to the findings. The proposed bridges were designed to avoid any increase in the Base Flood Elevation. The proposed bridges are located above the current 10-year floodplain, which is consistent with TxDOT standard design practices.</p> <p>Land acquisition of certain parcels has already begun. The remaining land acquisition will occur after a Finding of No Significant Impacts is received from the Federal Highway</p>
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		<p>detention ponds (which TxDOT has acknowledged are a critical element in mitigating the effect of flooding associated with the road project), have not been funded at this time. In fact, none of the property needed for those ponds has yet been acquired. We are informed by TxDOT that when an Environmental Impact Statement for one of the ponds is completed and approved, TxDOT will be able to commence the purchase of properties required for the ponds. It appears there is no guarantee of concurrency in building both these projects and we are concerned that without the ponds (even for a short period of time) water, which in the past has flooded I-10, would back up, seriously affecting adjacent areas in the Heights.</p> <p>In short we feel that these projects, conceived over 10 years ago and approved 7 years ago, do not reflect the best possible engineering design, do not satisfactorily address our neighborhood’s concerns and may well be detrimental to the future of what has been described by a previous Mayor as “one of the city’s greatest gems and assets”.</p> <p>We, as citizens, respectfully request the City’s timely assistance in working with us, coordinating with TxDOT and seeking to find the most cost and functionally effective solutions that will be of maximum benefit to all affected parties.</p>	<p>Administration. The detention basins will be constructed concurrently with the frontage road project. The frontage road project construction schedule is longer than the detention basin construction schedule and therefore, starting first. IH 10 will continue to serve as floodplain storage until the detention pond construction is complete.</p> <p>Comment noted.</p>
29.	Jonathon Smulian	<p>Land has not been purchased or funding allocated for the detention ponds. No work on the I-10 frontage roads, ramps to Yale Street or new bridges across White Oak Bayou should commence until the construction of the detention ponds is assured. TxDOT has informed us that without the detention ponds the Houston Heights neighborhood will be vulnerable to serious flooding. The public needs to participate in the design of the detention ponds if they are going to be an asset to the</p>	<p>Land acquisition of certain parcels has already begun. The remaining land acquisition will occur after a Finding of No Significant Impacts is received from the Federal Highway Administration. The detention basins will be constructed concurrently with the frontage road project. The frontage road project construction schedule is longer than the detention basin construction schedule and therefore, starting first. IH 10 will continue to serve as floodplain storage until</p>

		<p>neighborhood. TxDOT has only complied with statutory public information requirements and not in any serious public participation. This is unacceptable!!!</p>	<p>the detention pond construction is complete.</p> <p>Since the public hearing, coordination has continued with local agencies and officials regarding the final design of the detention basins. At any time, comments or questions regarding the proposed project can be sent to HOU-PIOWEBMAIL@DOT.STATE.TX.US or by calling Dr. Stanley Cooper at 713-802-5244.</p>
<p>30.</p>	<p>Elisa Goodwin</p>	<p>My husband and I live on the 700 block of Rutland, an area which could easily be compromised by the new Katy Freeway access roads, if the detention/drainage issues are postponed or neglected. I'd like to make two points:</p> <p>1) We feel very strongly that TxDOT has a responsibility to address drainage issues in a timely fashion. The detention pond project should be undertaken at the same time as the access road. Waiting a year, or two, or until funds are available, will endanger our property.</p> <p>2) We support the White Oak Bayou Association and their efforts to ameliorate the detention ponds, so that they become an asset to our community. As well as being very important functionally, the detention pond areas have the potential to become a focal point for wildlife, a link in the greenspace chain leading towards downtown, and a restful, attractive place for Heights residents to walk, bike, picnic, or birdwatch. We ask that TxDOT take the extra steps necessary to make the detention pond areas park-ready.</p>	<p>The detention basins will be constructed concurrently with the frontage road project. The frontage road project construction schedule is longer than the detention basin construction schedule and therefore, starting first. IH 10 will continue to serve as floodplain storage until the detention pond construction is complete.</p> <p>TxDOT is currently in the process of working with local agencies to determine construction and design methods which will allow for future recreational uses of the basins. TxDOT funding cannot be used to construct the parks; however, TxDOT is open to allow for the future development of parks and/or recreational use of the detention basins. The final design is still pending based on these discussions with local agencies.</p>

<p>31.</p>	<p>Tom Dornbusch, Super neighborhood 22 Council VP, Board Member, White Oak Bayou Association, Citizen’s Transportation Coalition</p>	<p>This project is designed to displace the stormwater holding capacity of the below-grade IH10 mainlanes. Without that holding capacity, IH10 adjacent neighborhoods would surely have flooded during TS Allison, and are likely to be severely impacted by this project if the proposed detention facilities - for which right-of-way acquisition has not yet occurred - are not functional prior to, or concurrent with, the roadway construction.</p> <p>As requested by many neighborhood organizations and White Oak Bayou Association, these detention facilities should be constructed to be park-ready neighborhood amenities, rather than inaccessible and neighborhood unfriendly pits.</p> <p>Additionally, there are multiple bridges planned as part of the roadway expansion connected to this project that are designed to be constructed below the base flood elevation. This is inconsistent with the City of Houston ordinance regulating the elevation of structures in the floodplain. FHWA FEDERAL-AID POLICY GUIDE December 7, 1994; SUBCHAPTER G - ENGINEERING AND TRAFFIC OPERATIONS; PART 650 - BRIDGES, STRUCTURES, AND HYDRAULICS; Subpart A - Location and Hydraulic Design of Encroachments on Flood Plains: Sec.650.115 Design Standards; states that: (5) The design of encroachments shall be consistent with standards established by the FEMA, State, and local governmental agencies for the administration of the National Flood Insurance Program.</p>	<p>The detention basins will be constructed concurrently with the frontage road project. The frontage road project construction schedule is longer than the detention basin construction schedule and therefore, starting first. IH 10 will continue to serve as floodplain storage until the detention pond construction is complete.</p> <p>TxDOT is currently in the process of working with local agencies to determine construction and design methods which will allow for future recreational uses of the basins. The final design is still pending based on these discussions with local agencies.</p> <p>The City of Houston and the Harris County Flood Control District reviewed the drainage report and the proposed design and stated no objections to the findings.</p> <p>The FHWA policy in 23CFR 650 is for “significant encroachment.” This is defined as a highway encroachment and any direct support of likely base floodplain development that would involve one or more of the following construction or flood –related impacts: (1) a significant potential for interruption or termination of a transportation facility which is needed for emergency vehicles or provides a community’s only evacuation route; (2) a significant risk, or; (3) a significant adverse impact on natural and beneficial floodplain values. The proposed project is not needed for emergency vehicles nor is the only evacuation route, is located above the 10-year floodplain, and would not adversely impact the floodplain; therefore, project does not meet the definition of significant encroachment.</p>
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<p>32.</p>	<p>Greater Houston Partnership and Quality of Life Coalition</p>	<p>The Greater Houston Partnership’s Quality of Life Committee in collaboration with the <i>Quality of Life Coalition</i>, an organization with 92 endorsing organizations in the Houston region, advocates for increased linear parks and open space, development of multi-use trails, improvement in water quality and enhanced flood control along all 9 of Houston’s major bayou arteries.</p> <p>In this regard, we support TxDOT’s proposal to construct flood mitigation ponds and minor drainage improvements along the White Oak Bayou watershed near IH 10, in conjunction with IH 10 roadway improvements. We strongly urge TxDOT to proceed with any remaining purchases of land needed for the six (6) recommended flood mitigation pond sites, located along sections of White Oak Bayou from Shepherd Drive to Yale Boulevard, so that construction can begin in the fall of 2010.</p> <p>We also encourage TxDOT to construct these detention ponds so that they may provide multiple benefits to the community. Over the past 5 years, the City of Houston and Harris County Flood Control District have discovered that use of water quality filtering features and recreational facilities (parks, trails, nature areas) are an important multi-use of flood mitigation detention ponds. An extensive system of linear parks and trails/green space along Houston’s major bayou arteries contributes to health and wellness, economic development, flood control, air and water quality. The costs of acquiring the land, excavating the detention ponds and maintaining the ponds can be productively balanced to also provide long-term water quality and recreational benefits to the communities around White Oak Bayou.</p> <p>Since 2006, TxDOT has targeted construction of these detention ponds along White Oak Bayou as a critical</p>	<p>Land acquisition of certain parcels has already begun. The remaining land acquisition will occur after a Finding of No Significant Impacts is received from the Federal Highway Administration. Construction is anticipated to begin in the fall of 2010.</p> <p>TxDOT is currently in the process of working with local agencies to determine construction and design methods which will allow for future recreational uses of the basins. The final design is still pending based on these discussions with local agencies. TxDOT funding cannot be used to design, construct or maintain the parks; however, TxDOT is open to allow for the future development of parks and/or recreational use of the detention basins.</p> <p>Comment noted.</p>
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		<p>component of flood reduction along the I-10 corridor in the Heights area. Beginning construction this fall is important and timely in conjunction with Harris County and the City of Houston’s continued flood control and water quality efforts along White Oak Bayou. We appreciate your cooperation in moving this project forward as quickly as possible. Please contact us with any questions or comments in this regard.</p>	
<p>33.</p>	<p>White Oak Bayou Association</p>	<p>Thank you for holding a public hearing which allowed representatives of the adjacent communities to voice their opinions. WOBA’s Detention Basin Design Alternative exhibit was well received and supported by most public hearing attendees, including elected officials who represent the area.</p> <p>Proposed Design Alternative The proposed design alternative will not only mitigate storm water impacts but also offset noise, air, visual and roadway runoff pollution, which are all by-products of highway expansion. It will further set the stage for needed urban park space and considers the values of the surrounding neighborhoods. The benefits of a park– ready detention basin construction will greatly offset the minimal additional construction cost.</p> <p>WOBA partners with fellow Community Organizations WOBA joins with community groups in the Heights area, the Citizens Transportation Coalition and the Floodway Coalition of Houston to request the following:</p> <ul style="list-style-type: none"> • Storm Water Detention be implemented prior to, or concurrent with Feeder Road Construction The historic & existing I-10 storm water storage capacity of I-10 between Patterson Street and Washington Avenue should remain intact until the new detention pond project is completed 	<p>Comment noted. TxDOT is open to allow for the future development of parks and/or recreational use of the detention basins.</p> <p>The detention basins will be constructed concurrently with the frontage road project. The frontage road project construction schedule is longer than the detention basin</p>

	<p>at a capacity that will offset the loss of this I-10 main lanes detention. Likewise, the new bayou feeder road bridges in the Yale Street area should not be constructed until the detention ponds are fully functional.</p> <ul style="list-style-type: none"> <p>• Clarification of Feeder Road Sections and Bridges below the BFE</p> <p>WOBA asks that TxDOT explain how the hazardous conditions created by sections of the feeder roads and bridges constructed well below the Base Flood Elevation (BFE) will be resolved. Specifically, the community concern that the new eastbound Yale/Heights exit ramp will take exiting drivers from the I-10 Freeway over a rise and then drop them immediately onto the bayou feeder road bridge at Yale Street which will be located below the BFE. During a heavy rain event this is particularly hazardous.</p> <p>• Coordination with other Public Entities and Community Organizations</p> <p>We understand that Stimulus Fund requirements for the I-10 Ramp/Feeder Road Project –CSJ:0271-07-242 impose time restrictions. We feel that TxDOT is rushing both the road and detention projects without full community involvement and coordination with local government entities. Improved updated designs and planning would result in better local traffic flow, traffic safety, storm water mitigation and over the long term substantial savings in taxpayer dollars.</p> 	<p>construction schedule and therefore, starting first. IH 10 will continue to serve as floodplain storage until the detention pond construction is complete.</p> <p>The proposed project is not anticipated to increase the Base Flood Elevation and is above the 10-year floodplain, consistent with TxDOT Houston District design standards. The bridge roadway grades provided are necessary for safe tie-ins at the existing intersection with Yale Street. The thinnest beam available to meet the span and skew requirements of the crossing were used to minimize impacts to the floodplain. Any impacts to White Oak Bayou from the bridges at Yale Street and IH 10 will be mitigated by removal of the timber approach spans of the abandoned railroad trestle bridge over White Oak Bayou, upstream of Yale Street.</p> <p>Public coordination for the frontage road project was conducted as part of the IH 10 roadway expansion project. Since FHWA approval of this project, additional neighborhood and civic association meetings were held in 2009 and 2010 to discuss the frontage road project. The public was given the opportunity to comment on the detention basins project through public involvement. Previous public meetings were held on November 29, 2005 and June 8, 2006. A Notice Affording Opportunity for Public Hearing was published in English in the Houston Chronicle on November 19, 2009 and in Spanish in La Voz on November 22, 2009. Requests were received; therefore,</p>
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		<p>WOBA’s recommended Detention Basin Design Alternative and Detention and Drainage Study Review and Recommendations by WOBA Vice President Jim Mackey are attached for inclusion in the public record.</p> <p>Conclusion We have long advocated for the mutually inclusive goals of sound flood reduction policy and projects as well as bayou parks and trails. We are able to offer a unique perspective as we work with our fellow community based organizations and elected officials in urging TxDOT to refine their plans for the benefit of their department and the community as a whole. The 2-18-2010 Public Hearing was another step as we work toward the goal of formulating a project we can all live with and be proud of. Our organization would welcome the opportunity to meet again with TxDOT to discuss flood risks and detention pond designs. We also ask that stakeholders be kept advised of TxDOT’s project progress, preferably by e-mail. Please let the 2-28-2010 Public Hearing not be the last communication. For instance, it was troubling to hear from owners of properties adjacent to the proposed detention basins who stated that access to their properties would be impaired and that they had not been notified of TxDOT’s plans as of the date of the public hearing. We presume this is because TxDOT has not yet purchased the properties. All the more reason to act swiftly.</p>	<p>the public hearing was held on February 18, 2010. All comments received from the public have been documented, reviewed, and provided responses.</p> <p>Meetings are currently being held with the White Oak Bayou Association and local officials. Coordination will be ongoing throughout the project.</p> <p>Comments or questions regarding the proposed project can be sent to HOU-PIOWEBMAIL@DOT.STATE.TX.US or by calling Dr. Stanley Cooper at 713-802-5244.</p> <p>Follow up coordination with the property owner that spoke at the public hearing determined he lived outside of the project area (south of IH 10) and would not be impacted by the proposed project.</p>
34.	Jim Mackey, Vice President, White Oak Bayou Association	WOBA undertook a detailed review of TxDOT’s 2/09 Drainage Mitigation Study for the I-10 Project as well as the 2/10 detention pond renditions and the road construction plan sets. In view of the following questions and comments, it is our	

	<p>hope that TxDOT will revisit their drainage study and make modifications to their plans as they relate to detention and mitigation of flows on lower WOB.</p> <p>For example:</p> <p><u>Limits of Drainage Study</u> While utilizing best practices and technically sound, TxDOT’s 2/09 Drainage Study is flawed by the fact that alternative simulations for the proposed new and replacement feeder road bridges did not include models for raising these components to feasible and practical levels close to or better yet above the Base Flood Elevation. If run, such modeling simulations would have provided TxDOT and the community with solid information about whether additional mitigation measures would be needed to offset any increased flows passing under the raised bridges.</p> <p>WOBA asks TxDOT to explain in detail how the capacities of the proposed detention pond facilities were calculated. WOBA is concerned that TxDOT may be unknowingly claiming existing storm water storage capacity as their own at site A-5. Please see attached document: I-10 A-5 Existing Detention</p> <p><u>Patterson Street</u> WOBA encourages TxDOT to secure an agreement with the City of Houston (COH) to abandon Patterson Street north of Cornish and to include that street ROW to expand the A-5 Detention Site.</p> <p>In addition to the benefit of increased detention, removing the Patterson dead-end near White Oak Bayou, reduces the likelihood that this area will become an illegal dumping ground</p>	<p>Additional alternatives were not evaluated in the drainage report because the proposed design and roadway grades are necessary for safe tie-ins at the existing intersections. Required mitigation for floodplain impacts will occur by removing timber approach spans of the abandoned railroad trestle bridge over White Oak Bayou, upstream of Yale Street. This will result in no increase in flood elevations upstream or downstream of IH 10.</p> <p>Required capacity was determined by conducting a hydraulic analysis using the U.S. Army Corps of Engineers program HEC-HMS and HEC-RAS models from the TSARP study.</p> <p>Including Patterson Street was not evaluated as an alternative. The A5 site is adequately-sized based on detention capacity needs. Additional land acquisition is not required.</p>
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	<p>as so often occurs on abandoned streets.</p> <p>Additionally, WOBA recommends that TxDOT and/or the COH purchase the undeveloped townhome lots along the north side of Cornish for further expansion of the A-5 site.</p> <p><u>Park-Ready Wet Bottom Detention</u> In conjunction with WOBA’s proposed “park ready” wet bottom detention sites. We encourage TxDOT and the COH to coordinate on the A9-10 design and construction. If CoH drainage were to be rerouted to provide semi-permanent flows into the site, additional detention capacity may be required. The adjoining CoH-owned MKT ROW south of the hike-n-bike trail could be sculpted to provide additional storm water storage. Please refer to WOBA’s attached Detention Pond Design Alternative.</p> <p><u>Project Components below the Base Flood Elevations</u> The more than 200’ of open land between Yale and White Oak Bayou on the westbound feeder ROW could be utilized to raise the approach for the westbound bayou feeder road bridge at Yale. By bringing the elevation of the approach up, the bridge itself could be raised 2-3’.</p> <p>Yale Street at I-10 and the nearby Yale bayou bridge are located at 42’ which is 2-7’ below the BFE at this location. The lower chord of the planned eastbound bayou feeder road bridge at this location is at 3-7’, again several feet below the BFE. We encourage TxDOT, the COH and Harris County Flood Control District (HCFCD) to coordinate and address flood risks at this location.</p> <p>If TxDOT proceeds as planned with the eastbound feeder</p>	<p>TxDOT is currently in coordination with the White Oak Bayou Association to coordinate final design of the A9/A10 detention basin.</p> <p>At this time, additional land acquisition is not proposed.</p> <p>As part of the EIS process, alternatives for the IH 10 improvements project were elevated and the proposed project was selected as the preferred alternative. The elevations and grades of the proposed roadways and bridges were designed to tie-in to existing roadways.</p> <p>City of Houston and HCFCD have reviewed the drainage report and proposed design and stated no objections to the findings. Additional flood risks are reduced by mitigation.</p> <p>Once the proposed roadway project is complete, additional improvements are not proposed. The proposed project is</p>
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		<p>connection to Yale, it will make it extremely difficult and expensive to raise the grade at Yale and/or replace the 80 year old Yale bayou bridge at a higher elevation in the future. Additionally, TxDOT's feeder roads and bridges will pull increased traffic into this high risk flood area. This is a public safety concern as noted earlier in this letter.</p> <p>WOBA asks TxDOT to study the impacts of raising the replacement bayou feeder road bridges just west of Studemont to an elevation above the current low lying flow obstructing bridges. While probably not practical from a traffic engineering perspective to raise the new bridges above the BFE, it may be feasible to raise the bridges 1-2'. This is especially the case for the westbound bridge. There appears to be adequate open space west of the Studemont intersection to raise the approaches for the westbound bridge.</p> <p>WOBA realizes that raising the bridges at this location may require additional mitigation measures. We once again encourage TxDOT to coordinate with the COH and HCFCD to develop long lasting cost effective flood reduction strategies.</p>	<p>not anticipated to increase public safety concerns and has been designed according to TxDOT standards.</p> <p>At this time, additional alternative analysis for the IH 10 roadway project is not proposed.</p>
<p>35.</p>	<p>White Oak Bayou Association</p>	<p>1. General:</p> <p>a. These notes and sketches recommend changes to the schematic plans for detention basins proposed by TxDOT for work associated with improvements to I-10 between Washington and Taylor Streets. The recommended changes leave the basic detention basin design as proposed by TxDOT, but with design modifications that will permit the basins to function as meaningful and useful open space for the surrounding communities, and will provide water quality, recreational and aesthetic benefits to the community at large.</p>	<p>Since the public hearing, TxDOT has continued to meet with the White Oak Bayou Association to coordinate the final design of the detention basins. TxDOT is open to allow for the future development of parks and/or recreational use of the detention basins and is willing to design the basins in a manner that will allow for future recreation.</p>

		<p>b. The design change recommendations assume that TxDOT will do the work that is well within its financial restrictions (and obligations) to perform, and that work that is clearly not a part of TxDOT’s mandate will be done by others at a later date. (See the Partnering suggestions at end of Recommendations).</p> <p>c. The drainage work should be coordinated with:</p> <p>i. The City Stormwater Master Plan (SWMP) to be sure that drainage work being installed as part of this project will match up with the proposed improvements shown in the SWMP. The City should be contributing to this project to increase the detention to a point where the mitigation for these future improvement is already in place when they are implemented. There are significant improvements near the east side of the North Bank Basin, and there are proposed storm drain lines connecting to the existing storm drain under the main lanes of I-10 and ultimately draining through the new TxDOT pump station. It is to mitigate the effects of these proposed improvements that we are recommending that the City contribute the land for Basin area A4-1 and that the City purchase the land for Basin area A6-1;</p> <p>ii. The HCFCD Federal Flood Damage Reduction Project for Buffalo Bayou and Lower White Oak Bayou to be sure the detention basin containment berms are constructed such that they will not limit or constrain possible channel adjustment or widening components at a later date. The project planning is under way and is currently evaluating Project Components.</p> <p>d. Mitigation earthwork volumes need to be carefully calculated using existing topography and the proposed designs as a basis of volume calculations. Areas or volumes currently</p>	<p>Correct; TxDOT funding cannot be used to construct or maintain any recreational features.</p> <p>TxDOT has coordinated with the City of Houston regarding the proposed project. Should the City of Houston require future work in the project area, additional coordination would occur.</p> <p>The proposed project has been coordinated with and reviewed by the HCFCD.</p> <p>Topography and current floodplain boundaries were evaluated as part of the drainage study. Detention volumes within the basins are adequate for mitigation.</p>
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	<p>connected to the floodplain cannot be counted towards mitigation.</p> <p>e. Design recommendations common to all Basins:</p> <p>i. 20' Top berm should be graded to provide a minimum 8' wide maintenance bench (on basin side of berm) with 2% cross-slope designed to accommodate future trails.</p> <p>1. Backslope swales shall be no wider than 12' and no deeper than 3' (4:1 side slopes);</p> <p>2. Backslope drain inlets shall be located to best utilize surrounding natural grades and shall be located frequently enough to limit swale depth to 3' (allows for a 500' run at 0.5% slope, or 1000' between inlets maximum);</p> <p>3. Backslope drain outfalls shall be extended to discharge below bottom feature pool elevation (for safety, for ease of mowing, and to provide aquatic habitat shelter).</p> <p>ii. Basins shall be provided with maintenance and access ramps to the bottom area. These ramps will provide equipment access to the basin bottom, security vehicle access to the bottoms, and will provide a base on which to build future trails by others. Ramps should be a graded bench of 10' wide in the 3:1 side slope with a gradient less than 5% (or a 20:1 slope).</p> <p>iii. Top 12" of detention basin grade (berms, slopes and planted bottom area) should be clayey topsoil with high organic content to support a strong stand of grass and to support reforestation plantings.</p> <p>iv. Reforestation plantings should be installed as shown on the sketches per normal TxDOT Green Ribbon planting</p>	<p>The comments are noted regarding the design recommendations. As noted, TxDOT is continuing to work with the White Oak Bayou Association regarding the final design.</p> <p>A natural ramp is being proposed for maintenance access, which could be developed into trails around the basins.</p> <p>Comment noted. The vegetation planting designed is still being finalized. Proper erosion control measures would be</p>
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	<p>specifications, with the exception that the heavy mulching should be immediately top-seeded so that grass cover and roots will bind the mulch to the soil and prevent it from floating away during a storm event.</p> <p>v. Basins should not be fenced. However, vehicular control is important for safety and in order to restrict motorized vehicles that could damage the basin vegetation. Vehicular control could consist of wooden dome top posts (a TxDOT standard detail) where the basin is open to streets running parallel to the basin. Low, heavy metal pipe guardrails should be used where streets terminate into the basin (for instance at 6th Street).</p> <p>2. North Bank Basin (A7, A9, A10):</p> <p>a. Add a wet bottom pond to the basin in order to get better utilization of the basin bottom capacity, allowing for a flat bottom profile. Adding the wet bottom provides an additional 94,000cf of storage which allows for the creation of the access ramps into the basins.</p> <p>i. Bottom feature pond to have 5:1 banks to 8’ depth;</p> <p>ii. Bottom feature channel to have 5:1 wetland shelves (variable width) and 3:1 lower slopes to minimum 15’ wide bottom at 6’ depth;</p> <p>iii. Provide a bottom feature crossing point for maintenance and future trail access as shown on sketch plan:</p> <p>1. 3- 6’x10’ box culverts (16’ long) side by side.</p> <p>b. Inflow weir:</p>	<p>in place during construction. As soon as possible after construction is complete, exposed soils would be stabilized by revegetation.</p> <p>Fencing is not proposed. Vehicular control and safety will be considered in the final design.</p> <p>The comments are noted regarding the design recommendations. As noted, TxDOT is continuing to work with the White Oak Bayou Association regarding the final design.</p>
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	<ul style="list-style-type: none"> i. Should be designed to accommodate a future trail crossing to meet ADA standards. ii. Solid concrete slope protection, if deemed necessary, should be used only directly in line with bottom of weir; iii. Flanking areas on either side of solid concrete should have open cell cable linked concrete matting with topsoil and grass cover; iv. Minimize visible solid concrete, maximize use of grass-covered erosion control methods wherever possible; v. Outfall headwall and pipe should be relocated to occur at base of inflow weir spillway (incoming flows will clear the outfall area); vi. Toe of spillway and outfall headwall to be designed to allow low-water pedestrian access. c. Add two access ramps to bottom of basin, diagonally opposite from northeast corner towards center of basin and from southwest corner towards center of basin. d. Add one access ramp on south face of south berm to provide access from future White Oak North Bank Trail to future top of berm trail. e. Regrade 7th Street right of way (south side of MKT Trail) to direct sheet flows coming south on Tulane Street into basin (Refer to COH SWMP). i. Construct new culvert under MKT Trail; 	<p>A natural ramp is being proposed for maintenance access, which could be developed into trails around the basins.</p> <p>Recommendations regarding City of Houston projects must be decided by the City.</p>
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	<p>ii. Construct wide swale along south side of right of way (30' wide, 5:1 side slopes, 4' deep at center);</p> <p>iii. Construct backslope inlet and drain at north end of basin and discharging into bottom feature channel;</p> <p>iv. Extreme event spillway from swale to basin shall have open cell cable linked concrete matting with topsoil and grass cover.</p> <p>f. Backslope drain running parallel to Rutland Place shall be combined with street drainage swale so there are not two swales running parallel to one another.</p> <p>g. Backslope drains should not be necessary on top of the south berm (since there is no contributing drainage area to the top of this separation berm).</p> <p>h. Current plans call for filling up to 24' of fill at southwest corner of the Rutland Place and 5th Street rights of way where the natural ground currently falls off into the bayou. It does not seem appropriate to be filling this area when the project is generally intended to offset other losses of floodplain storage capacity. See sketch plan for recommended alternative geometry and grading, which conforms to existing ground within public rights of way.</p> <p>i. Was the option of leaving the existing north bank of the bayou in place considered?:</p> <p>i. This would entail leaving the existing north bank of the bayou in place as the south detention basin berm;</p> <p>ii. Closing off the berm along the southeast corner at the</p>	<p>The project area currently does not extend into White Oak Bayou.</p>
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	<p>existing stormwater outfall,</p> <p>iii. Grading the top of the berm to the correct elevation and adding the inlet weir structure;</p> <p>iv. Closing off the berm along the southeast corner at the existing stormwater outfall;</p> <p>v. This would significantly increase the amount of storage in the off-line portion of the basin and it would reduce the amount of grading required.</p> <p>j. North Basin by the Numbers (all approximate):</p> <p>i. Overall land area: 20.4 acres</p> <p>ii. Storage: approx 300 acre/feet (to berm elevation)</p> <p>iii. Bottom area: 5.7 acres</p> <p>iv. Bottom feature wetted area: xx acres</p> <p>v. Trails (inside ROW): xx lf</p> <p>k. Consider COH participation in enlargement of basin through acquisition of additional properties, as highlighted on the attached maps and designated A6-1. Part of this area was studied by TxDOT at an earlier date and designated A6; we are recommending that all of the highlighted area be acquired before the land is redeveloped.</p> <p>i. The COH SWMP website shows a planned 120” storm drain line replacing an older 84” line running down Yale Street to the bayou. This line could be diverted to the west at the MKT</p>	<p>Additional acquisition of land by TxDOT is not proposed at this time.</p> <p>Recommendations regarding City of Houston projects must be decided by the City.</p>
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	<p>Trail (7th Street) and run into the north end of the detention basin, providing a good base flow for the wet bottom feature and at the same time the detention basin would provide detention to mitigate increases in discharge rates in the larger pipes and will provide important water quality benefits for the base flow.</p> <p>ii. The City could participate in the project by acquiring undeveloped or abandoned industrial land on the east side of the basin (to the alley east of Allston) as well as the A6 property to provide for its share of the basin capacity.</p> <p>iii. This could add about 90 acre/feet to the basin.</p> <p>3. South Bank Basin (A5):</p> <p>a. Design A5 basin as if the A4-1 basin will be built at a future date.</p> <p>b. Remove west inflow weir to save cost and reduce amount of concrete visible from North Shepherd. This small basin will be adequately served by one weir.</p> <p>c. East Inflow weir to be designed to accommodate trail and maintenance crossing to meet ADA standards.</p> <p>i. Solid concrete slope protection only in line with bottom of weir;</p> <p>ii. Flanking areas to have open cell cable linked concrete matting with topsoil and grass cover;</p> <p>iii. Minimize visible solid concrete, maximize use of grass-covered erosion control methods;</p>	<p>The comments are noted regarding the design recommendations. As noted, TxDOT is continuing to work with the White Oak Bayou Association regarding the final design.</p>
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	<p>iv. Outfall headwall and pipe relocated to occur at base of inflow weir spillway (incoming flows will clear the outfall area);</p> <p>v. Toe of spillway and outfall headwall to be designed to allow low-water pedestrian access.</p> <p>d. Backslope drain running parallel to Cornish Street and Patterson shall be combined with street drainage swale so there are not two swales running parallel to one another.</p> <p>e. Backslope drains should not be necessary on top of the north berm, nor along North Shepherd (since there is no contributing drainage area to the top of this separation berm).</p> <p>f. An accessible maintenance ramp shall be installed in the east end of the basin.</p> <p>g. Did TxDOT consider buying some or all of the un-built lots (11) at the “Villas on Shepherd” development to increase the capacity of the project?</p> <p>h. South Basin by the Numbers (all approximate):</p> <p>i. Overall land area: xx acres</p> <p>ii. Storage: approx xx acre/feet (to berm elevation)</p> <p>iii. Bottom area: xx acres</p> <p>iv. Bottom feature wetted area: xx acres</p> <p>v. Trails (inside ROW): xx lf</p>	<p>This area was not evaluated as an alternative due to the new, plotted developments planned for the area.</p>
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	<p>i. Consider COH participation in enlargement of basin through donation of additional properties, as highlighted on the attached maps and designated A4-1. Part of this area was studied by TxDOT at an earlier date and designated A4; we are recommending that all of the highlighted area be used for detention.</p> <p>i. The City of Houston SWMP shows planned improvements to a number of storm drain lines on the south side of I-10 that run into the I-10 storm drain system. This added capacity will mitigate the impact of those improvements;</p> <p>ii. The enlarged basin and wet bottom will also allow for treatment of the stormwater running off the main lanes before it runs into White Oak Bayou.</p> <p>4. Capital and Construction Partnering (for All the Basins):</p> <p>a. TxDOT will perform or construct the following as a part of the basic project:</p> <p>i. Land acquisition, remediation, demolition, etc.</p> <p>ii. Grading and earthwork related to creating the berms, the basins, the access ramps and the wet bottom ponds;</p> <p>iii. Drain inlets and discharge piping and culverts;</p> <p>iv. Vehicular access controls;</p> <p>v. Grass establishment;</p> <p>vi. Reforestation as shown on sketches.</p>	<p>A4 was evaluated, but eliminated due to potential hazardous material concerns.</p> <p>Any floodplain impacts from future City of Houston projects must be mitigated by the City of Houston.</p> <p>Comment noted.</p>
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	<p>b. The City of Houston will have need for mitigation detention as part of its SWMP in this part of the City and the community will be advocating for the City to participate in potential enlargements of these basins. This is not intended to delay the design or construction of the basins in any way and the design recommendations are made with possible future enlargements in mind. Most likely places for expansion are:</p> <p>i. Property between Rutland and Rutland Place that would allow the A9/A10 basin to expand eastward and properties between the Bayou and Yale Street identified as “Danburg Estates” on HCAD maps which would allow the further enlargement of the A9/A10 basin eastward and connect to the TxDOT designated area A6. We recommend combining the A9/A10 basin to the A6 basin by acquisition of the “Danburg Estates” properties;</p> <p>ii. The City of Houston Traffic Operations Patterson Street facility is located ideally to be an expansion of the off-line storage and was early identified as a qualified detention basin site (Basin A4). There have been discussions about the City wanting to move this facility to another location. A basin constructed on this land could provide excellent water quality treatment to discharges from the main lanes and feeder roads of I-10 in addition to buffering the pumped discharges during a major event from the proposed I-10 pump station.</p> <p>c. TIRZ #5 recently extended its boundaries to run up White Oak Bayou and is a likely partner for recreational improvements to the basins. A TIRZ can only fund capital projects and cannot take on maintenance obligations since TIRZ’s all have sunset dates when they cease to exist. Features that could be constructed or installed by TIRZ #5 include:</p>	<p>The City of Houston will be responsible for any required floodplain mitigation from future City projects.</p>
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	<p>i. Trail surfaces;</p> <p>ii. Supplemental special tree and wetlands plantings;</p> <p>iii. Benches;</p> <p>iv. Signage.</p> <p>5. Long Term Maintenance Partnering:</p> <p>a. TxDOT: will own and operate the storm drain lines under its roadways and the pump station that serves the depressed freeway section.</p> <p>b. HCFCD: It is our understanding that TxDOT has asked the HCFCD to pick up the long term maintenance of the detention basins and mitigation areas along the main White Oak Bayou channel. This would involve mowing several times per year and maintenance of the backslope drains and other pipes and culverts.</p> <p>c. HPARD: The City of Houston Parks and Recreation Department maintains trails in many places in the City, including the White Oak Bayou and MKT Trails, and would be the logical entity to adopt and maintain any permanent trail improvements constructed in and around the basins. HPARD might also be responsible for maintaining benches and trash receptacles (if any). HPARD usually supplements the HCFCD mowing cycles with more frequent mowing along the edges of the trail systems where trails are located on HCFCD right of way.</p> <p>d. Heights Association (or other community organization): might formally or informally adopt one or both detention</p>	<p>Future maintenance of any recreational facilities will be the responsibility of the park developer. If a park is not developed (or until one is developed), TxDOT will maintain the vegetation and detention basins.</p>
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		basins. Informal adoption might mean that members of the association would provide ‘eyes on the trail’ for security and maintenance issues, and might provide volunteer labor for special tree or wildflower plantings, trash pickup or other special events. Formal adoption might mean assuming responsibility for special vegetation management of the reforestation areas (such as invasive plant control) and for supplemental mowing, within the basins themselves, above and beyond the infrequent HCFCFCD mowing.	
36.	Val K. Hatley	<p>I am currently living in Nevada and selling my home in Texas. I am seeking a summary report from the Feb 18th public hearing on the I-10 White Oak Bayou project. Specifically, I need to know whether my home is impacted by the outcome before I enter into escrow.</p> <p>Please let me know where I can find the summary report from the hearing.</p>	<p>Additional coordination took place with commenter to determine if home was within the proposed project limits.</p> <p>All persons submitting comments have been added to the distribution list for the proposed project. Notices will be sent once the public hearing summary report is available for review.</p>
37.	Matt Wilcox	<p>Simply:</p> <p>Detention good, low bridges bad.</p> <p>Raise or omit the bridges and start the work on the basins and road.</p>	<p>Comment noted regarding the detention basins.</p> <p>The IH 10 frontage roads project was not the subject of the public hearing. The IH 10 frontage roads, including bridges, were included in the IH 10 roadway widening project. An Environmental Impact Statement was prepared and approved by FHWA for this project. The proposed project is not anticipated to increase the Base Flood Elevation. The bridge roadway grades provided are necessary for safe tie-ins at the existing intersection with Yale Street. The thinnest beam available to meet the span and skew requirements of the crossing were used to minimize impacts to the floodplain. Any impacts to White</p>

			<p>Oak Bayou from the bridges at Yale Street and IH 10 will be mitigated by removal of the timber approach spans of the abandoned railroad trestle bridge over White Oak Bayou, upstream of Yale Street.</p>
38.	Jim Mackey	<p>As an IH-10 user and White Oak Bayou (WOB) watershed resident, I welcome and appreciate the opportunity to once again offer comments regarding the proposed TxDOT IH-10 Detention Pond Project - CSJ 0271-07-292. I support, in concept, the construction of the detention basins, but given the need to spend federal Stimulus Funding dollars on the IH-10 Feeder Road/ Ramp Project - CSJ: 0271-07-242, I feel that TxDOT is moving forward with both the road and detention projects without full coordination with the community and local government entities. While TxDOT must move quickly, updated coordinated designs and planning would result in improved traffic flow, increased traffic safety, reduced flood risks, and overall improvement in quality of life and over the long haul, substantial savings in taxpayer dollars. As such, I join with my fellow citizens in requesting TxDOT act on the following points related to the basins and associated flood risks:</p> <ul style="list-style-type: none"> I urge TxDOT to finalize required environmental studies, secure funding and proceed quickly with the acquisition of the detention basin sites. At the Feb. 18th Public Hearing, it was disappointing to hear a property owner from inside the project confines who had yet to be contacted by TxDOT. 	<p>The IH 10 frontage roads project was not the subject of the public hearing. The IH 10 frontage roads were included as part of IH 10 roadway widening project, which included public involvement. An Environmental Impact Statement was prepared and approved by FHWA for this project. In 2009 and 2010, neighborhood and civic association meetings were held to discuss the frontage road project. Modifications were made to the design based on public comments from these meetings.</p> <p>The public was given the opportunity to comment on the detention basins project through public involvement. Previous public meetings were held on November 29, 2005 and June 8, 2006. A Notice Affording Opportunity for Public Hearing was published in English in the Houston Chronicle on November 19, 2009 and in Spanish in La Voz on November 22, 2009. Requests were received; therefore, the public hearing was held on February 18, 2010. All comments received from the public have been documented, reviewed, and provided responses.</p> <p>TxDOT is in the process of finalizing the environmental study process and requesting a Finding of No Significant Impacts from Federal Highway Administration. This will allow the acquisition of all property. Construction is anticipated to begin in Fall 2010. Follow up coordination with the property owner that spoke at the public hearing determined he lived outside of the project area (south of IH 10) and would not be impacted by the proposed project.</p>

		<ul style="list-style-type: none"> • I strongly request that all detention and mitigation measures be constructed concurrently if not before the feeder road construction that will impact WOB and the drainage of the below grade section of I-10. • TxDOT to address and resolve the hazardous conditions created by having sections of the feeder roads and bridges constructed well below the Base Flood Elevation (BFE). Specifically, there is concern the new eastbound feeder road and Yale/Heights exit ramp will take exiting drivers from the freeway over a rise and then drop them on the bayou feeder road bridge at Yale several feet below the BFE. • The detention basins should be designed and built in such a way as to support the future development of the sites as parks complete with "wet bottoms," native plantings and trails. The White Oak Bayou Assoc. renditions for such a design are a good starting point for this discussion. <p>In order to make more fully informed comments and to gain a broader understanding of potential and possible flood risks related to both the Detention Pond Project - CSJ: 0271-07-292 and the Road Project - CSJ: 0271-07-242, I reviewed TxDOT's 2/09 Drainage Mitigation Study for the I-10 Project, the 2/10 detention pond renditions, the road construction plan sets and the Federal Highway Administration (FHWA) Policy Guide 23.650 pertaining to encroachments in the floodplain. Upon considering the following comments, questions and</p>	<p>The detention basins will be constructed concurrently with the frontage road project. The frontage road project construction schedule is longer than the detention basin construction schedule and therefore, starting first. IH 10 will continue to serve as floodplain storage until the detention pond construction is complete.</p> <p>The proposed bridges were designed to avoid any increase in the Base Flood Elevation, provide the grades necessary for safe tie-ins to existing roadways, and are above the 10-year floodplain. These design features are consistent with standard TxDOT Houston District procedures.</p> <p>TxDOT is currently in the process of working with the White Oak Bayou Association to design the detention basins so that future recreation can be added to the area. A natural ramp is being proposed for maintenance access, which could be developed into trails around the basins. Additionally, proposed fencing was removed.</p>
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	<p>recommendations; my hope is TxDOT will see the need to revisit the drainage study and make modifications to their plans as they relate to detention, bridge design and mitigation of flows on lower WOB. I make the following points acknowledging that TxDOT is not a flood control/reduction agency and that several of the below recommendations will have to be carried out in partnership with others.</p> <ul style="list-style-type: none"> • Regarding TxDOT's 2/09 Drainage Study, the road construction plan sets and specifically the proposed bridge elevations, I find it difficult to understand how TxDOT is compliant with FHWA Floodplain Policy Guide. From the outset in Sec. 650.103 Policy - there are statements that read, "...a broad and unified approach to prevent uneconomic, hazardous or incompatible use of the Nation's floodplains... Too avoid significant encroachments, where practicable... To restore and preserve the natural and beneficial floodplain values that are adversely impacted by highway agency actions." Another key point is made in Sec. 650.115 Design Standards - stating that, "The design selected for an encroachment shall be supported by analyses of design alternatives with consideration given to capital costs and risks, and to other economic, engineering, social and environmental concerns... The design of encroachments shall be consistent with standards established by the FEMA, State and local governmental agencies..." As a private citizen, I find it difficult to fathom a number of TxDOT's design decisions after reading the FHWA words. Given the "social concerns" alone over flooding that is part of the culture of the WOB citizenry, TxDOT needed to avoid designs placing the bridges low in the floodplain. 	<p>The FHWA policy in 23CFR 650 is for "significant encroachment." This is defined as a highway encroachment and any direct support of likely base floodplain development that would involve one or more of the following construction or flood –related impacts: (1) a significant potential for interruption or termination of a transportation facility which is needed for emergency vehicles or provides a community’s only evacuation route; (2) a significant risk, or; (3) a significant adverse impact on natural and beneficial floodplain values. The proposed project is not needed for emergency vehicles or is the only evacuation route, is located above the 10-year floodplain, and would not adversely impact the floodplain; therefore, project does not meet the definition of significant encroachment.</p>
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		<ul style="list-style-type: none"> • While utilizing best practices and technically sound, TxDOT's 2/09 Drainage Study is flawed by the fact that TxDOT didn't consider running simulations with the proposed new and replacement bayou feeder road bridges raised to feasible and practical levels approaching or above the BFE. If run, such modeling simulations would have given TxDOT and the community a solid indication if additional mitigation measures would be needed to offset any additional flows passing under the raised bridges. Sec. 650.113 of the FHWA Policy Guide states that alternatives must be considered and analyzed. Further, the FHWA states in Sec. 650.109 that the public should be afforded the opportunity to comment on alternatives. • I must ask TxDOT to more fully explain how they calculated the capacities of the proposed detention pond facilities. My concern is TxDOT may be unknowingly claiming existing stormwater storage capacity as their own at site A-5. The table at the bottom of page 5-1 in the Drainage Mitigation Study makes no mention of the existing 25ac/ft of stormwater storage at that location. See attached images. • I must encourage TxDOT to secure an agreement with the City of Houston (COH) to abandon Patterson St. north of Cornish and to include that street ROW in an expansion of the A-5 Detention Site. In addition to the benefit of increased detention, removing the Patterson bayou dead-end reduces the likelihood this area will become an illegal dumping ground as so often occurs. Additionally, I suggest TxDOT and/or the COH 	<p>The proposed bridges were designed to avoid any increase in the Base Flood Elevation. As stated, an Environmental Impact Statement was prepared and approved by FHWA for this project. Alternatives were evaluated as part of this process.</p> <p>Required capacity was determined by conducting a hydraulic analysis using the U.S. Army Corps of Engineers program HEC-HMS and HEC-RAS models from the TSARP study.</p> <p>That area was evaluated as A4 in the alternatives analysis, but was dismissed due to potential hazardous material concerns. The recommended alternatives provide the required capacity and no additional land acquisition is required.</p>
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		<p>purchase the undeveloped townhome lots along the north side of Cornish for further expansion of the A-5 site.</p> <ul style="list-style-type: none"> • In conjunction with proposals for "park ready" wet bottom detention sites, I encourage TxDOT and the COH to coordinate on the A9-A10 design and construction. If city drainage were to be rerouted to provide semi-permanent flows into the site, additional detention capacity may be required. The adjoining city owned MKT ROW south of the hike-n-bike trail could be sculpted to provide additional stormwater storage. • There is more than 200' of open land between Yale and the bayou on the westbound feeder ROW that could be utilized to raise the approach for the westbound bayou feeder bridge at Yale. By bringing the elevation of the approach up, the bridge itself could be raised 2-3'. • Yale St. I-10 and the nearby Yale bayou bridge lay at 42' which is 2-7' below the BFE at this location. The lower chord of the planned eastbound bayou feeder road bridge at this location is at 37', again several feet below the BFE. We encourage TxDOT, the COH and Harris County Flood Control District (HCFCD) to coordinate and address flood risks at this location. If TxDOT proceeds as planned with the eastbound feeder connection to Yale, they will make it extremely difficult and expensive in the future to raise the grade at Yale and/or replace the 80 year old Yale bayou bridge at a higher elevation. Additionally, TxDOT's feeder roads and bridges will pull increased traffic into 	<p>TxDOT has coordinated with the City of Houston regarding the proposed project. Any City drainage projects will be the responsibility of the City of Houston.</p> <p>As part of the Environmental Impact Statement review process, alternatives for the IH 10 improvements project were elevated and the proposed project was selected as the preferred alternative. The elevations and grades of the proposed roadways and bridges were designed to tie-in to existing roadways.</p> <p>The City of Houston and HCFCD have reviewed the proposed plans and drainage study for the proposed project and had no objections to the findings. Additional flood risks are reduced by mitigation. Once the proposed roadway project is complete, additional improvements are not proposed. The proposed project is not anticipated to increase public safety concerns and has been designed according to TxDOT standards.</p>
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		<p>this high risk flood area. This is a public safety concern as noted earlier in this letter.</p> <ul style="list-style-type: none"> I ask TxDOT to study the impacts of raising the replacement bayou feeder road bridges just west of Studemont to an elevation above the current low lying flow obstructing bridges. While probably not practical from a traffic engineering perspective to raise the new bridges above the BFE, it may be feasible to raise the bridges 1-2', especially in the case of the westbound bridge where there appears to be adequate room west of the Studemont intersection to raise the approaches for the westbound bridge. I realize that raising the bridges at this location may require additional mitigation measures. Again, I encourage TxDOT to coordinate with the COH and HCFCFCD in developing long lasting cost effective flood reduction strategies. <p>I appreciated the opportunity to once again offer comments and thoughts regarding the proposed TxDOT I-10 Detention Pond Project at the Public Hearing. I join with my fellow citizens in urging TxDOT to truly consider our input and to refine their plans for the benefit of their department, IH-10 drivers and the community as a whole. The Public Hearing was another step in working towards that goal as we move forward on formulating a project we all can live with and be proud of.</p>	<p>Additional studies are not proposed at this time. The proposed project will not increase the Base Flood Elevation and there will be no increase in flood elevations upstream or downstream of IH 10.</p> <p>TxDOT is considering all public comments in formulation of the final design for the proposed project.</p>
39.	Washington Avenue Coalition - Memorial Park	<p>On behalf of the Washington Avenue Coalition/Memorial Park Super Neighborhood (SN 22) community, I urge you and your agency, please, to ensure (1) that the IH 10 at White Oak Bayou Detention Basins are constructed as "park-ready," multi-purpose assets built prior to or contemporaneously with the construction of the IH 10 frontage road projects, and (2)</p>	<p>(1) TxDOT is currently in the process of working with local agencies to determine construction and design methods which will allow for future recreational uses of the basins. TxDOT funding cannot be used to construct the parks; however, TxDOT is open to allow for the future development of parks and/or recreational use of</p>

		<p>that any and all bridges built as part of either the IH 10 frontage road project or the detention basins project be designed and built above the base flood elevation.</p> <p>Because, together, these two projects, i.e., the frontage road project and the detention basins project, area intended to displace storm water holding capacity of the main lanes of IH 10 between the IH 610 loop on the west and IH 45 on the east, property owners (residents and businesses, alike) in the adjacent SN 22 neighborhoods are likely to be severely and adversely impacted if the detention basins are not constructed and operational BEFORE the frontage road construction is completed and operational.</p> <p>In addition, SN 22 joins in and supports the requests already made by other community organizations (e.g., White Oak Bayou Association, Citizens Transportation Coalition, Bayou Preservation Alliance and Greater Heights Super Neighborhood (SN 15)), urging you and your agency to work</p>	<p>the detention basins. The detention basins will be constructed concurrently with the frontage road project. The frontage road project construction schedule is longer than the detention basin construction schedule and therefore, starting first. IH 10 will continue to serve as floodplain storage until the detention pond construction is complete.</p> <p>(2) The proposed project is not anticipated to increase the Base Flood Elevation. The bridge roadway grades provided are necessary for safe tie-ins at the existing intersection with Yale Street. The thinnest beam available to meet the span and skew requirements of the crossing were used to minimize impacts to the floodplain. Any impacts to White Oak Bayou from the bridges at Yale Street and IH 10 will be mitigated by removal of the timber approach spans of the abandoned railroad trestle bridge over White Oak Bayou, upstream of Yale Street.</p> <p>Comment noted.</p> <p>Coordination is continuing with the White Oak Bayou Association and local officials regarding the final design of the proposed project. The City of Houston and HCFCD have both reviewed the proposed plans for the project.</p>
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	<p>closely and cooperatively with the surrounding communities and with other effected governmental entities including, especially, the City of Houston and the Harris County Flood Control District, to ensure that the detention basins are designed and built in such a way that they can be readily up-graded for use as public park and green space akin to the Willow Waterhole in southwest Houston.</p> <p>The SN 22 Council, along with its constituent neighborhoods and businesses, looks forward to developing a close working relationship with you and your agency to achieve the goals outlined above with respect to the IH-10 at White Oak Bayou Detention Basins.</p>	<p>Comments or questions regarding the proposed project can be sent to HOU-PIOWEBMAIL@DOT.STATE.TX.US or by calling Dr. Stanley Cooper at 713-802-5244.</p>
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WHITE OAK BAYOU FLOODING

TxDOT hearing will detail proposed mitigation

By **ROBIN FOSTER CHRONICLE CORRESPONDENT**

Feb. 12, 2010, 4:46PM

Heights-area residents and civic groups hope to learn details about Texas Department of Transportation's plans to mitigate flooding on Interstate 10 at White Oak Bayou during a public hearing Feb. 18, at the Reagan High School auditorium, 413 E. 13th.

TxDOT is proposing to build two detention ponds and other drainage improvements along the bayou in conjunction with a separate feeder road project through the area.

Six sites are suggested for the ponds along a section of White Oak Bayou from Shepherd Drive to Yale Boulevard. The flood mitigation project also would remove a railroad bridge that spans White Oak Bayou upstream of Yale Boulevard.

According to TxDOT, the project would require nearly 30 acres of additional right-of-way and would displace 24 homes and two businesses. Maps and exhibits will be displayed prior to the hearing from 6 to 7 p.m., and TxDOT personnel will be available to discuss the right-of-way acquisition process. The hearing is scheduled to begin at 7 p.m.

Planning for the project began after Tropical Storm Allison drenched the White Oak Bayou watershed and caused extreme flooding that

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shut down I-10. The public reviewed potential pond sites in a hearing in 2006. Thursday's hearing will present findings of the preliminary engineering and environmental studies that have been completed since then.

Some see the mitigation ponds as an opportunity to create new park space for the redeveloping neighborhoods.

But a separate plan to improve and extend the I-10 feeder roads from Washington Avenue to Taylor Street has raised concerns that neighborhood flooding could worsen along the bayou. As a result, the city of Houston is reviewing the feeder road project to verify whether bridge elevations comply with its floodplain regulations. State Rep. Jessica Farrar-D, Houston, has pledged also that citizens' concerns about the project will be answered.

Jim Mackey, vice president of the White Oak Bayou Association, said a drainage study done by TxDOT doesn't show whether flooding impacts from the feeder road project are being mitigated properly.

"What has become apparent to those who have looked closely at TxDOT's drainage study is that the low bridges are part of the mitigation plan to hold White Oak Bayou water back in the Yale and Studemont areas to protect downtown from additional flooding," Mackey said. "TxDOT will argue that they are only maintaining current conditions and will not make matters worse with

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the low bridges. The community would prefer not to take that gamble with the low bridges.”

When the feeder road project was first proposed in 2003, residents of the area objected to elevating the roads.

A diagram of the mitigation pond sites, environmental and other studies are available for review at TxDOT's Houston District Office, 7600 Washington Ave.

People with special access or communication needs, including an interpreter, should contact TxDOT at 713-802-5072 at least two days before the hearing.

Written comments on the flood mitigation ponds must be postmarked by March 4. They can be mailed to the director of project development at P.O. Box 1386, Houston, TX, 77251-1386, or submitted via e-mail to hou-piowebmail@dot.state.tx.us.

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TxDot ready to extend feeder roads along Interstate 10

By Robin Foster Chronicle Correspondent

Feb. 23, 2010, 2:41PM

Heights-area residents reiterated their concerns about the Texas Department of Transportation's plans to extend feeder roads on Interstate 10 during a public hearing Feb. 18 about a related project to build two detention ponds on flood-prone White Oak Bayou.

Residents want assurances both projects will be completed simultaneously to ease concerns that the feeder road project could worsen neighborhood flooding during heavy rains. They're also asking that the detention ponds serve as the basis for future parks and trails, an idea elected officials said they also support.

"Leave the beds ready and we'll come back and plant the flowers later," said state Rep. Jessica Farrar.

"I ask our elected officials to review everything with the Houston Heights Association and the White Oak Bayou Association to ensure this protects the neighborhoods and not just the freeway," said Ken Culotta, one of 10 citizens who spoke.

The projects were conceived after Tropical Storm Allison caused severe flooding that shut down I-10. The feeder road project would cut off the bayou from overflowing onto I-10 during a 1 percent storm event. The mitigation ponds would receive the overflow instead, storing about 420.

Thursday's hearing sought public input on preliminary engineering and environmental studies of the pond sites, located north of the highway along a .6-mile section of the bayou

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between Shepherd Drive and Yale. The cost for the ponds is estimated at around \$9 million and construction could begin this fall. Nearly 30 acres of additional right-of-way is needed, impacting 11 residences and 2 commercial properties. Instead, residents continued to raise concerns about the feeder roads, which some said are not needed.

Houston Heights Association President Anne Culotta said the feeder road project "could generate increased congestion or risk of flooding in our historic neighborhood."

TxDOT's last public hearing on the feeder roads was in 2003. The agency resurrected the project in order to take advantage of federal stimulus dollars before the funding expires. By the time local leaders arranged a meeting on the plans, TxDOT announced the contract already had been let. Balfour Beatty Infrastructure Inc. got the award with a low bid of \$48.6 million.

TxDOT officials met with representatives of the White Oak Bayou Association earlier in the week to explore the idea of building the basins so they can be used as green space later.

But WOBA still seeks further evidence that the proposed feeder road improvements between Washington and Taylor Streets won't contribute to neighborhood flooding upstream.

"We're somewhat troubled that TxDOT didn't run any simulations showing the bridges at higher levels," said Jim Mackey, WOBA vice president. "We encourage you to raise the levels and see if additional mitigation is needed."

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