

STATE OF TEXAS
DEPARTMENT OF TRANSPORTATION

PLANS OF PROPOSED
STATE HIGHWAY IMPROVEMENT

PROJECT NO. STP ()

CONTROL ROADWAY

MONTGOMERY CO. CSJ 1400-04-035 = 12,251.83 FT. = 2.32 MI.

NET LENGTH OF PROJECT 1400-04-035 = 12,251.83 FT. = 2.32 MI.

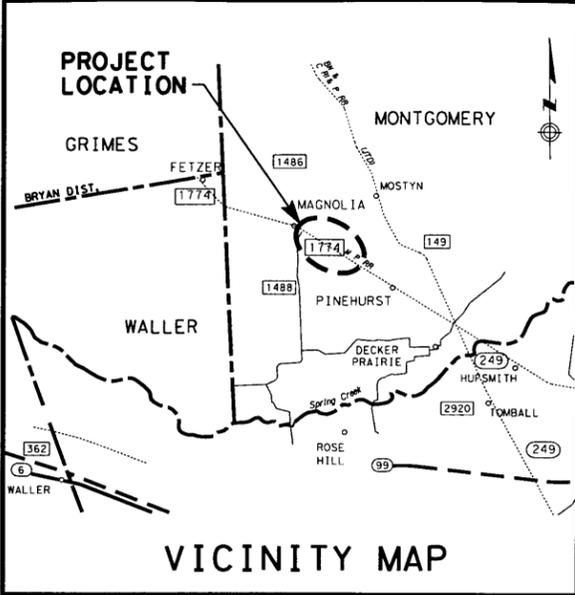
MONTGOMERY COUNTY
FM 1774

FOR THE CONSTRUCTION OF: THE WIDENING OF A NON-FREEWAY FACILITY

CONSISTING OF: GRADING, LIME TREATED SUBGRADE, CEMENT TREATED BASE, BOND BREAKER, CONTINUOUSLY REINFORCED CONCRETE PAVEMENT, STORM SEWERS, SIGNS, PAVEMENT MARKINGS, TRAFFIC SIGNALS

LIMITS: FROM 576.50 FT (RRMP 21.56) NORTH OF FM 1488 TO 237.60 FT (RRMP 21.08) SOUTH OF W. LOST CREEK BLVD.

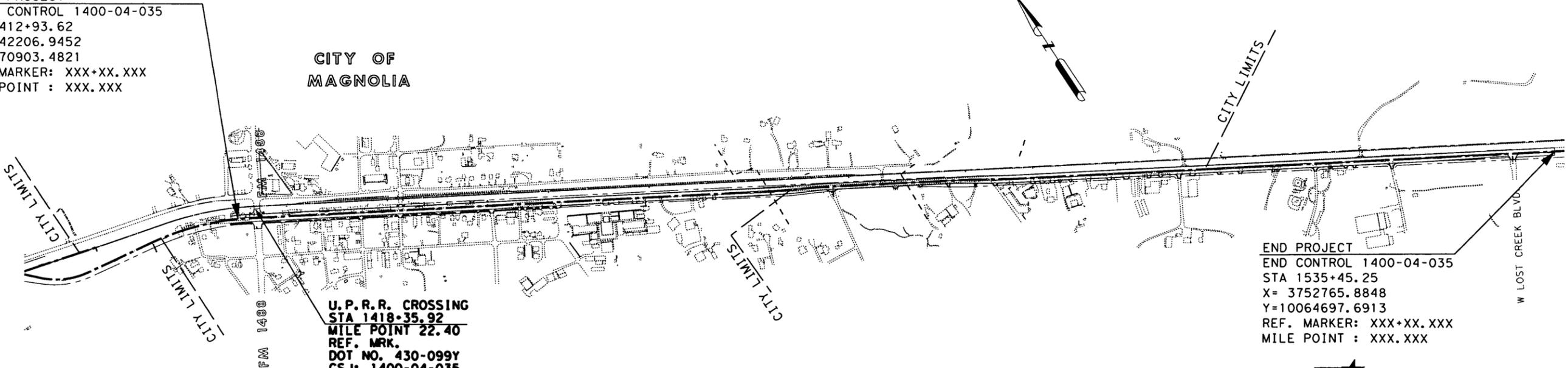
STATE	TEXAS	FEDERAL AID PROJECT NO.	
COUNTY	MONTGOMERY	CONTROL NO.	1400 04 035
SECTION NO.		JOB NO.	1
DWG FILE NAME: FM1774+1+1e 2.32MILES NEW.dgn			



COUNTY/CSJ	DESIGN SPEED	ADT. (2011)	ADT. (2041)
MONTGOMERY/1400-04-035	45 MPH	14,600	27,100

BEGIN PROJECT
BEGIN CONTROL 1400-04-035
STA 1412+93.62
X= 3742206.9452
Y=10070903.4821
REF. MARKER: XXX+XX.XXX
MILE POINT : XXX.XXX

CITY OF
MAGNOLIA



U.P.R.R. CROSSING
STA 1418+35.92
MILE POINT 22.40
REF. MRK.
DOT NO. 430-099Y
CSJ: 1400-04-035

END PROJECT
END CONTROL 1400-04-035
STA 1535+45.25
X= 3752765.8848
Y=10064697.6913
REF. MARKER: XXX+XX.XXX
MILE POINT : XXX.XXX

COUNTY MONTGOMERY PROJ. NO. STP ()
HWY. NO. FM 1774 LETTING DATE JULY 2011
DATE ACCEPTED

LAYOUT MAP
LAYOUT SCALE: 1" = 1000'

EQUATIONS : NONE
EXCEPTIONS : NONE
RR CROSSING : NONE

NOTES:
HORIZONTAL CONTROL
ALL BEARINGS AND COORDINATES SHOWN ARE BASED ON THE TEXAS STATE PLANE COORDINATE SYSTEM, CENTRAL ZONE, NORTH AMERICAN DATUM OF 1983 (NAD83), 1993 ADJUSTMENT.

ALL DISTANCES AND COORDINATES ARE SURFACE DATUM. AND MAY BE CONVERTED TO GRID BY MULTIPLYING BY A COMBINED ADJUSTMENT FACTOR OF 0.999970.

VERTICAL CONTROL
VERTICAL CONTROL IS REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88), 1991 ADJUSTMENT.

SPECIFICATIONS ADOPTED BY THE TEXAS DEPARTMENT OF TRANSPORTATION, JUNE 1, 2004, AND THE SPECIFICATION ITEMS LISTED AND DATED AS FOLLOWS SHALL GOVERN ON THIS PROJECT: REQUIRED CONTRACT PROVISIONS FOR ALL FEDERAL-AID CONSTRUCTION CONTRACTS (FORM FHWA 1273, MARCH, 1994).

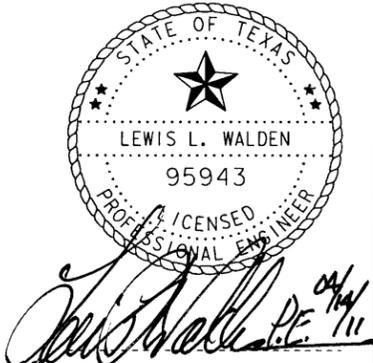
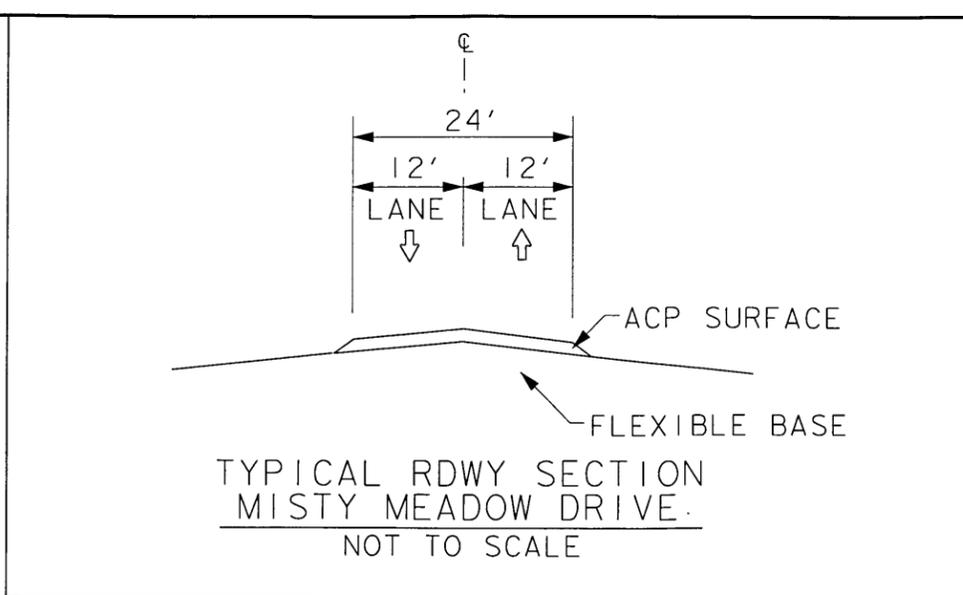
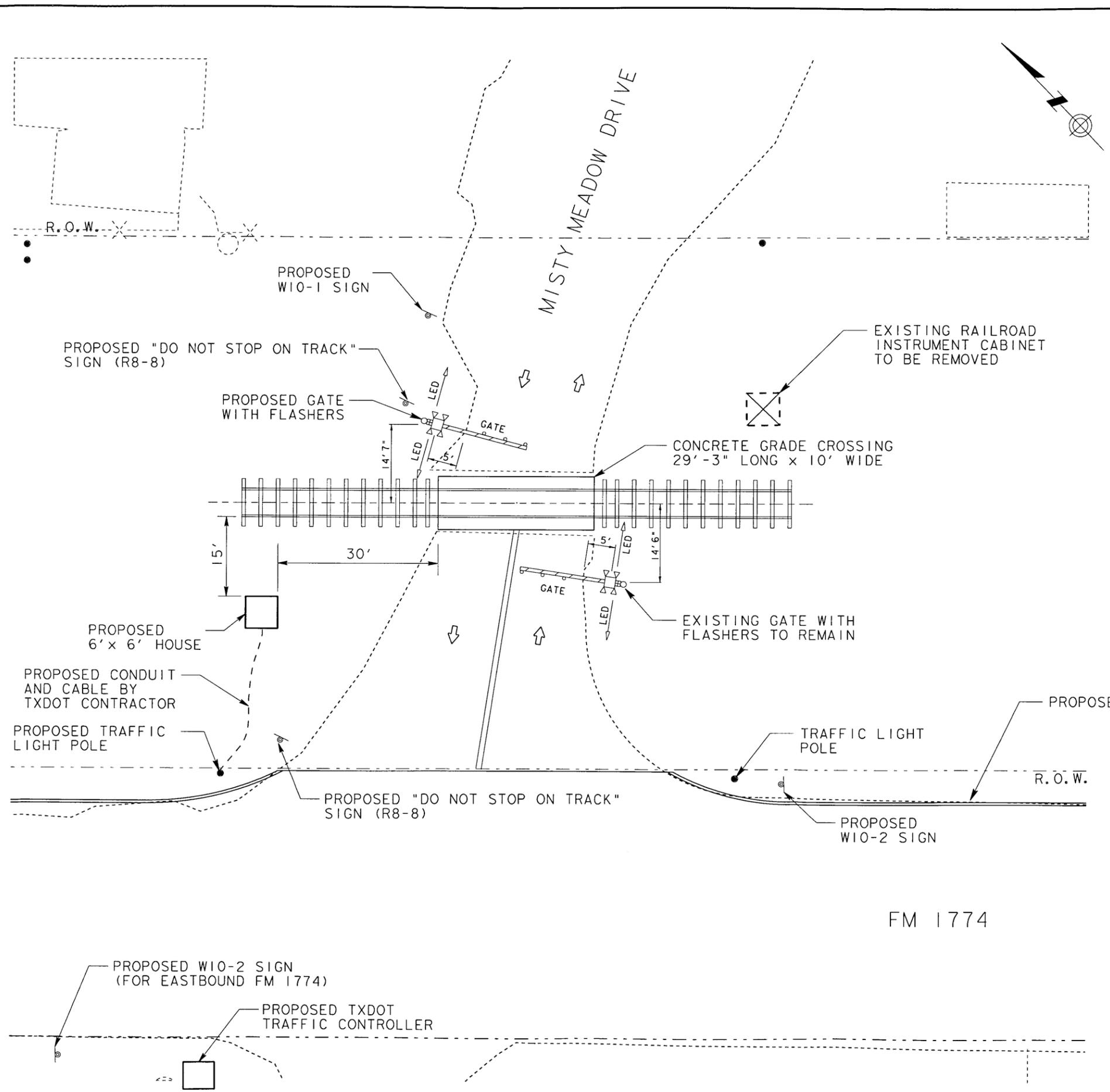
CITY OF MAGNOLIA
CONCURRENCE
CITY MANAGER

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TEXAS DEPARTMENT OF TRANSPORTATION

SUBMITTED FOR LETTING
SUPERVISING DESIGNING ENGINEER
RECOMMENDED FOR LETTING
DISTRICT ENGINEER
APPROVED FOR LETTING
DIRECTOR, TRAFFIC OPERATIONS DIVISION
APPROVED FOR LETTING
DIRECTOR, DESIGN DIVISION

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- NOTES:
- UNLESS INDICATED OTHERWISE INFORMATION SHOWN IS EXISTING.
 - SEE SIGNING AND PAVEMENT MARKING LAYOUTS.

FM 1774

 TEXAS DEPARTMENT OF TRANSPORTATION EXHIBIT 'A' - DETAILS UPRR GRADE CROSSING MISTY MEADOW DRIVE AT FM 1774 UPRR, DOT 430 090M, RRMP 19,23 NAVASOTA SUBDIVISION						
DRAWING FILE NAME:	FED. RD. DIV. RD.	STATE	PROJECT NO.			HIGHWAY
ORIGINAL DATE OF DRAWING:	6	TX				FM 1774
REVISIONS:	DISTRICT	COUNTY	CONTROL	SECT	JOB	SHEET NO.
	HOU	MONTGOMERY	1400	04	035	1

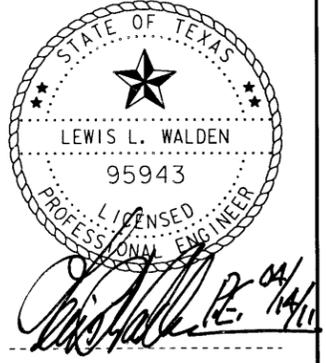
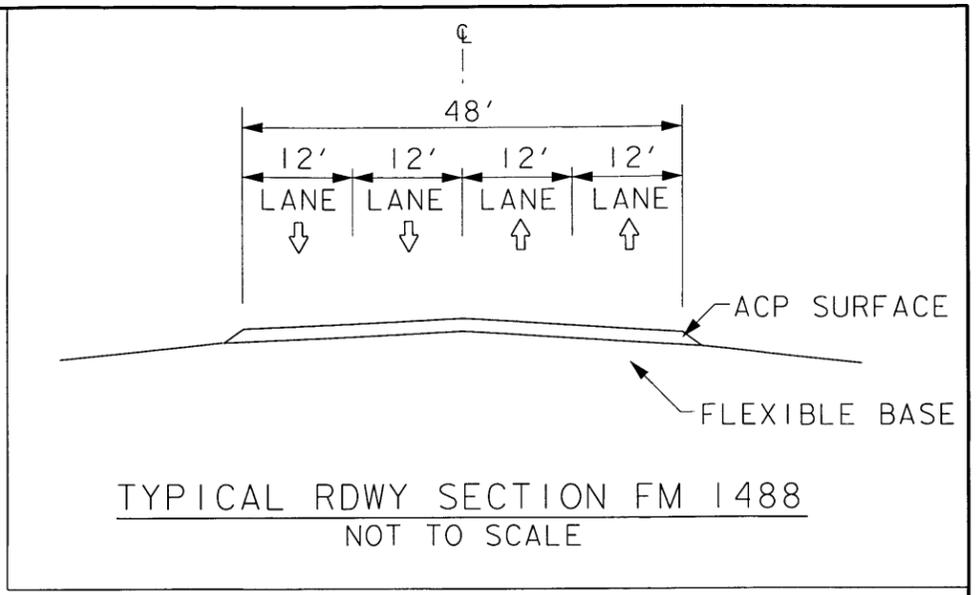
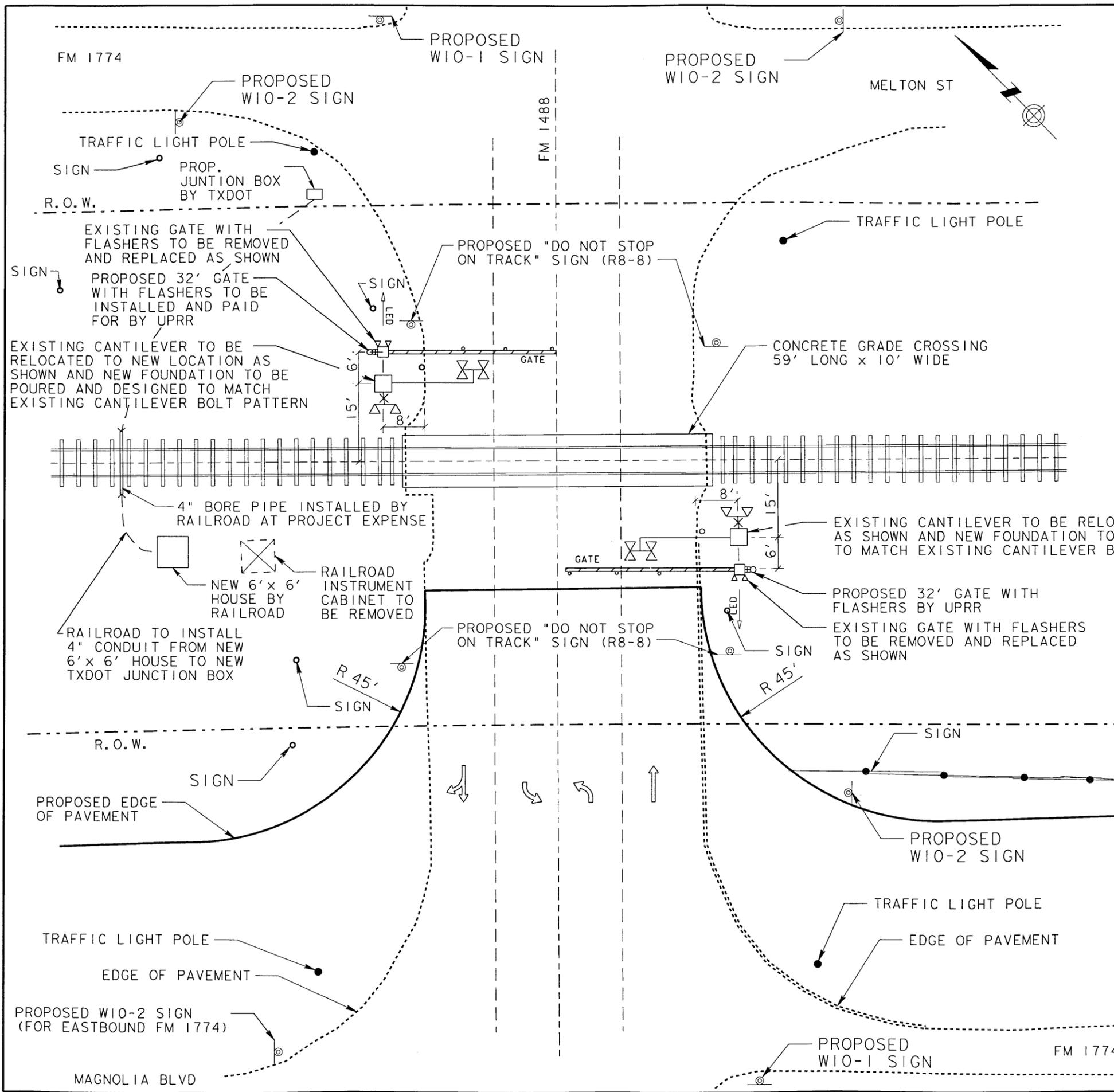
GENERAL NOTES

1. SIGNAL CIRCUITS ARE DESIGNED TO GIVE 20 SECONDS MINIMUM WARNING TIME, PLUS 0 SECONDS CLEARANCE TIME, PLUS 5 SECONDS BUFFER TIME, PLUS 5 SECONDS EQUIPMENT RESPONSE TIME, PLUS 40 SECONDS ADVANCE PREEMPTION TIME, FOR A TOTAL OF 70 SECONDS APPROACH TIME, PRIOR TO THE ARRIVAL OF THE FASTEST TRAIN AT THIS CROSSING. REFER TO SIGNAL LAYOUT FOR TOTAL APPROACH TIME.
2. PROPOSED CONSTANT WARNING CIRCUITS ARE TO BE USED AT THIS LOCATION. DAXING TO BLOCK SIGNAL IN THE WEST APPROACH FOR CIRCUIT UPGRADE.
3. CONDUIT, FILL DIRT AND CRUSHED COVER ROCK TO BE FURNISHED IN PLACE BY THE RAILROAD COMPANY OR ITS CONTRACTOR AT STATES EXPENSE.
4. THE RAILROAD COMPANY OR ITS CONTRACTOR WILL REMOVE THE EXISTING GATE AND DISPOSE OF THE FOUNDATION.
5. THE STATE'S CONTRACTOR WILL FURNISH AND INSTALL OR REPLACE THE APPROPRIATE PAVEMENT MARKINGS AS OUTLINED ON THE ATTACHED LAYOUT AND STANDARD SHEET AND IN ACCORDANCE WITH THE GUIDELINES IN THE TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
6. THE STATE'S CONTRACTOR WILL FURNISH AND INSTALL OR REPLACE THE FOLLOWING SIGNS IN ACCORDANCE WITH THE GUIDELINES IN THE TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (TMUTCD) AND THE STANDARD HIGHWAY SIGN DESIGNS MANUAL FOR TEXAS (SHSD): 1 EA. (W10-1), 2 EA. (W10-2), 2 EA. (R15-4). 2 EA. (R8-8) ("DO NOT STOP ON TRACK").
7. THE STATE AGREES TO MAINTAIN THE PAVEMENT MARKINGS AND ADVANCE WARNING SIGNS PLACED ALONG THE ROADWAYS UNDER THEIR JURISDICTION IN ACCORDANCE WITH THE GUIDELINES IN THE TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND AS SHOWN ON THE LAYOUT AND STANDARD SHEETS AS ACKNOWLEDGED ON THE TITLE SHEET.
8. THE RAILROAD COMPANY OR ITS CONTRACTOR SHALL FURNISH, INSTALL AND MAINTAIN SIGN MOUNTING BRACKETS FOR THE REPORT SIGN (R15-4) AT THE STATES EXPENSE.
9. THE RAILROAD COMPANY OR ITS CONTRACTOR SHALL STENCIL THE DOT-ARR NUMBERS ON THE SIGNAL MASTS FACING THE ADJACENT ROADWAY IN THE 2" BLACK LETTERING.
10. THE RAILROAD COMPANY OR ITS CONTRACTOR WILL PROVIDE TRAFFIC CONTROL IN ACCORDANCE WITH THE GUIDELINES IN THE TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
11. THE RAILROAD OR ITS CONTRACTOR WILL FURNISH AND INSTALL A RELAY TO PROVIDE ADVANCE PREEMPTION TO THE PROPOSED TRAFFIC SIGNAL. NORMALLY A CLOSED CIRCUIT IS REQUIRED BETWEEN THE CONTROL RELAY OF THE GRADE CROSSING WARNING DEVICE AND THE TRAFFIC SIGNAL CONTROLLER OR FLASHER AS STATED IN THE TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
12. THE STATE'S CONTRACTOR SHALL INSTALL 35', 1#12/4C AND CONDUIT BETWEEN RAILROAD INSTRUMENT CASE AND TRAFFIC CONTROLLER AS SHOWN ON LAYOUT.

DESCRIPTION OF PROJECT

- 1 COMPLETE FLASHING LIGHT WITH GATE ASSEMBLY AND 1 BELL
- 12" LAMP HOUSING SHALL BE USED AND EQUIPPED WITH LED'S (LIGHT EMITTING DIODES) LIGHTS, OPERATED AT NOT LESS THAN 8.5 VOLTS UNDER NORMAL OPERATING CONDITIONS.

		TEXAS DEPARTMENT OF TRANSPORTATION EXHIBIT 'A' - GENERAL NOTES UPRR GRADE CROSSING MISTY MEADOW DRIVE AT FM 1774 UPRR, DOT 430 090M, RRMP 19,23 NAVASOTA SUBDIVISION				
		DRAWING FILE NAME:	FED. RD. DIV. RD.	STATE	PROJECT NO.	HIGHWAY
ORIGINAL DATE OF DRAWING:	6	TX			FM 1774	
REVISIONS:	DISTRICT	COUNTY	CONTROL	SECT	JOB	SHEET NO.
	HOU	MONTGOMERY	1400	04	035	2



NOTES:
 1. UNLESS INDICATED OTHERWISE INFORMATION SHOWN IS EXISTING.
 2. SEE SIGNING AND PAVEMENT MARKING LAYOUTS.

NTS

		TEXAS DEPARTMENT OF TRANSPORTATION				
		EXHIBIT 'A' - DETAILS				
		UPRR GRADE CROSSING				
		FM 1488 AT FM 1774				
		UPRR, DOT 430 099Y, RMP 22,43				
		NAVASOTA SUBDIVISION				
DRAWING FILE NAME:	FED. RD. DIV. RD.	STATE	PROJECT NO.		HIGHWAY	
ORIGINAL DATE OF DRAWING:	6	TX			FM 1774	
REVISIONS:	DISTRICT	COUNTY	CONTROL	SECT	JOB	
	HOU	MONTGOMERY	1400	04	035	
					SHEET NO. 1	

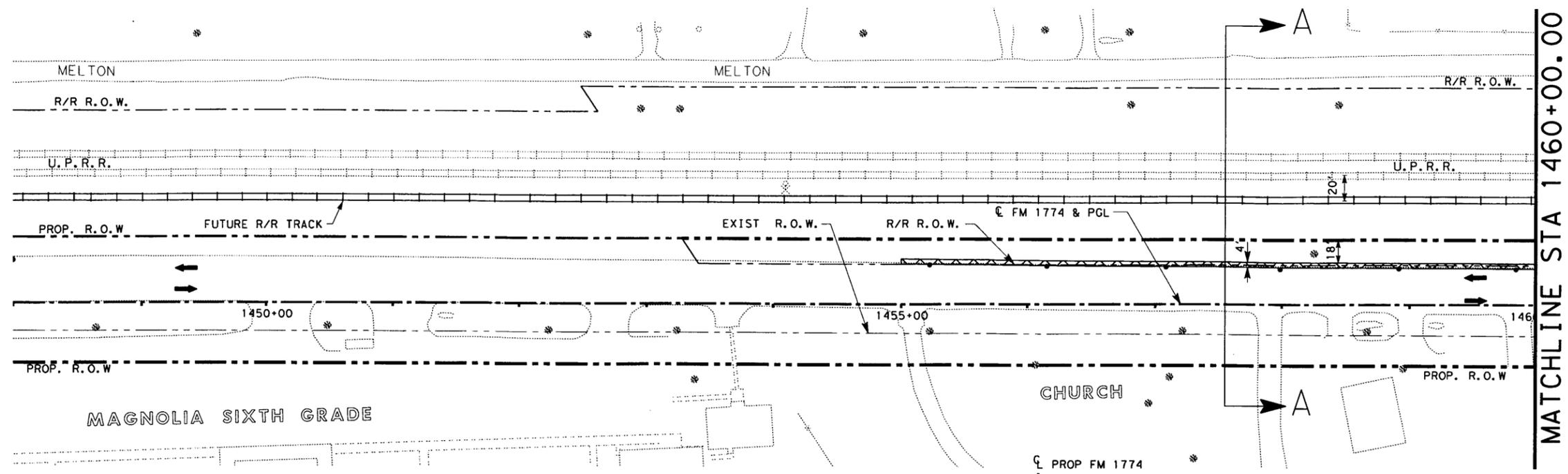
GENERAL NOTES

1. SIGNAL CIRCUITS ARE DESIGNED TO GIVE 20 SECONDS MINIMUM WARNING TIME, PLUS 1 SECONDS CLEARANCE TIME, PLUS 5 SECONDS BUFFER TIME, PLUS 5 SECONDS EQUIPMENT RESPONSE TIME, PLUS 42 SECONDS ADVANCE PREEMPTION TIME, FOR A TOTAL OF 73 SECONDS APPROACH TIME, PRIOR TO THE ARRIVAL OF THE FASTEST TRAIN AT THIS CROSSING. REFER TO SIGNAL LAYOUT FOR TOTAL APPROACH TIME.
2. PROPOSED CONSTANT WARNING CONTROL CIRCUITS ARE TO BE USED AT THIS LOCATION.
3. CONDUIT, FILL DIRT AND CRUSHED COVER ROCK TO BE FURNISHED IN PLACE BY THE RAILROAD COMPANY OR ITS CONTRACTOR AT STATES EXPENSE.
4. THE STATE'S CONTRACTOR WILL FURNISH AND INSTALL OR REPLACE THE FOLLOWING SIGNS IN ACCORDANCE WITH THE GUIDELINES IN THE TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (TMUTCD) AND THE STANDARD HIGHWAY SIGN DESIGNS MANUAL FOR TEXAS (SHSD): 2 EA. (W10-1), 4 EA. (W10-2), 2 EA. (R15-4), 4 EA. (R8-8) ("DO NOT STOP ON TRACKS").
5. THE STATE AGREES TO MAINTAIN THE PAVEMENT MARKINGS AND ADVANCE WARNING SIGNS PLACED ALONG THE ROADWAYS UNDER ITS JURISDICTION IN ACCORDANCE WITH THE GUIDELINES IN THE TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND AS SHOWN ON THE LAYOUT AND STANDARD SHEETS AS ACKNOWLEDGED ON THE TITLE SHEET.
6. THE RAILROAD COMPANY OR ITS CONTRACTOR SHALL FURNISH, INSTALL AND MAINTAIN SIGN MOUNTING BRACKETS FOR THE REPORT SIGN (R15-4) AT THE STATES EXPENSE.
7. THE STATE AGREES TO TRIM AND MAINTAIN TREES AND VEGETATION FOR ADEQUATE VISIBILITY OF THE CROSSING SIGNALS AND ADVANCE WARNING SIGNS AS ACKNOWLEDGED ON THE TITLE SHEET.
8. THE RAILROAD COMPANY OR ITS CONTRACTOR WILL PROVIDE TRAFFIC CONTROL IN ACCORDANCE WITH THE GUIDELINES IN THE TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
9. THE RAILROAD OR ITS CONTRACTOR WILL FURNISH AND INSTALL A RELAY TO PROVIDE ADVANCE PREEMPTION TO PROPOSED TRAFFIC SIGNAL CONTROLLER. NORMALLY A CLOSED CIRCUIT IS REQUIRED BETWEEN THE CONTROL RELAY OF THE GRADE CROSSING WARNING DEVICE AND THE TRAFFIC SIGNAL CONTROLLER AS STATED IN THE TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
10. THE STATE'S CONTRACTOR SHALL INSTALL 90', 1-#12/4C CABLE BETWEEN RAILROAD INSTRUMENT CASE AND TRAFFIC CONTROLLER AS SHOWN ON LAYOUT.
11. THE RAILROAD SHALL PROVIDE 4" PVC CONDUIT FROM NEW HOUSE TO TXDOT'S JUNCTION BOX AS SHOWN ON LAYOUT, AT PROJECT EXPENSE.

DESCRIPTION OF PROJECT

- 2 PROPOSED FLASHING LIGHTS WITH GATE ASSEMBLIES WITH 2 BELLS TO BE INSTALLED AS SHOWN ON LAYOUT. NEW FLASHING LIGHTS AND GATE IN THE NORTHWEST QUADRANT TO BE INSTALLED AT UPRR EXPENSE.
- EXISTING 12" LAMP HOUSINGS SHALL BE USED.
- 2 EXISTING CANTILEVERS TO BE RELOCATED TO NEW LOCATION AS SHOWN AND NEW FOUNDATIONS SHALL BE POURED TO ACCOMODATE EXISTING CANTILEVER BASE BOLT PATTERN.

		TEXAS DEPARTMENT OF TRANSPORTATION EXHIBIT 'A' - GENERAL NOTES UPRR GRADE CROSSING FM 1488 AT FM 1774 UPRR, DOT 430 099Y, RRMP 22, 43 NAVASOTA SUBDIVISION					
		DRAWING FILE NAME:	FED. RD. DIV. RD.	STATE	PROJECT NO.		HIGHWAY
ORIGINAL DATE OF DRAWING:		6	TX			FM 1774	
REVISIONS:		DISTRICT	COUNTY	CONTROL	SECT	JOB SHEET NO.	
		HOU	MONTGOMERY	1400	04	035 2	



LEGEND

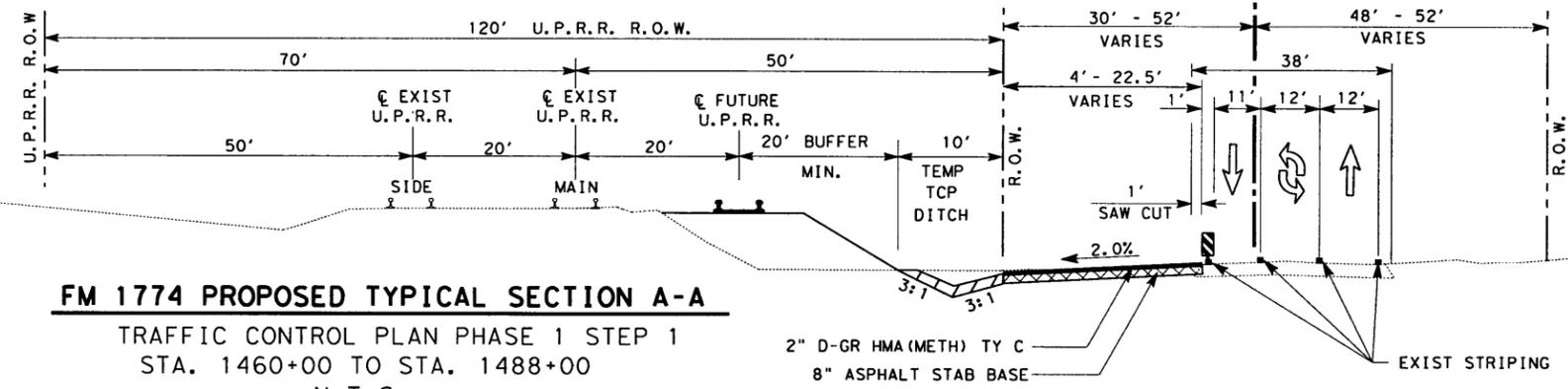
- TRAFFIC LANE (GENERAL USE)
- Base line
- - - PROPOSED ROW
- - - EXISTING ROW
- ||| CONTROL OF ACCESS LINE
- DITCH & FLOW DIRECTION
- TEMP JOINT-USE DITCH W/ RR
- TEMPORARY ASPHALT PAVEMENT
- PROPOSED CONCRETE PAVEMENT



MAGNOLIA SIXTH GRADE

CHURCH

MATCHLINE STA 1460+00.00

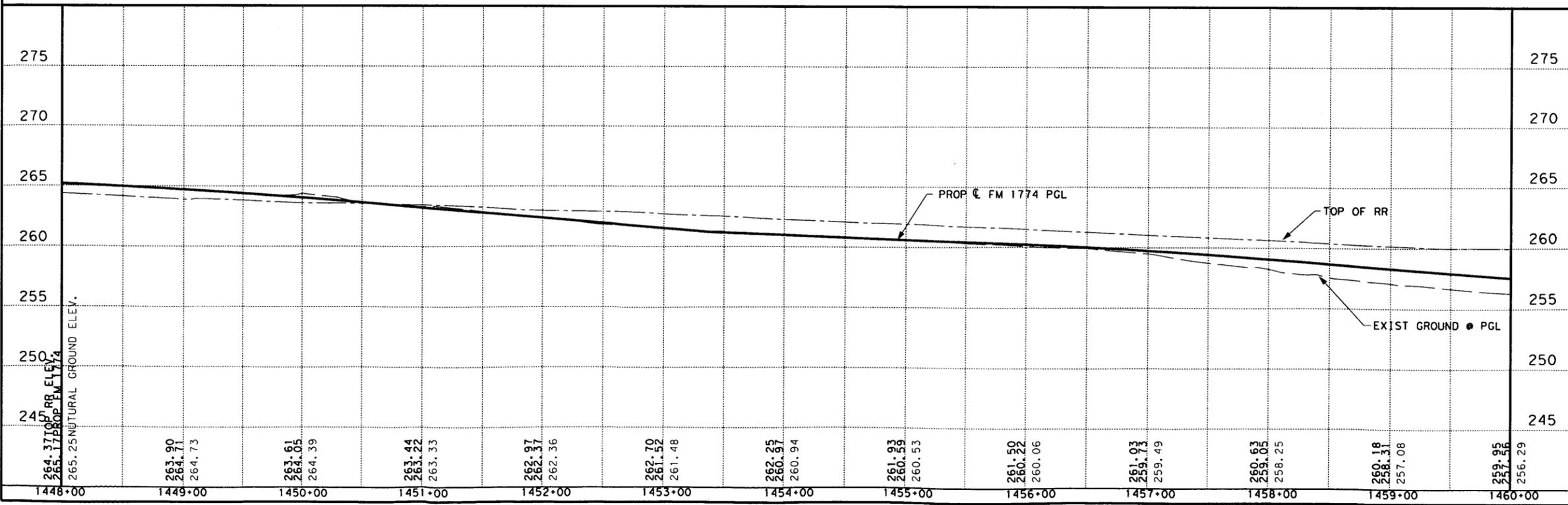


FM 1774 PROPOSED TYPICAL SECTION A-A
 TRAFFIC CONTROL PLAN PHASE 1 STEP 1
 STA. 1460+00 TO STA. 1488+00
 N. T. S.

NOTE:
 THE RAILROAD ROW WILL BE RESTORED TO IT'S CURRENT CONDITION UPON COMPLETION OF THE ROADWAY PROJECT.



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 King Yuen, P.E.



Texas Department of Transportation
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EXHIBIT "A"
FM 1774
PROP. TEMP TCP
JOINT-USE DITCH
FROM RRP 21.56
TO RRP 21.08
NAVASOTA SUBDIV

SCALE: 1" = 100' HOR.
 1" = 10' VERT.

SHEET 1 OF 4

FED. RD. DIV. NO.	FEDERAL AID PROJECT NO.			SHEET NO.
6				
STATE	STATE DIST. NO.	COUNTY		
TEXAS	12	MONTGOMERY		
CONT.	SECT.	JOB	HIGHWAY NO.	
1400	04	035	FM 1774	
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DWG. FILE NAME:	1774TCP P&P1.DGN			

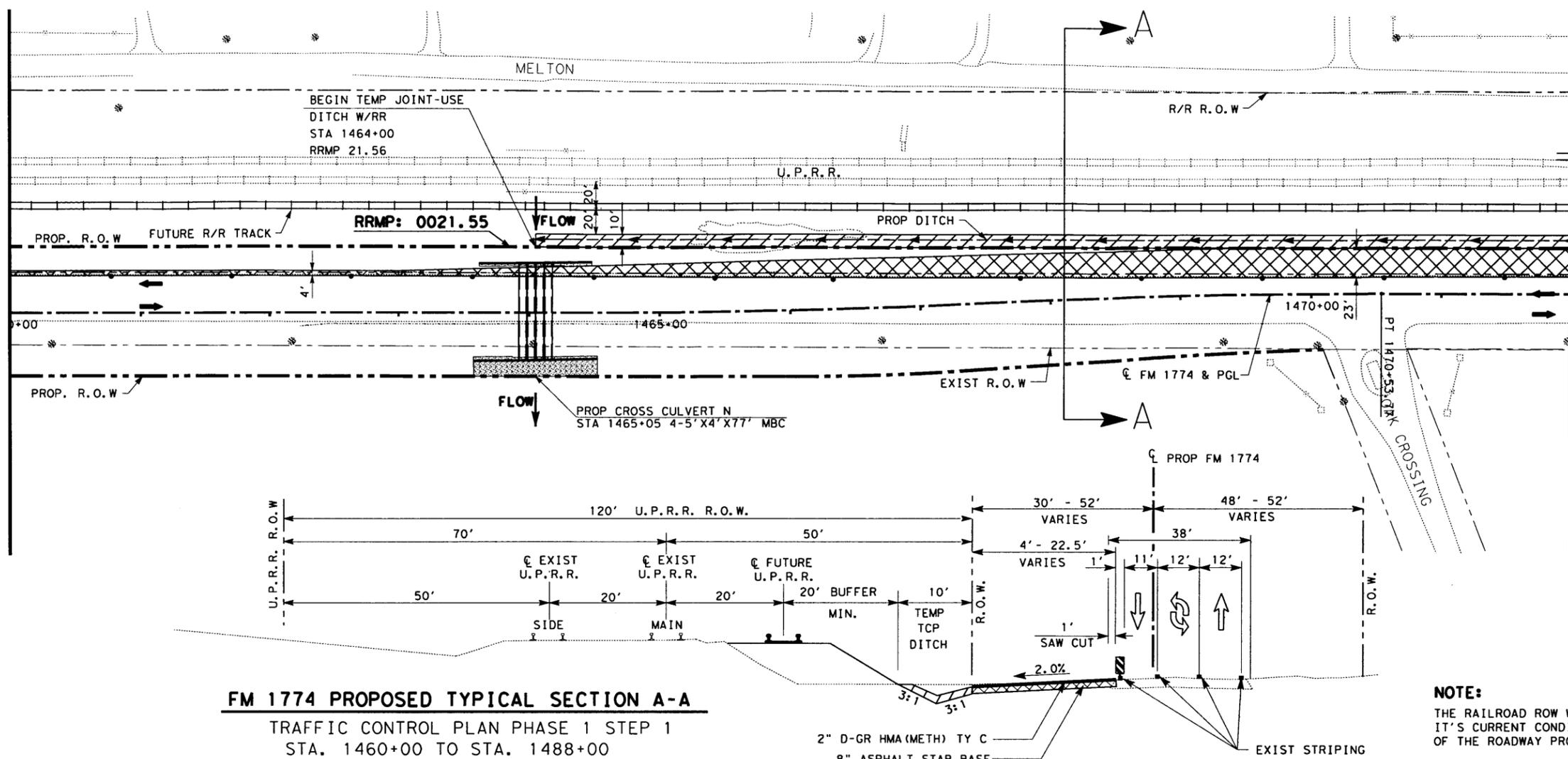
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MATCHLINE STA 1460+00.00

MATCHLINE STA 1472+00.00

LEGEND

- TRAFFIC LANE (GENERAL USE)
- Baseline
- PROPOSED ROW
- EXISTING ROW
- CONTROL OF ACCESS LINE
- DITCH & FLOW DIRECTION
- TEMP JOINT-USE DITCH W/ RR
- TEMPORARY ASPHALT PAVEMENT
- PROPOSED CONCRETE PAVEMENT

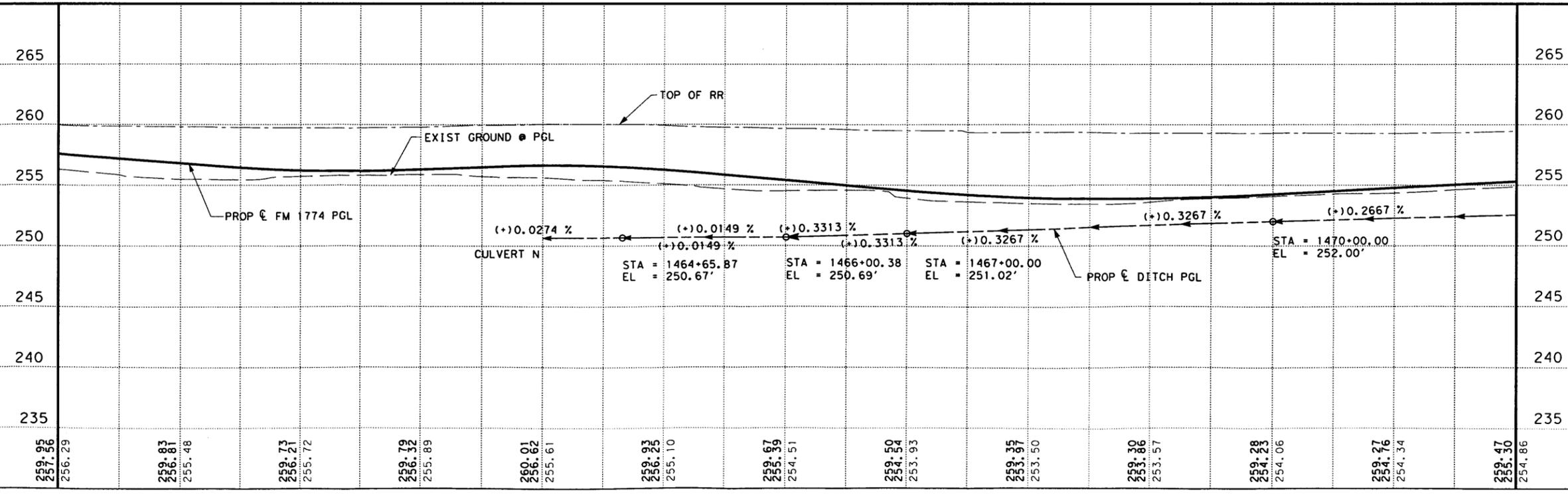


FM 1774 PROPOSED TYPICAL SECTION A-A
 TRAFFIC CONTROL PLAN PHASE 1 STEP 1
 STA. 1460+00 TO STA. 1488+00
 N. T. S.

NOTE:
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 King Yuen, P.E.
 Signed



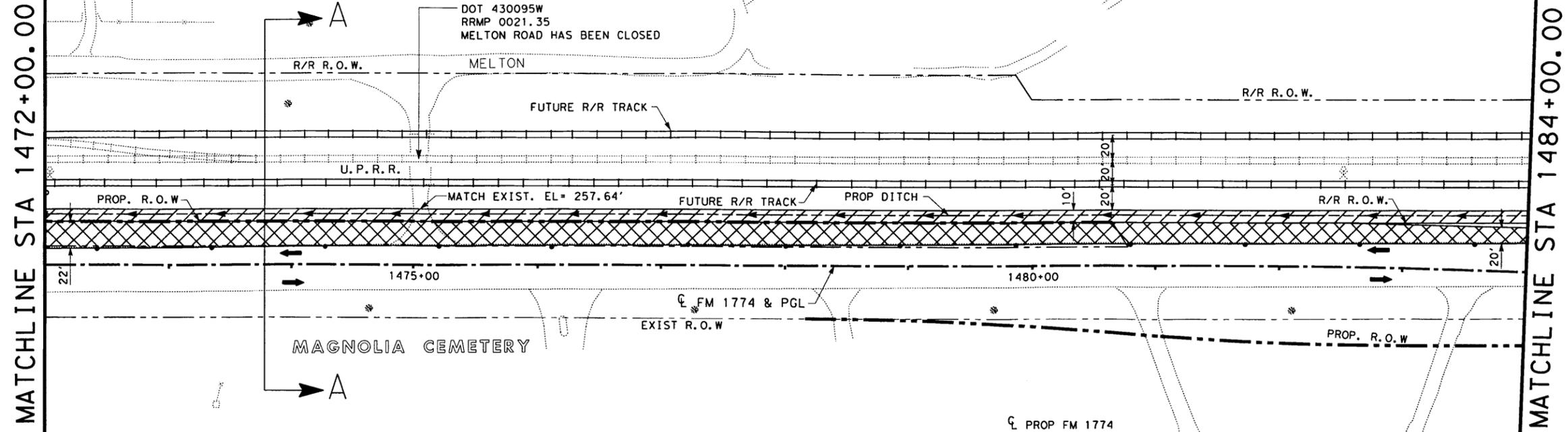
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EXHIBIT "A"
FM 1774
PROP. TEMP TCP
JOINT-USE DITCH
FROM RRMP 21.56
TO RRMP 21.08
NAVASOTA SUBDIV

SCALE: 1" = 100' HOR.
 1" = 10' VERT.

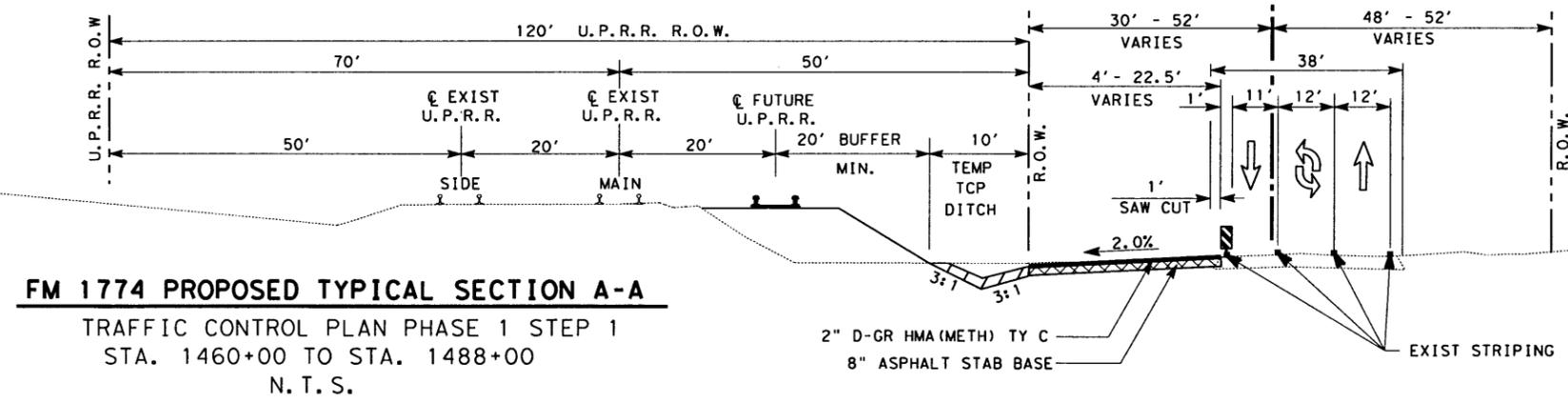
SHEET 2 OF 4

FED. NO.	FEDERAL AID PROJECT NO.		SHEET NO.
6			
STATE	STATE DIST. NO.	COUNTY	
TEXAS	12	MONTGOMERY	
CONF.	SECT.	JOB	HIGHWAY NO.
1400	04	035	FM 1774
DN:	DR:	EX:	
FIG. FILE NAME: 1774TCP P&P2.DGN			



LEGEND

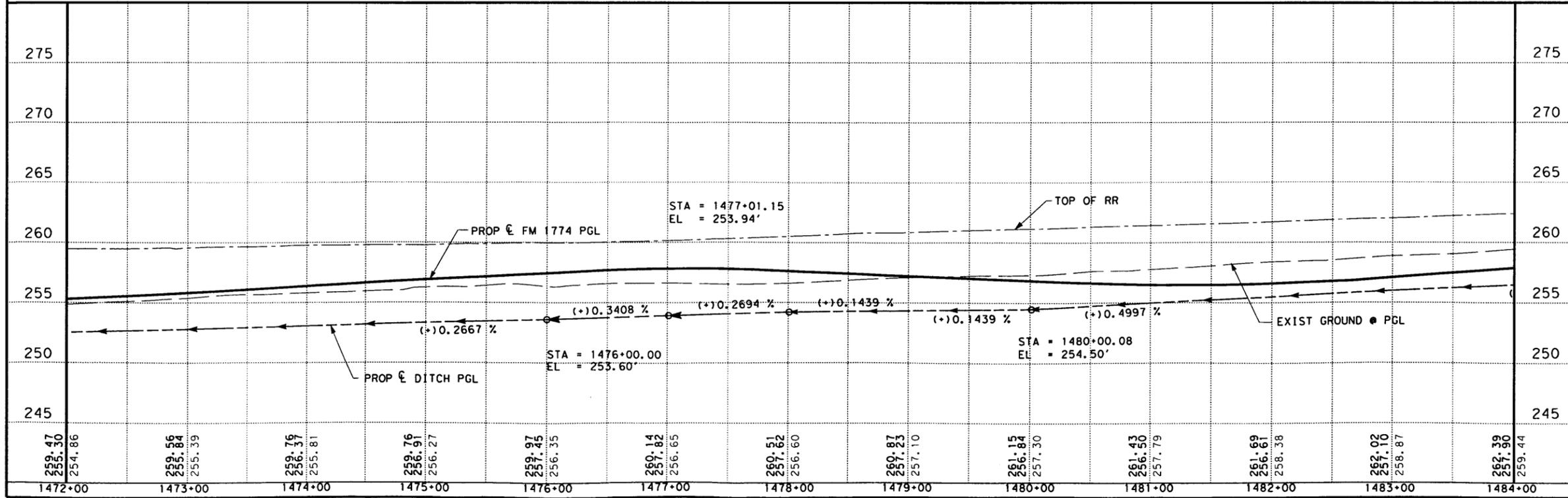
- TRAFFIC LANE (GENERAL USE)
- Baseline
- - - PROPOSED ROW
- - - EXISTING ROW
- ||| CONTROL OF ACCESS LINE
- DITCH & FLOW DIRECTION
- TEMP JOINT-USE DITCH W/ RR
- TEMPORARY ASPHALT PAVEMENT
- PROPOSED CONCRETE PAVEMENT



NOTE:
 THE RAILROAD WILL BE RESTORED TO IT'S CURRENT CONDITION UPON COMPLETION OF THE ROADWAY PROJECT.



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EXHIBIT "A"
FM 1774
PROP. TEMP TCP
JOINT-USE DITCH
FROM RRMF 21.56
TO RRMF 21.08
NAVASOTA SUBDIV

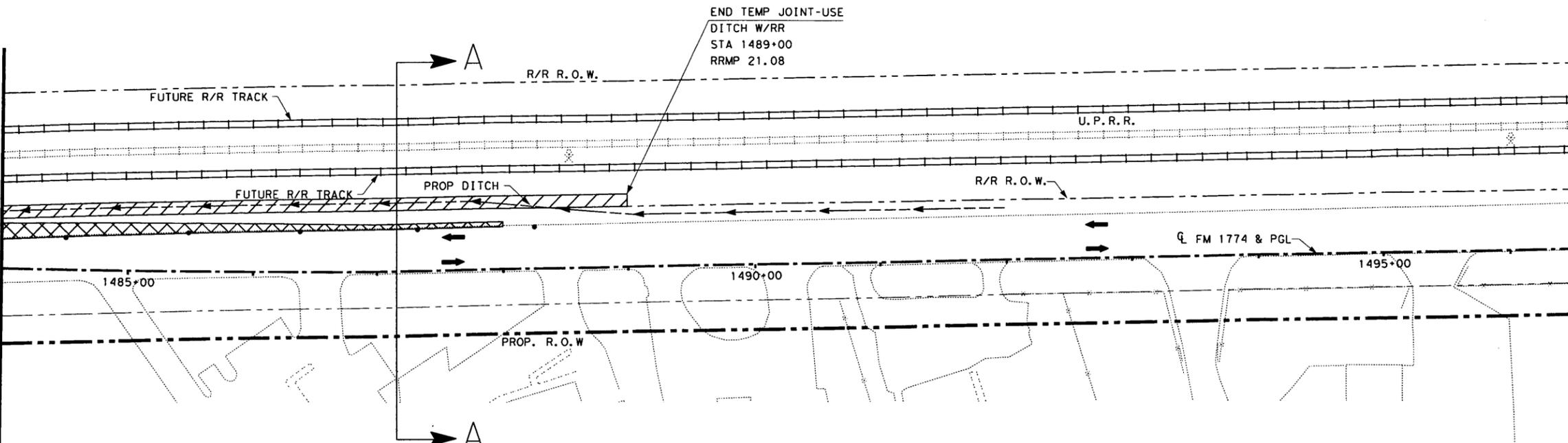
SCALE: 1" = 100' HOR.
 1" = 10' VERT.

SHEET 3 OF 4

FED. RD. DIV. NO.	FEDERAL AID PROJECT NO.		SHEET NO.
6			
STATE	STATE DIST. NO.	COUNTY	
TEXAS	12	MONTGOMERY	
CONTRACT	SECT.	JOB	HIGHWAY NO.
1400	04	036	FM 1774
DWG.	DATE		CHK.
	1774TCP P&P3.DGN		

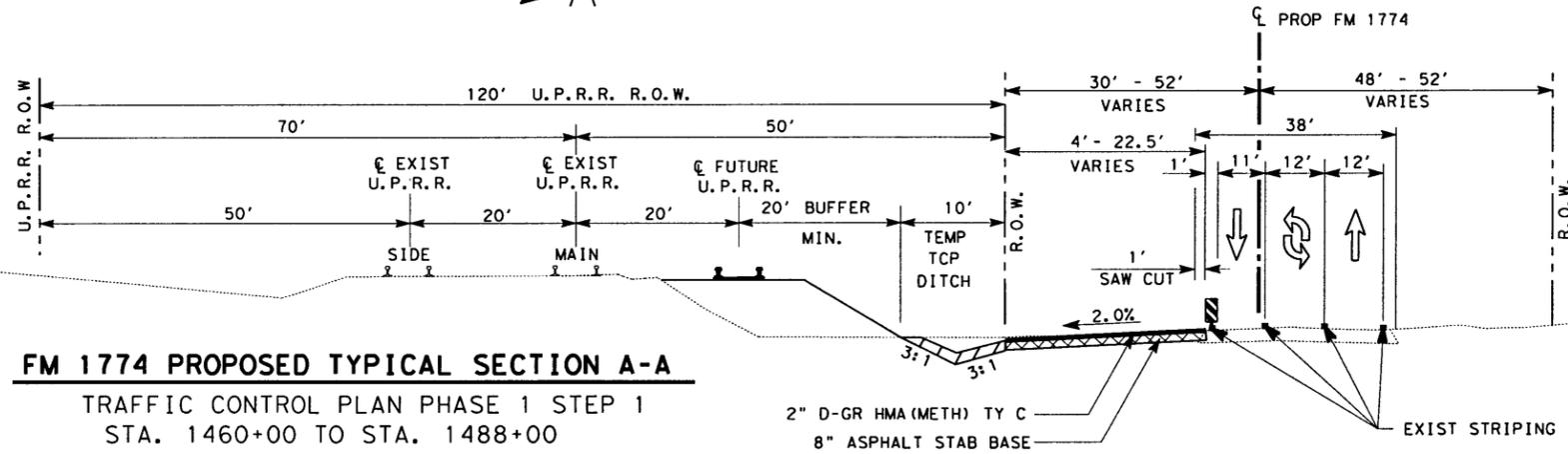
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 REVISED: 04/14/2011

MATCHLINE STA 1484+00.00



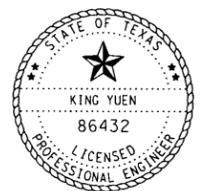
LEGEND

- TRAFFIC LANE (GENERAL USE)
- Baseline
- - - PROPOSED ROW
- - - EXISTING ROW
- ||| CONTROL OF ACCESS LINE
- DITCH CL & FLOW DIRECTION
- TEMP JOINT-USE DITCH W/ RR
- TEMPORARY ASPHALT PAVEMENT
- PROPOSED CONCRETE PAVEMENT

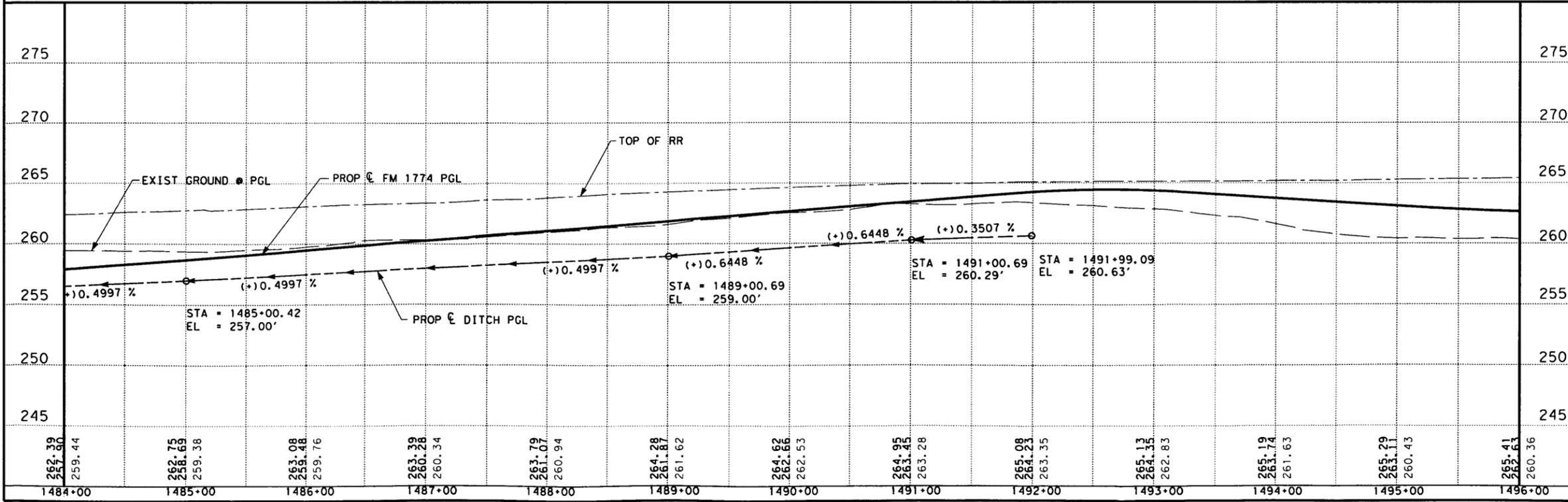


FM 1774 PROPOSED TYPICAL SECTION A-A
 TRAFFIC CONTROL PLAN PHASE 1 STEP 1
 STA. 1460+00 TO STA. 1488+00
 N. T. S.

NOTE:
 THE RAILROAD ROW WILL BE RESTORED TO IT'S CURRENT CONDITION UPON COMPLETION OF THE ROADWAY PROJECT.



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 on 4-14-11
 King Yuen, P.E.
 Signed



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EXHIBIT "A"
FM 1774
PROP. TEMP TCP
JOINT-USE DITCH
FROM RRMP 21.56
TO RRMP 21.08
NAVASOTA SUBDIV
 SCALE: 1" = 100' HOR.
 1" = 10' VERT.
 SHEET 4 OF 4

FED. RD. DIV. NO.		STATE DIST. NO.		COUNTY		FEDERAL AID PROJECT NO.		SHEET NO.	
6		12		MONTGOMERY					
STATE		CONT.		JOB		HIGHWAY NO.			
TEXAS		1400		035		FM 1774			
DWN.		DWN.		DWN.		DWN.			
FILE NAME: 1774TCP P&P4.DGN									

PART 1 - GENERAL

1.01 DESCRIPTION

This project includes construction work within the Right-of-Way and/or properties of the Railroad Company and adjacent to its tracks, wire lines and other facilities. These sheets describe the minimum special requirements for coordination with the Railroad when working upon, over or under Railroad Right-of-Way or when impacting current or future Railroad operations. Coordinate with the Railroad while performing the work outlined herein, and afford the same cooperation with the Railroad as with TxDOT. Complete all submittals and work in accordance with TxDOT Standard Specifications, Railroad Guidelines and AREMA recommendations as modified by these minimum special requirements or as directed in writing by the Railroad Designated Representative.

For purposes of this project, the Railroad Designated Representative is the person or persons designated by the Railroad Manager of Industry and Public Projects to handle specific tasks related to the project.

1.02 RAILROAD CONTACTS

The primary Railroad point of contact for this project and the contact for Railroad flagging services and track work, can be obtained by contacting the TxDOT Area Engineer responsible for the project.

1.03 REQUEST FOR INFORMATION / CLARIFICATION

Submit Requests for Information ("RFI") involving work within any Railroad Right-Of-Way to the TxDOT Engineer. The TxDOT Engineer will submit the RFI to the Railroad Designated Representative for review and approval for RFI's corresponding to work within Railroad Right-Of-Way. Allow six (6) weeks total time for review and approval, which includes four (4) weeks for review and approval by the Railroad.

1.04 PLANS / SPECIFICATIONS

TxDOT has received written Railroad approval of the plans and specifications for this project. Any revisions or changes in the plans after award of the Contract must have the approval of TxDOT and the Railroad.

PART 2 - UTILITIES AND FIBER OPTIC

Construct all utility installations in accordance with current AREMA recommendations, Railroad, TxDOT and owning utility specifications and requirements. Railroad general guidelines can be found on the Railroad website or by contacting the Railroad Designated Representative.

PART 3 - CONSTRUCTION

3.01 GENERAL

- A. Perform all work in compliance with all applicable Railroad, FRA and TxDOT rules and regulations. Arrange and conduct work in a manner that does not endanger or interfere with the safe operation of the tracks and property of the Railroad and the traffic moving on such tracks, or the wires, signals and other property of the Railroad, its tenants or licensees, at or in the vicinity of the Work. The Contractor is responsible for train delay cost and lost revenue claims due to any delays or interruption of train operations resulting from Contractor's construction or other activities.
- B. Construction activities within 12 feet of the operational tracks will only be allowed if absolutely necessary and the Railroad's Designated Representative grants approval. Construction activities within 12 feet of the operational track(s) preferably allow the tracks to stay operational. In such cases, coordination and approval by the Railroad Track Manager is required with regard to schedule, flagging, and slow orders. See Sections 3.07 and 3.08 for additional information.
- C. Provide track protection for all work equipment (including rubber tired equipment) operating within 25 feet from nearest rail.
- D. The Contractor is also advised that new railroad facilities within the project may be built by the Railroad. If applicable, these facilities are delineated in the plans. Be aware of the limits of responsibilities and coordinate efforts with the Railroad and TxDOT.

3.02 RAILROAD OPERATIONS

- A. Trains and/or equipment are expected on any track, at any time, in either direction. Become familiar with the train schedules in this location and structure bid assuming intermittent track windows in this period, as defined in Paragraph B that follows.
- B. All railroad tracks within and adjacent to the Contract Site are active, and rail traffic over these facilities shall be maintained throughout the Project. Activities may include both through moves and switching moves to local customers. Railroad traffic and operations will occur continuously throughout the day and night on these tracks and shall be maintained at all times as defined herein. Coordinate and schedule the work so that construction activities do not interfere with railroad operations.
- C. Coordinate work windows with TxDOT and the Railroad's Designated Representative. Types of work windows include Conditional Work Windows and Absolute Work Windows, as defined below:

1. Conditional Work Window: A Conditional Work Window is a period of time that railroad operations have priority over construction activities. When construction activities may occur on and/or adjacent to the railroad tracks within 25 feet of the nearest track, a Railroad flag person will be required. At the direction of the Railroad flag person, upon approach of a train, and when trains are present on the tracks, the tracks must be cleared (i.e., no construction equipment, materials or personnel within 25 feet, or as directed by the Railroad Designated Representative, from the tracks). Conditional Work Windows are available for the Project.
2. Absolute Work Window: An Absolute Work Window is a period of time that construction activities are given priority over railroad operations. During this time frame the designated railroad track(s) will be inactive for train movements and may be fouled by the Contractor. At the end of an Absolute Work Window the railroad tracks and/or signals must be completely operational for train operations and all Railroad, Public Utilities Commission (PUC) and Federal Railroad Administration (FRA) requirements, codes and regulations for operational tracks must be satisfied. In the situation where the operating tracks and/or signals have been affected, the Railroad will perform inspections of the work prior to placing that track back into service. Railroad flag persons will be required for construction activities requiring an Absolute Work Window. Absolute Work Windows will not generally be granted. Any request will require a detailed explanation for Railroad review.

3.03 RIGHT OF ENTRY, ADVANCE NOTICE AND WORK STOPPAGES

- A. Prior to beginning any work on or over the property of, or affecting the facilities of the Railroad, enter into a "Contractor's Right of Entry Agreement" with Railroad. The fee for processing the agreement is the responsibility of the Contractor. Submit a copy of the executed agreement and the insurance policies, binders, certificates and endorsements set forth therein to TxDOT prior to commencing work on Railroad property. Within the right of entry agreement, specify working time frames, flagging and inspection requirements, and any other items specified by the Railroad.
- B. Give advance notice to the Railroad as required in the "Contractor's Right of Entry Agreement" before commencing work in connection with construction upon or over Railroad Right-of-Way and observe the Railroad's rules and regulations with respect thereto.
- C. Perform all work upon Railroad Right-of-Way in a manner to avoid interference with or endanger the operations of the Railroad. Whenever work may affect the operations or safety of trains, submit the work method to the Railroad Designated Representative for approval. Approval does not relieve the Contractor from liability. Do not commence any work which requires flagging service or inspection service until the flagging protection required by the Railroad is available at the job site. See Section 3.18 for railroad flagging requirements.
- D. Make requests in writing for both Absolute and Conditional Work Windows, at least two weeks in advance of any work. Include in the written request:
 1. Exactly what the work entails.
 2. The days and hours that work will be performed.
 3. The exact location of work, and proximity to the tracks.
 4. The type of window requested and the amount of time requested.
 5. The designated contact person.

Provide a written confirmation notice to the Railroad at least 48 hours before commencing work in connection with approved work windows when work is within 25 feet of any track center line. Perform all work in accordance with previously approved work plans.

- E. Make provisions to protect operations and property of the Railroad should a condition arising from, or in connection with the work, require immediate and unusual action. If in the judgment of the Railroad Designated Representative such provisions are insufficient, the Railroad Designated Representative may require or provide such provisions as deemed necessary. In any event, such provisions shall be at the Contractor's expense and without cost to the Railroad or TxDOT. The Railroad or TxDOT shall have the right to order the Contractor to temporarily cease operations in the event of an emergency or, if in the opinion of the Railroad Designated Representative, the Contractor's operations could endanger railroad operations. In the event of such an order, immediately notify TxDOT of the order.

3.04 INSURANCE

Do not begin work upon or over Railroad Right-of-Way until furnishing the Railroad with the insurance policies, binders, certificates and endorsements required by the "Contractor's Right-of-Entry Agreement", and until the Railroad Designated Representative has advised TxDOT that such insurance is in accordance with the Agreement. Further information on insurance requirements appear in the Special Provisions to Item 7 of the TxDOT Standard Specifications and/or the project General Notes.

3.05 RAILROAD SAFETY ORIENTATION

Complete the Railroad course "Orientation for Contractor's Safety", and maintain current registration prior to working on Railroad property. This orientation is available at www.contractororientation.com. This course is required to be completed annually by Contractor and Subcontractor personnel working on site.

"KCS/TEXMEX will not accept on-track safety training certificates from other railroads. Contractor's employees entering the KCS railroad shall hold current certificates at all times. The training can be had by contacting Larry Slater of TrackSense Inc. at 330-847-8661 or by email at lslater@neo.rr.com."

3.06 COOPERATION

The Railroad will cooperate with Contractor so that work may be conducted in an efficient manner, and will cooperate with Contractor in enabling use of Railroad Right-of-Way in performing the work.

3.07 MINIMUM CONSTRUCTION CLEARANCES FOR FALSEWORK AND OTHER TEMPORARY STRUCTURES

Abide by the following minimum temporary clearances during the course of construction:
A. 15' - 0" (BNSF) and 12'-0" (UPRR & KCS) horizontal from centerline of track
B. 21' - 0" vertically above top of rail.

For construction clearance less than listed above, obtain local Railroad Operating Unit review and approval.

3.08 APPROVAL OF REDUCED CLEARANCES

- A. Maintain minimum track clearances during construction as specified in Section 3.07.
- B. Submit any proposed infringement on the specified minimum clearances to the Railroad Designated Representative through TxDOT at least 30 days in advance of the work. Do not proceed with such infringement until written approval by the Railroad Designated Representative.
- C. Do not commence work involving an approved infringement until receiving written assurance from the Railroad Designated Representative that arrangements have been made for any necessary flagging service.

ACTIVE FILE LEVELS DISPLAYED
1 2 3 4 5 6 7 8 9 10 11 12 13 14 15 16
17 18 19 20 21 22 23 24 25 26 27 28 29 30 31 32
33 34 35 36 37 38 39 40 41 42 43 44 45 46 47 48
49 50 51 52 53 54 55 56 57 58 59 60 61 62



RAILROAD REQUIREMENTS FOR NON-BRIDGE CONSTRUCTION PROJECTS

EXHIBIT "A"

FILE: STD SHT1-2.DGN	DW: TxDOT	CR: TxDOT	DR: TxDOT	CK: TxDOT
© TxDOT JUNE 2010		DISTRICT	FEDERAL AID PROJECT	SHEET
REVISIONS		12		
06/24/10 - Revised Part 2 - Utilities And Fiber Optic Note		COUNTY	CONTROL SECT	JOB HIGHWAY
		MONTGOMERY	1400 04	035 FM1774

3.09 MAINTENANCE OF RAILROAD FACILITIES

- A. Maintain all ditches and drainage structures free of silt or other obstructions resulting from contractor's operations. Repair eroded areas and any other damage within Railroad Right-of Way and repair any other damage to the property of the Railroad, or its tenants.
- B. Perform all such maintenance and repair of damages due to the Contractor's operations at Contractor's expense.
- C. Submit a proposed method of erosion control for review by the Railroad prior to beginning any grading on the Project Site. Comply with all applicable local, state and federal regulations when developing and implementing such erosion control.

3.10 SITE INSPECTIONS BY RAILROAD'S DESIGNATED REPRESENTATIVE

- A. In addition to the office reviews of construction submittals, site inspections may be performed by the Railroad Designated Representative at significant points during construction, including the following if applicable:
 1. Pre-construction meetings.
 2. Pile driving/drilling of caissons or drilled shafts.
 3. Reinforcement and concrete placement for railroad bridge substructure and/or superstructure.
 4. Erection of precast concrete or steel bridge superstructure.
 5. Placement of waterproofing (prior to placing ballast on bridge deck).
 6. Completion of the bridge structure.
- B. Site inspection is not limited to the milestone events listed above. Site visits to check progress of the work may be performed at any time throughout the construction as deemed necessary by the Railroad.
- C. Provide a detailed construction schedule, including the proposed temporary horizontal and vertical clearances and construction sequence for all work to TxDOT for submittal to the Railroad Designated Representative for review prior to commencement of work. Include the anticipated dates when the above listed events will occur. Update this schedule for the above listed events as necessary and each month at a minimum to allow the Railroad to schedule site inspections.

3.11 RAILROAD REPRESENTATIVES

Railroad representatives, conductors, flag person or watch person will be provided by the Railroad at expense of TxDOT to protect Railroad facilities, property and movements of its trains or engines. In general, the Railroad will furnish such personnel or other protective services as follows:

- A. When any part of any equipment is standing or being operated within 25 feet, measured horizontally, from centerline of any track on which trains may operate, or when any object is off the ground and any dimension thereof could extend inside the 25 foot limit, or when any erection or construction activities are in progress within such limits, regardless of elevation above or below track.
- B. For any excavation below elevation of track subgrade if, in the opinion of the Railroad Designated Representative, track or other Railroad facilities may be subject to settlement or movement.
- C. During any clearing, grubbing, excavation or grading in proximity to Railroad facilities, which, in the opinion of the Railroad Designated Representative, may endanger Railroad facilities or operations.
- D. During any contractor's operations when, in the opinion of the Railroad Designated Representative, Railroad facilities, including, but not limited to, tracks, buildings, signals, wire lines, or pipe lines, may be endangered.
- E. Arrange with the Railroad Designated Representative to provide the adequate number of flag persons to accomplish the work.

3.12 COMMUNICATIONS AND SIGNAL LINES

If required, the Railroad will rearrange its communications and signal lines, its grade crossing warning devices, train signals and tracks, and facilities that are in use and maintained by the Railroad's forces in connection with its operation at expense of TxDOT. This work by the Railroad will be done by its own forces and it is not a part of the Work under this Contract.

3.13 TRAFFIC CONTROL

Coordinate any operations that control traffic across or around Railroad facilities with the Railroad Designated Representative.

3.14 CONSTRUCTION EXCAVATIONS

- A. Take special precaution and care in connection with excavating and shoring. Excavations for construction of footings, piers, columns, walls or other facilities that require shoring shall comply with requirements of TxDOT, OSHA, AREMA and Railroad "Guidelines for Temporary Shoring".
- B. The project plans indicate whether there are fiber optic lines or other such telecommunications systems that require consideration. Regardless, contact the necessary call center to determine if such cable systems are present:

UPRR 1-800-336-9193
7:00 AM to 9:00 PM CST Monday-Friday except holidays,
staffed 24 hrs/day for emergencies
48 hrs notice required

BNSF 1-800-533-2891
24 hour number
5 working days notice required

KCS 1-800-344-8377
Texas One Call, a 24 hour number
48 hrs notice required, excluding weekends and holidays

If a telecommunications system is buried anywhere on or near Railroad property, coordinate with TxDOT, the Railroad and the Telecommunication Company(ies) to arrange for relocation or protective measures prior to beginning work on or near Railroad property. Refer to the project General Notes for additional information.

3.15 RAILROAD FLAGGING

Railroad flagging will be required for any work within 25 ft horizontally of the track. Provisions for flagging are also described in Item 7.16 "Work Near Railroads" in the TxDOT Standard Specifications.

3.16 CLEANING OF RIGHT-OF-WAY

When work is complete, remove all tools, implements, and other materials brought into Railroad Right-of-Way and leave the Right-of-Way in a clean and presentable condition to satisfaction of TxDOT and the Railroad.

ACTIVE FILE LEVELS DISPLAYED
1 2 3 4 5 6 7 8 9 10 11 12 13 14 15 16
17 18 19 20 21 22 23 24 25 26 27 28 29 30 31 32
33 34 35 36 37 38 39 40 41 42 43 44 45 46 47 48
49 50 51 52 53 54 55 56 57 58 59 60 61 62



RAILROAD REQUIREMENTS FOR NON-BRIDGE CONSTRUCTION PROJECTS

EXHIBIT "A"

FILE: STD SHT1-2.DGN	DN: TxDOT	CR: TxDOT	DR: TxDOT	CK: TxDOT
© TxDOT JUNE 2010	DISTRICT	FEDERAL AID PROJECT		SHEET
REVISIONS				
06/24/10 - Revised Part 2 - Utilities And Fiber-Optic Note				
12				
COUNTY	CONTROL	SECT	JOB	HIGHWAY
MONTGOMERY	1400	04	035	FM1774