



**Technical Work Group Meeting,
Southeast Connector Update**

I-20, I-820 & US 287

February 21, 2019

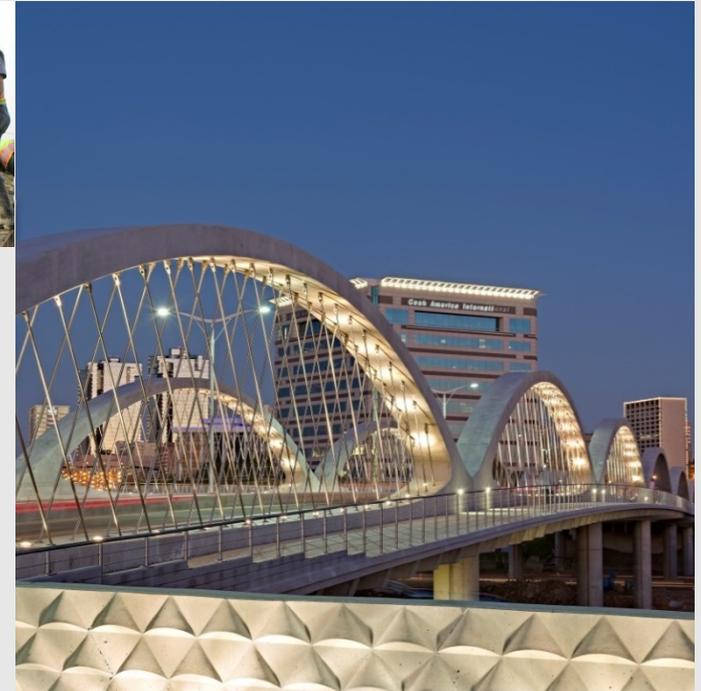


Photo by Liam Frederick



Agenda

- 1 Introductions
- 2 July 2018 Public Meeting
- 3 Past Activities
- 4 Current Status
- 5 Next Steps
- 6 Other Issues / Discussion



2. July 19, 2018 Public Meeting

➤ 273 Attended

- 203 General Public
- 14 Elected Officials
- 56 Project Representatives

➤ 174 Written Comments

- 33 Received at Meeting
- 115 E-mailed
- 26 Mailed





Comments from July Public Meeting

NB Entrance Ramp from Meadowbrook

– 88 Comments

SB Entrance Ramp from Brentwood Stair

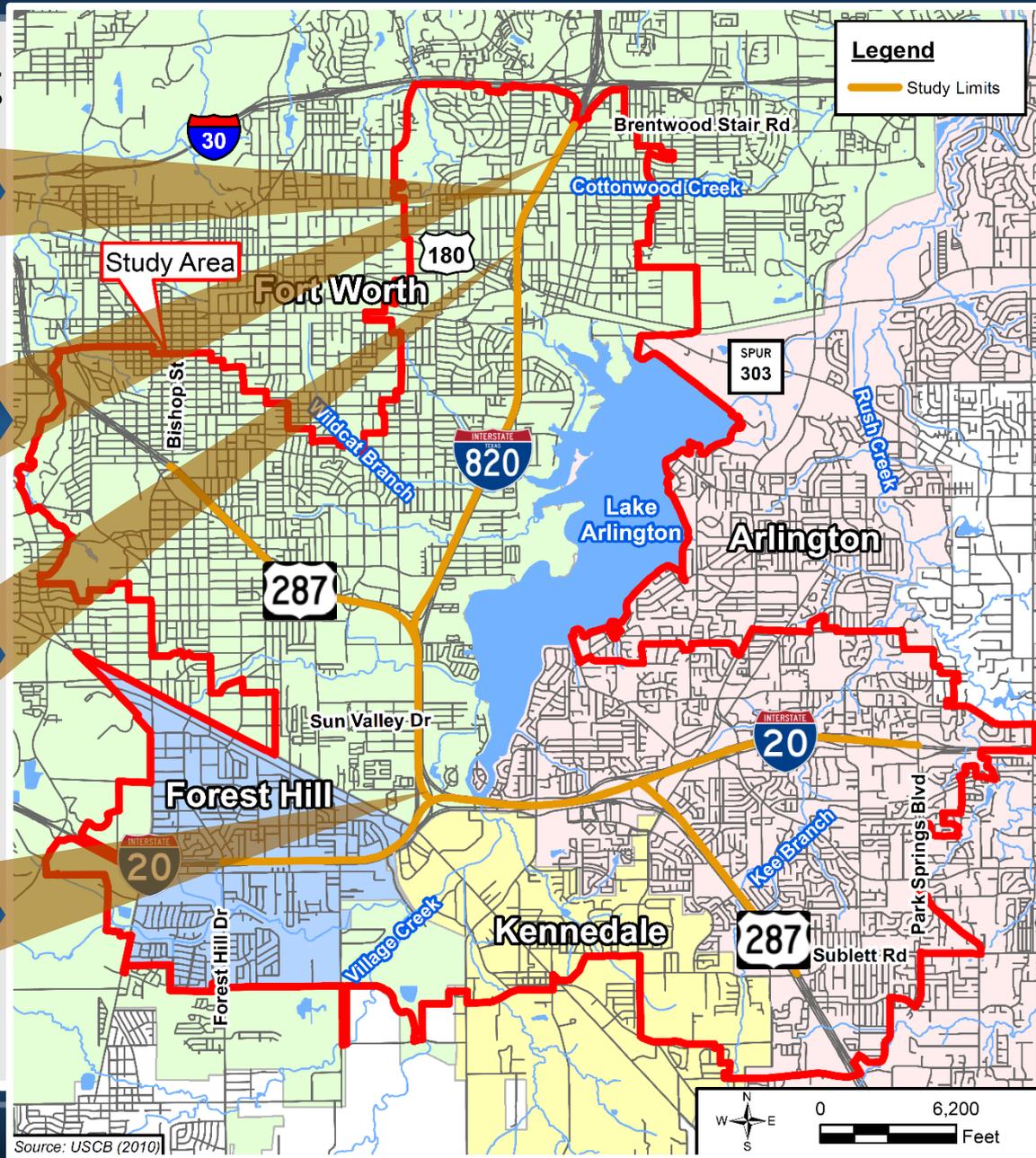
– 49 Comments

Craig Street Bridge Access / Ped Bridge

– 25 Comments

Noise Wall Study

– 51 Comments





3. Past Activities

- **Compiling Public Meeting Summary**
- **Held Additional Public Outreach**
 - Town Hall Meetings with State Rep Collier
 - Community Meeting with Councilwoman Bivens
 - Various Cities
 - Lions and Optimist Clubs
- **Focus Study Area on Northern I-820 near Meadowbrook**
 - NB Entrance Ramp from Meadowbrook
 - SB Entrance Ramp from Brentwood Stair
 - Craig Street Bridge Access / Pedestrian Bridge



Northbound Entrance Ramp from Meadowbrook



WHY?

- Congestion and Safety
- Conflicts with Exit to Brentwood Stair and I-30 Direct Connections causing heavily congested weaving movements and accidents
- Distance between ramps near minimum spacing lengths described in roadway design guidelines



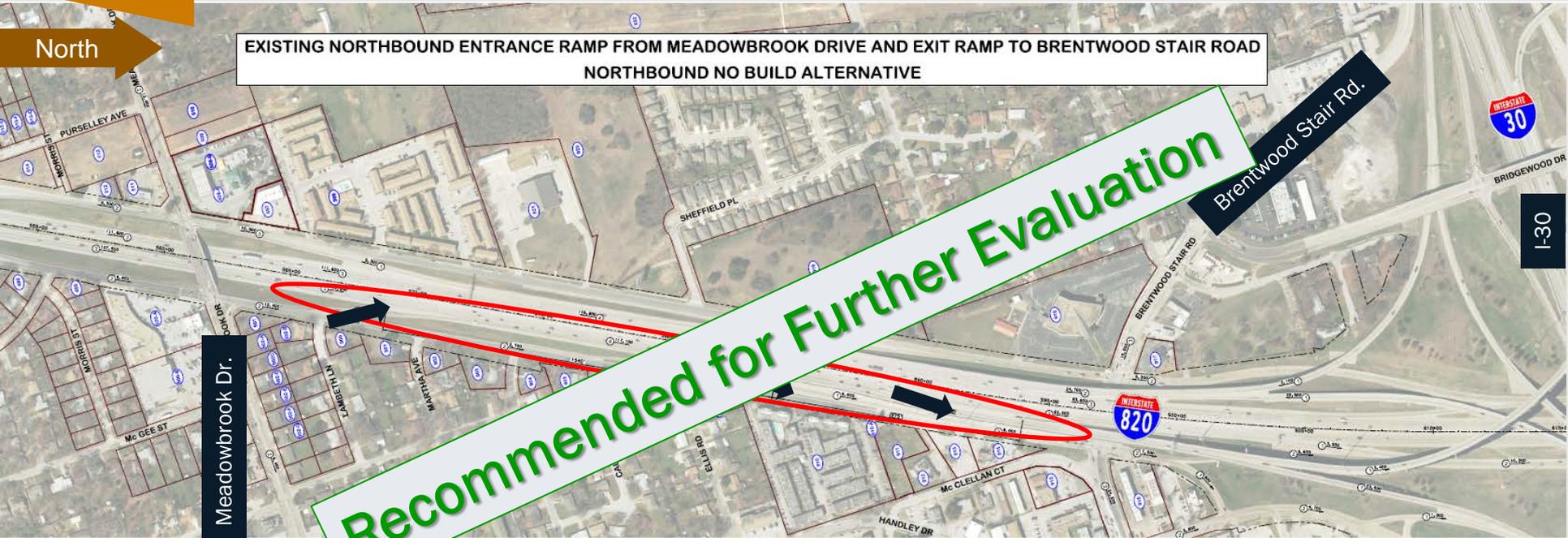
Northbound Entrance Ramp from Meadowbrook

PROPOSED ADDITIONAL ALTERNATIVES EVALUATED

- **No-Build (Do Nothing) Alternative**
- **A1:** Removal of northbound entrance ramp from Meadowbrook Dr (as shown at Public Meeting)
- **A2:** Reconstruct northbound entrance ramp from Meadowbrook Dr and exit ramp to Brentwood Stair Rd
- **A3:** Construct braided ramps (*ramps that are bridged, one over the other*) for northbound entrance from Meadowbrook Dr and exit to Brentwood Stair Rd
- **A4:** Construct northbound entrance ramp from Meadowbrook Dr and removal of the exit ramp to Brentwood Stair Rd



Northbound Entrance Ramp from Meadowbrook – **No Build Alternative**



Pros:

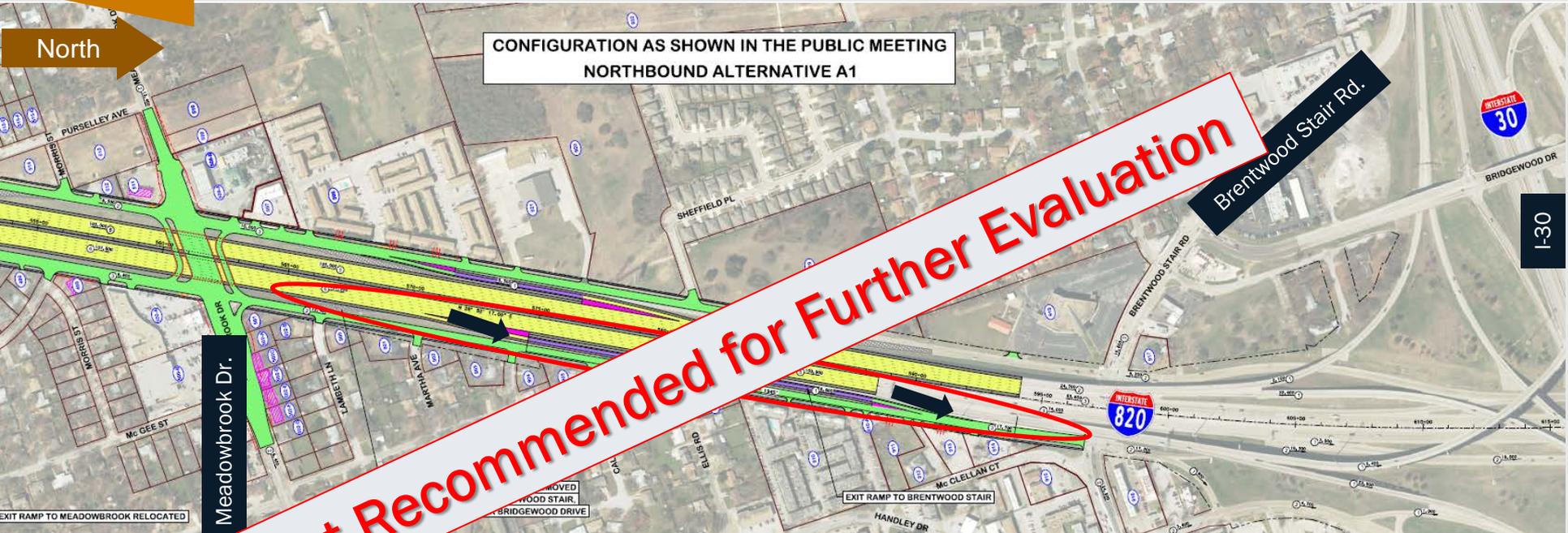
- Access remains as today

Cons:

- Distance between ramps near minimum spacing lengths described in TxDOT and National Guidelines
- Heavily congested weaving movements



Northbound Entrance Ramp from Meadowbrook – Alternative A1



Pros:

- Eliminates existing mainlane weaving from Meadowbrook entrance ramp reducing congestion and improving safety
- Design meets TxDOT and National Guidelines for ramp spacing criteria

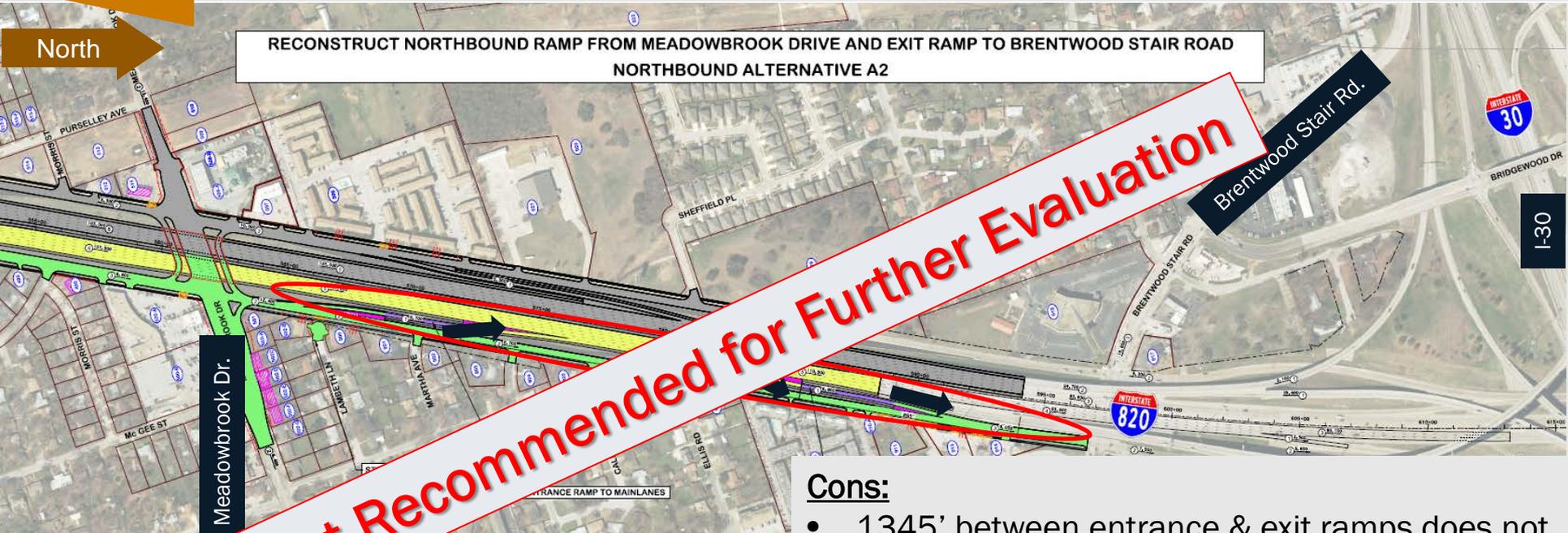
Cons:

- Traffic from Meadowbrook must pass through a signal to access mainlanes

Based on input from Town Hall Meetings, Not Recommended for Further Evaluation



Northbound Entrance Ramp from Meadowbrook – **Alternative A2**



RECONSTRUCT NORTHBOUND RAMP FROM MEADOWBROOK DRIVE AND EXIT RAMP TO BRENTWOOD STAIR ROAD
NORTHBOUND ALTERNATIVE A2

Not Recommended for Further Evaluation

Pros:

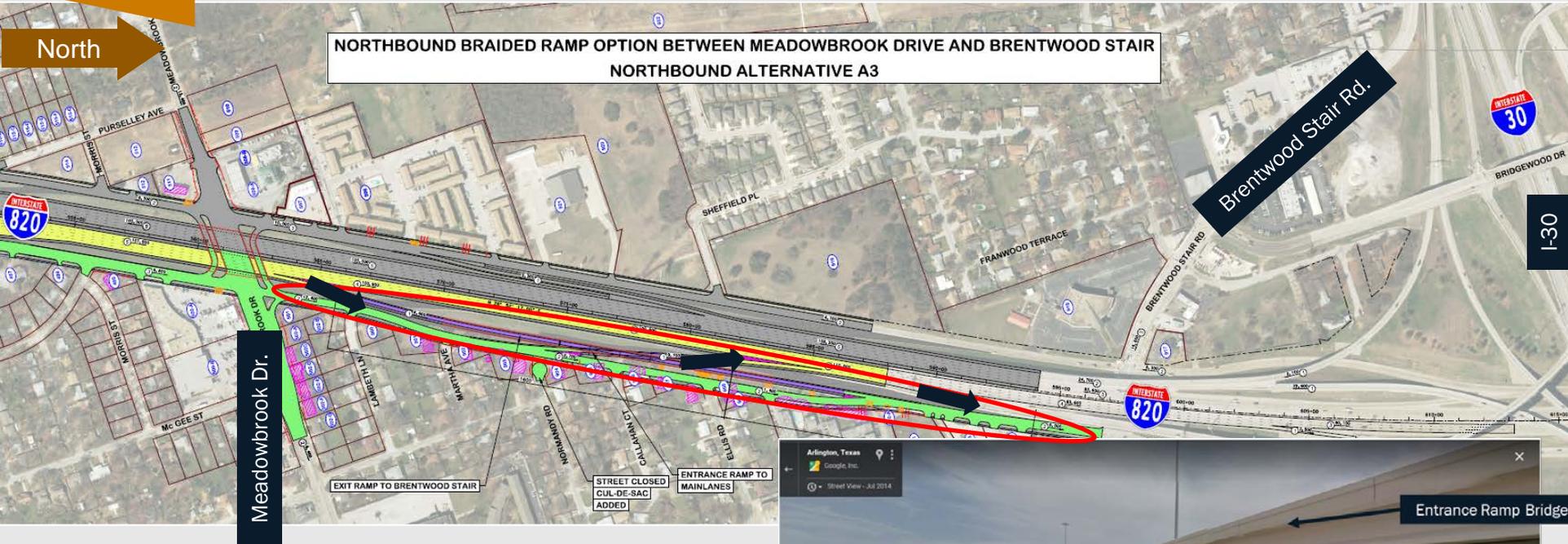
- Existing northbound ramps are reconstructed to allow additional mainlanes capacity

Cons:

- 1345' between entrance & exit ramps does not meet minimum TxDOT and National Guidelines
- 865' between exit ramp & direct connection does not meet minimum TxDOT and National Guidelines
- Traffic from the northbound I-820 on-ramp from Meadowbrook wanting to continue northbound on I-820 must weave over 2 lanes in a distance of 1330'
- Reduced shoulders on existing mainlanes



Northbound Entrance Ramp from Meadowbrook – Alternative A3



Example of “Braided Ramps”

- Eastbound I-30 exit to Nolan Ryan Expwy/Baird Farm Rd/ AT&T Way in Arlington

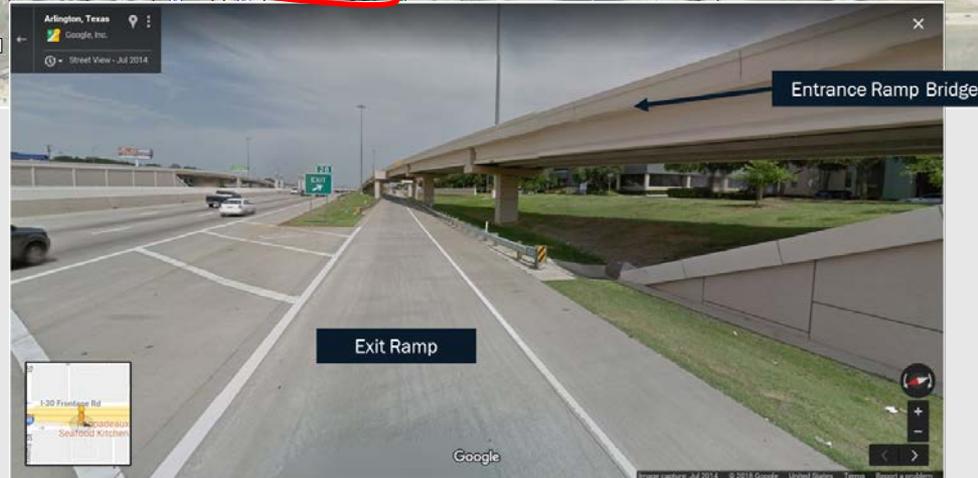
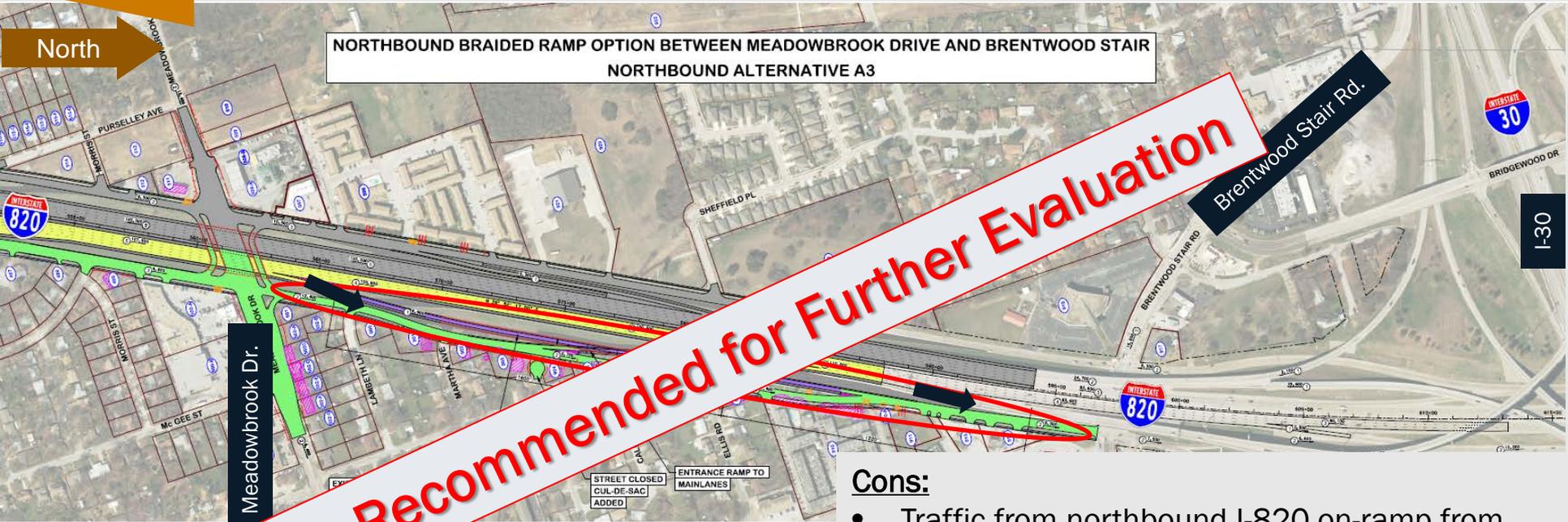


Image from Google.com street view



Northbound Entrance Ramp from Meadowbrook – **Alternative A3**



Not Recommended for Further Evaluation

Pros:

- All direct movements remain available

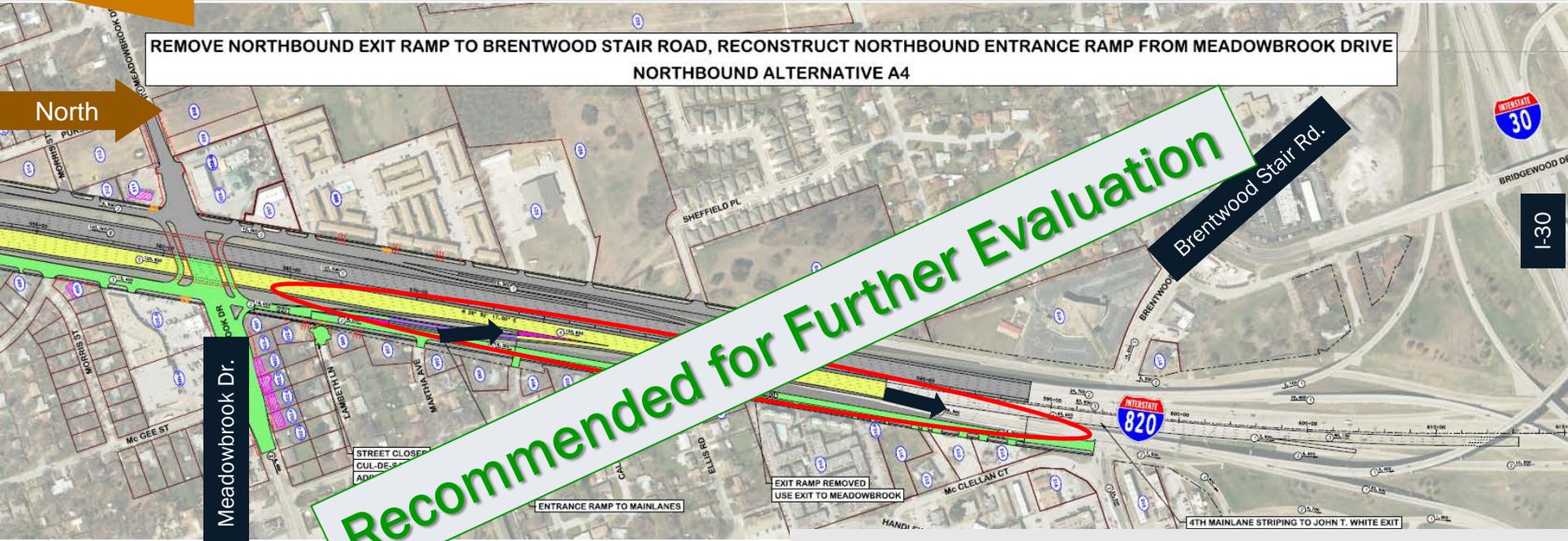
Cons:

- Traffic from northbound I-820 on-ramp from Meadowbrook wanting to continue northbound on I-820 must weave over 1 lane in 450'
- 1220' ramp spacing would not meet minimum National Guidelines
- Elevated ramp near neighborhood could increase noise levels
- Potential displacements of residents
- Reduced shoulders on mainlanes
- Normandy Road cul-de-sac
- Grades on elevated ramp greater than 4% max



Northbound Entrance Ramp from Meadowbrook – **Alternative A4**

REMOVE NORTHBOUND EXIT RAMP TO BRENTWOOD STAIR ROAD, RECONSTRUCT NORTHBOUND ENTRANCE RAMP FROM MEADOWBROOK DRIVE
NORTHBOUND ALTERNATIVE A4



Pros:

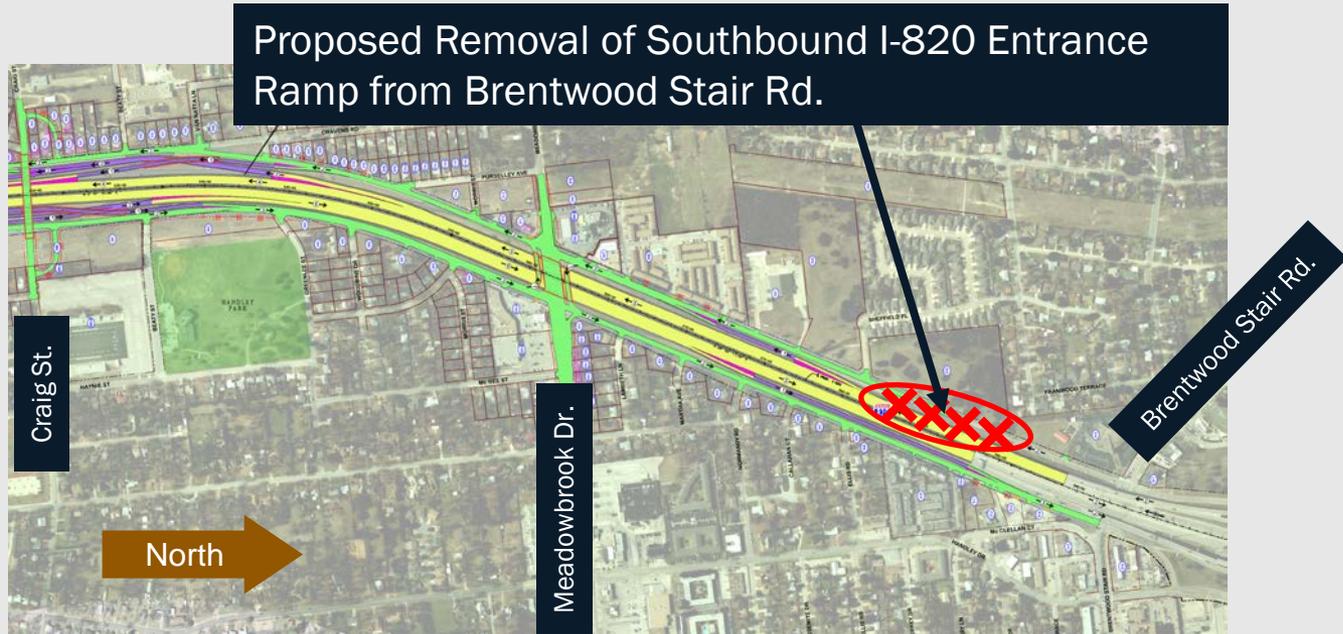
- Ramp spacing would meet minimum TxDOT and National Guidelines
- Weaving and congestion levels would improve compared to the No-Build alternative by maintaining 4 mainlanes until the John T. White exit ramp

Cons:

- Lambeth Lane would be closed off from frontage road (cul-de-sac)
- Remove exit ramp to Brentwood Stair (traffic must travel through signal at Meadowbrook)
- Increase volume on the northbound exit ramp to Meadowbrook
- Increase traffic volumes at the Meadowbrook intersection
- Reduced shoulders on mainlanes



Southbound Entrance Ramp from Brentwood Stair



WHY?

- Congestion and Safety
- Conflicts with I-30 Direct Connections and Exit Ramp to Meadowbrook Dr causing heavily congested weaving movements
- Distance between ramps near minimum spacing lengths described in roadway design guidelines



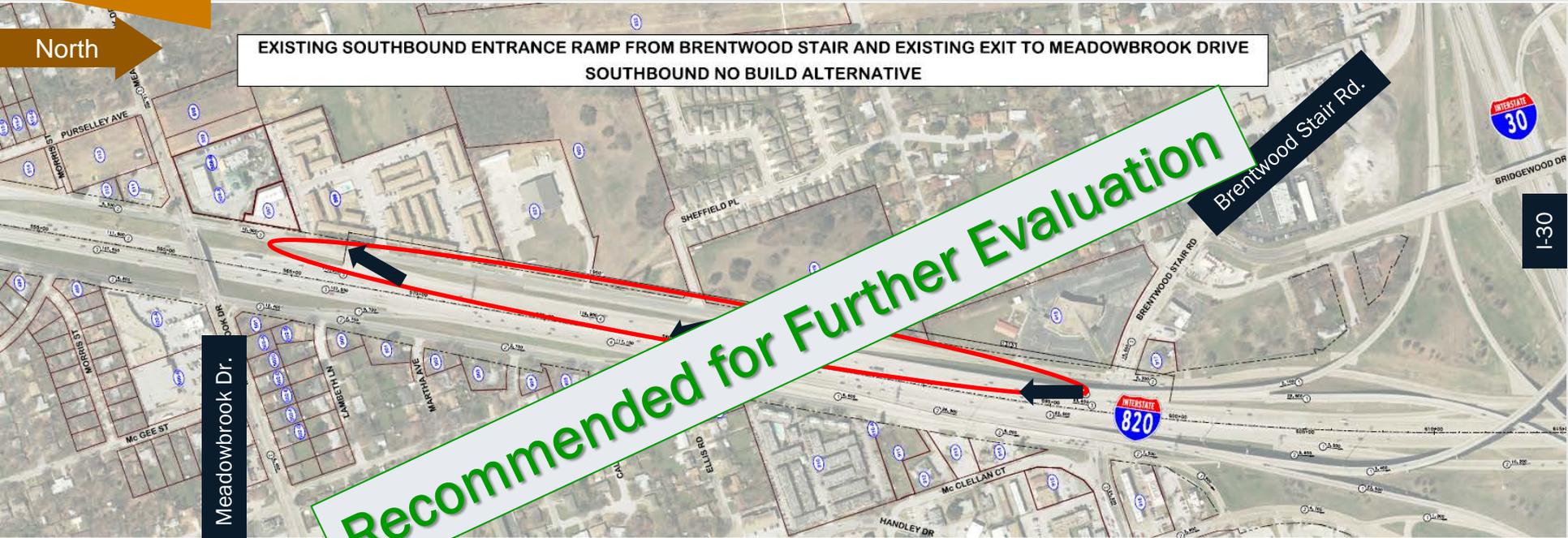
Southbound Entrance Ramp from Brentwood Stair

PROPOSED ADDITIONAL ALTERNATIVES EVALUATED

- **No-Build (Do Nothing) Alternative**
- **B1:** Removal of southbound entrance ramp from Brentwood Stair Rd (as shown at Public Meeting)
- **B2:** Construct **braided ramps** (*ramps that are bridged, one over the other*) for southbound entrance ramp from Brentwood Stair Rd and exit ramp to Meadowbrook Dr
- **B3:** Removal of southbound entrance ramp from Brentwood Stair Rd but construct southbound **bypass frontage road lane** under Meadowbrook Dr to access southbound I-820 entrance ramp
- **B4:** Reconstruct southbound entrance ramp from Brentwood Stair Rd and exit ramp to Meadowbrook Dr



Southbound Entrance Ramp from Brentwood Stair – **No Build Alternative**



EXISTING SOUTHBOUND ENTRANCE RAMP FROM BRENTWOOD STAIR AND EXISTING EXIT TO MEADOWBROOK DRIVE
SOUTHBOUND NO BUILD ALTERNATIVE

Recommended for Further Evaluation

Pros:

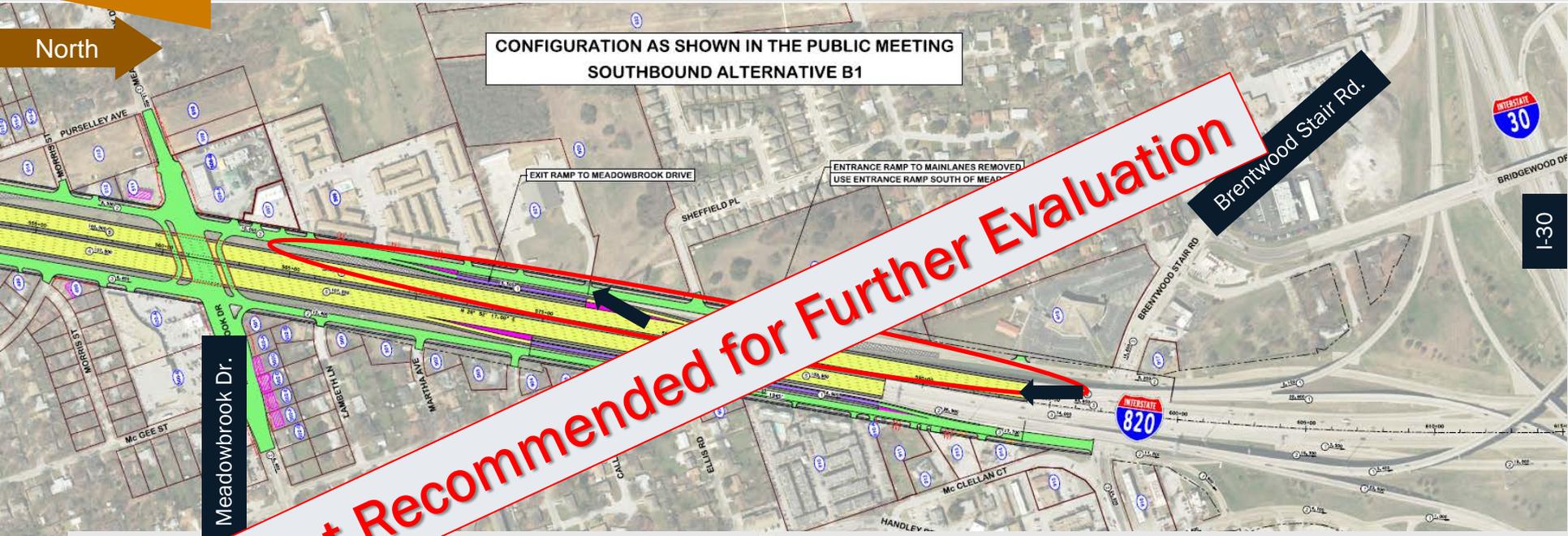
- Access remains as today

Cons:

- Distance between ramps near minimum spacing lengths described in TxDOT and National Guidelines
- Heavily congested weaving movements



Southbound Entrance Ramp from Brentwood Stair – Alternative B1



Pros:

- Eliminates existing mainlane weave from Brentwood Stair entrance ramp reducing congestion and improving safety
- Meets TxDOT and National Guidelines for ramp spacing criteria
- Relocated exit ramp to Meadowbrook provides for additional storage or queueing of vehicles from intersection

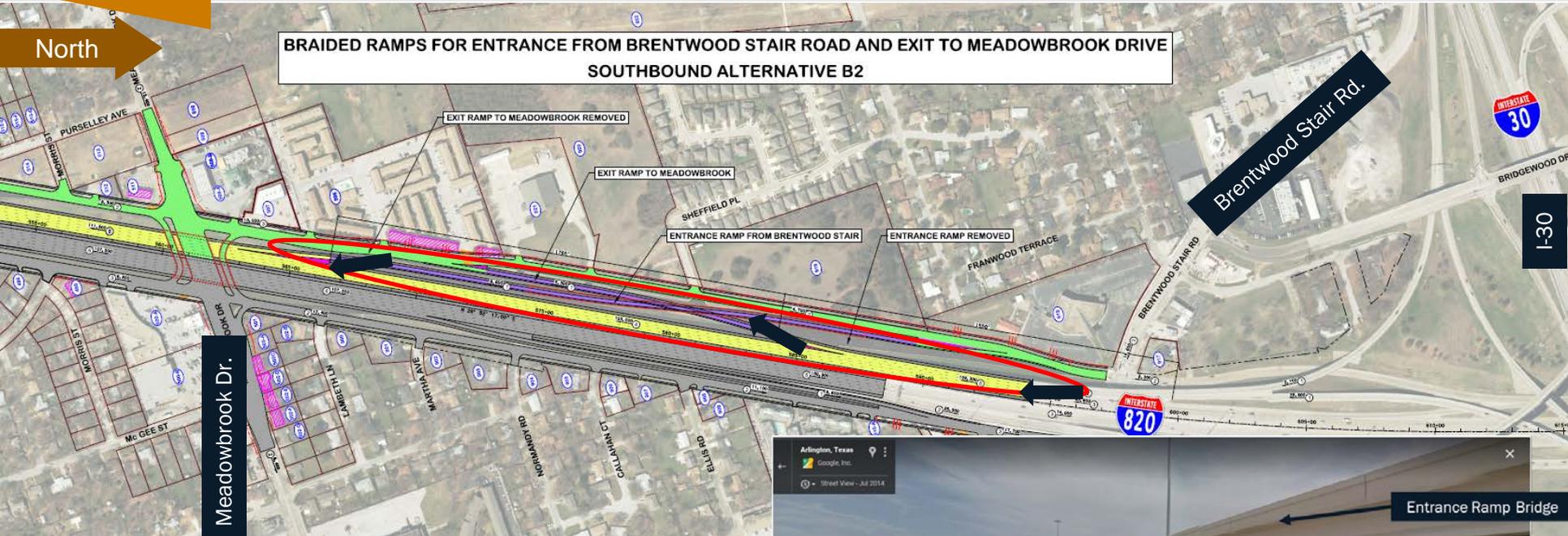
Cons:

- Traffic from Brentwood Stair must pass through a signal to enter mainlanes
- Distance between proposed southbound Meadowbrook exit and existing I-30 connections is less than the distance between the existing southbound Meadowbrook exit and I-30 connections

Based on input from Town Hall Meetings, Not Recommended for Further Evaluation



Southbound Entrance Ramp from Brentwood Stair – Alternative B2



Example of “Braided Ramps”

- Eastbound I-30 exit to Nolan Ryan Expwy/Baird Farm Rd/AT&T Way in Arlington

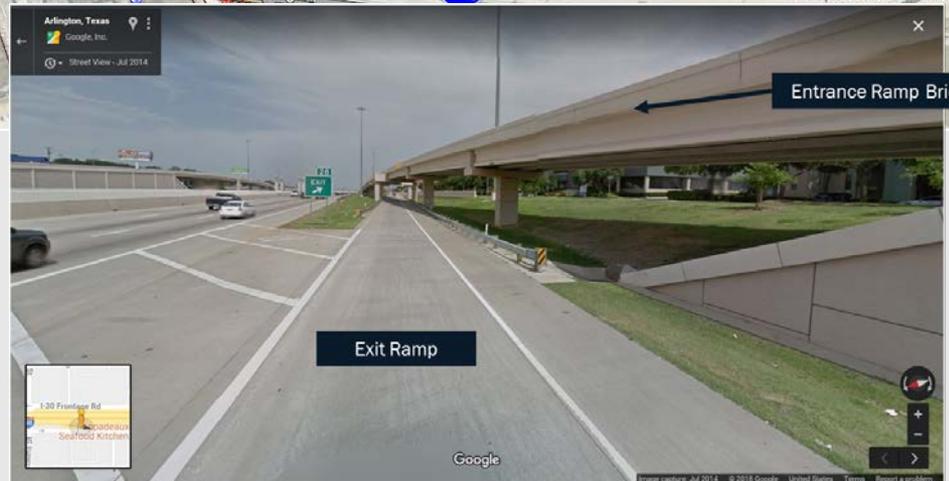
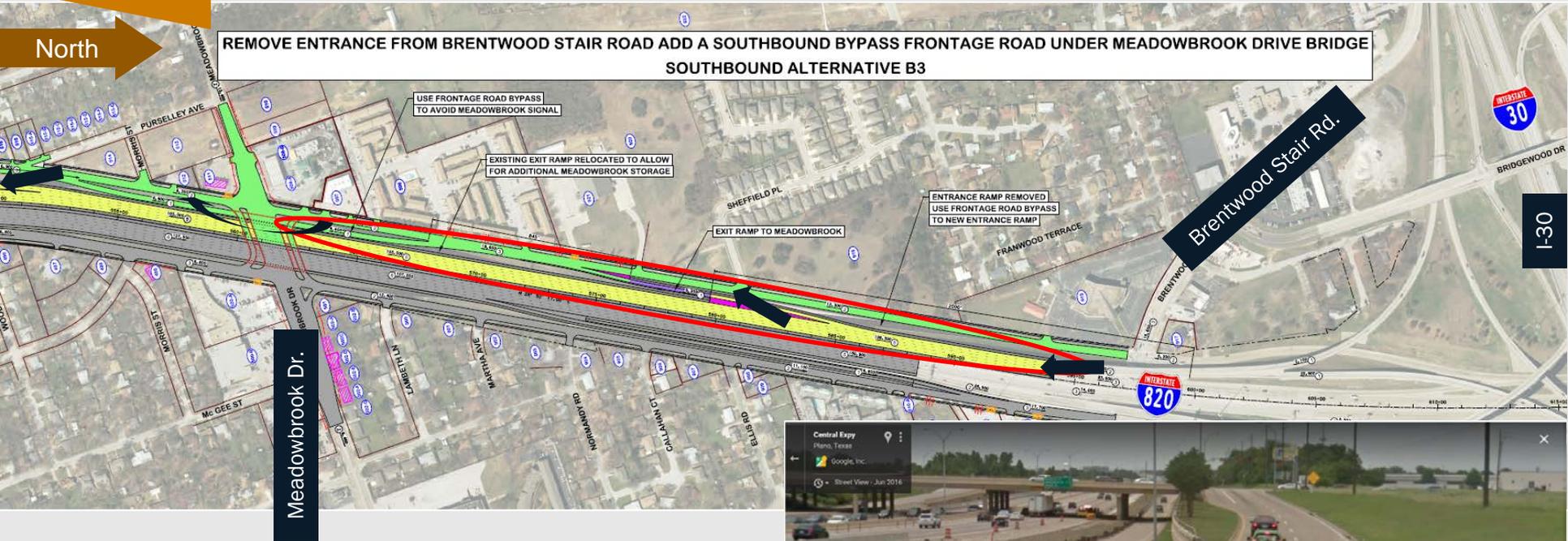


Image from Google.com street view



Southbound Entrance Ramp from Brentwood Stair – Alternative B3



Example of “Bypass Frontage Road Lane”

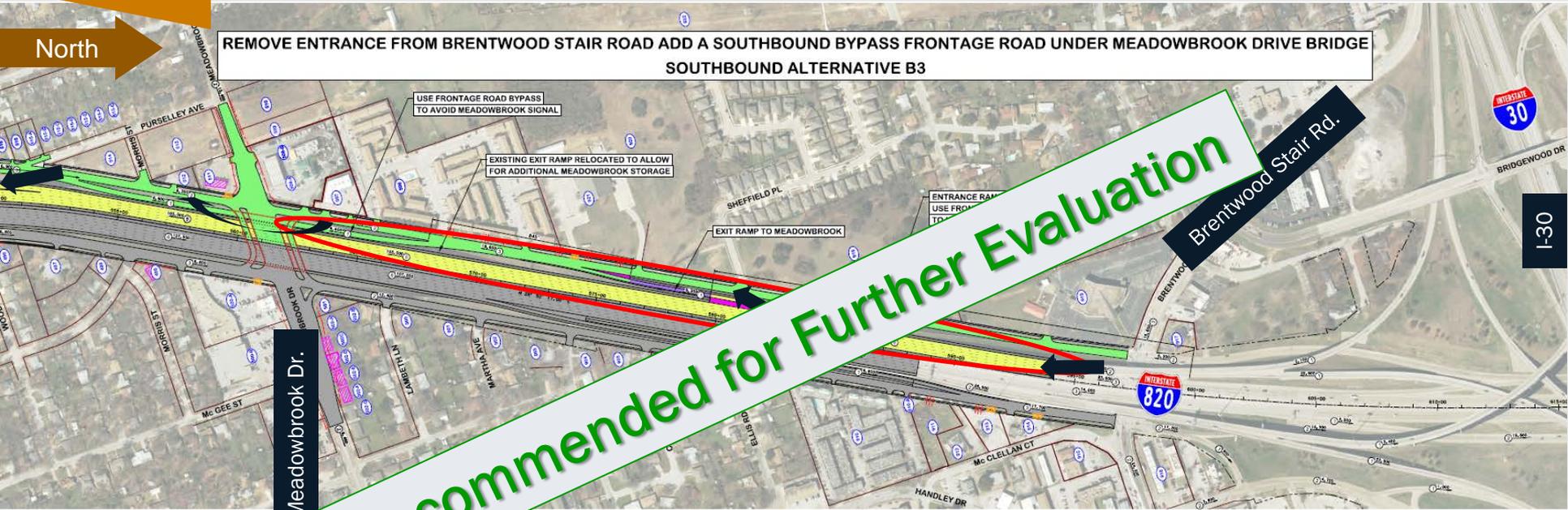
- Northbound US 75 at Legacy Dr in Plano



Image from Google.com street view



Southbound Entrance Ramp from Brentwood Stair – Alternative B3



Pros:

- Traffic southbound from Brentwood Stair bypass Meadowbrook signal
- 2000' ramp spacing meets minimum TxDOT and National Guidelines
- Relocated exit ramp to Meadowbrook provides for additional storage or queueing of vehicles from intersection

Cons:

- Ramp spacing between Meadowbrook exit and frontage road bypass undesirable
- Weaving between exit ramp and frontage road bypass
- Properties #516 and #517 do not meet recommended access control at exit ramp criteria



Southbound Entrance Ramp from Brentwood Stair – **Alternative B4**



Pros:

- Southbound ramps would be reconstructed to allow additional mainlane capacity

Cons:

- Heavy weaving traffic causes high congestion
- 1500' ramp spacing does not meet National Guidelines
- Less space from Meadowbrook to exit ramp for vehicle storage from signal
- Requires channelization to prevent access to properties #507 and #508-A from the Meadowbrook exit ramp
- Existing driveway along frontage road providing access to property #918 is removed—access maintained via Brentwood Stair Road



Craig Street Bridge Access / Pedestrian Bridge

- Existing Craig Street bridge has narrow sidewalks



Image from Google.com street view
Looking West on Craig Street



Craig Street Bridge Access / Pedestrian Bridge

- Proposed Craig Street bridge would be wider with shared use lanes (cars and bikes) and wide sidewalks

Proposed Craig St. Bridge: 1 shared use lane in each direction with Wide Sidewalks / Paths (see typical section below)

Proposed Craig Street





Craig Street Bridge Access / Pedestrian Bridge

- Existing pedestrian bridge to be removed because of vertical clearance/height conflicts
- Potential conflicts with overhead/high mast power lines
- Potential pedestrian bridge meeting American with Disability Act (ADA) requirements expected to be very long and high over roadway



Image from Google.com street view
Looking South on I-820



Craig Street Bridge Access / Pedestrian Bridge – Alternative Presented at Public Meeting



Pros:

- Wider Craig St Bridge with wide sidewalks and shared use lanes for bikes
- Cross over frontage roads and mainlanes
- Loop connections would have sidewalks

Cons:

- Removal of existing Pedestrian Bridge



Craig Street Bridge Access / Pedestrian Bridge – Alternative Pedestrian Bridge Concept

Pros:

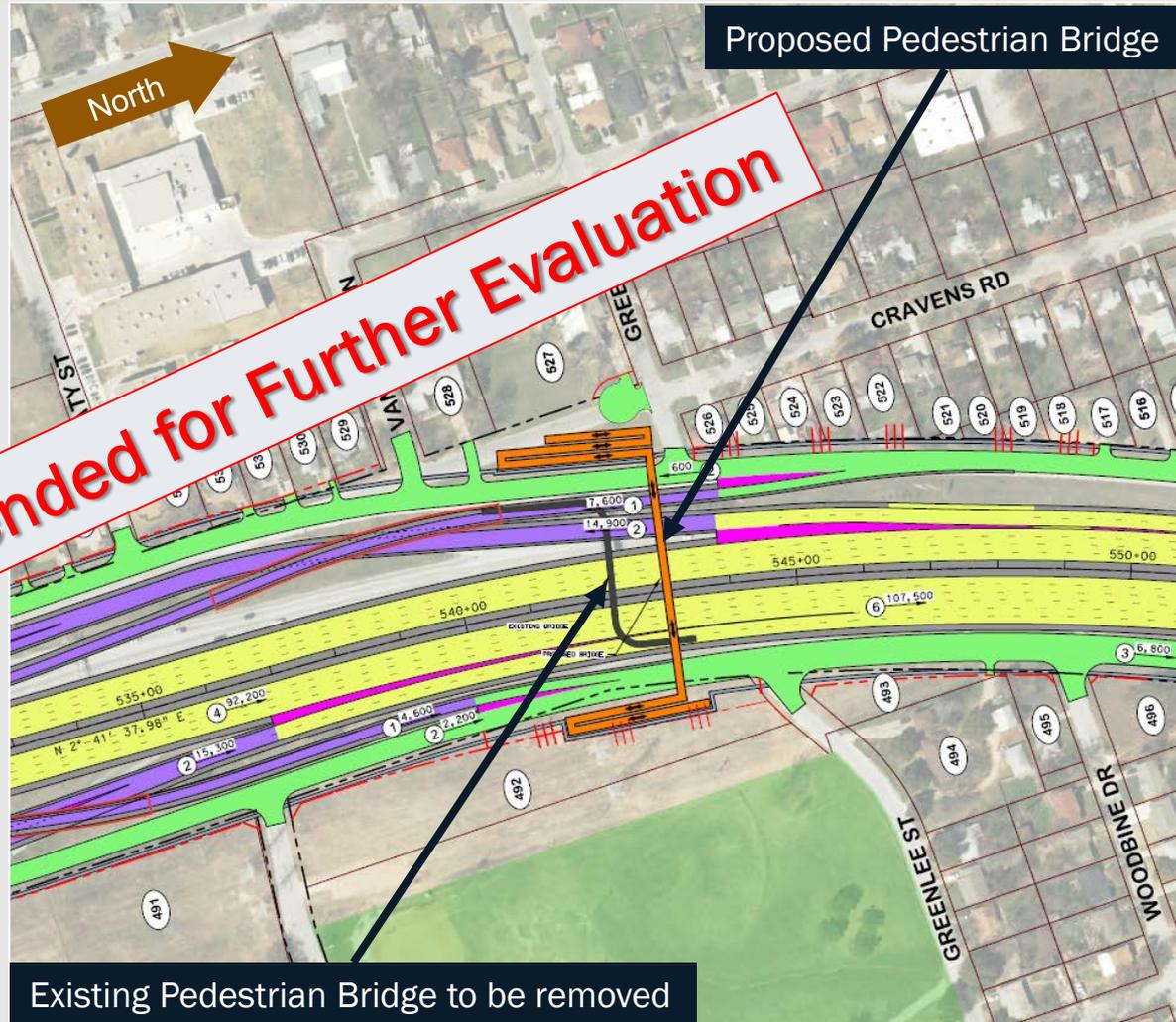
- Pedestrian bridge reconstructed
- Cross over frontage roads

Cons:

- Oncor transmission lines must be relocated much higher to avoid new pedestrian bridge
- Cravens Road access revised (cul-de-sac)
- Additional right-of-way required
- “Switch-back” required to meet ADA guidelines adds to travel length

Based on input from Town Hall Meetings, Not Recommended for Further Evaluation

Not Recommended for Further Evaluation





3. Past Activities (continued)...

- **Completed Level C/D and some Level B Sub-surface Utility Engineering**

- **Intersection Traffic Counts and Projections**
 - Currently under review by TxDOT

- **Refinement of Preferred Alternative**
 - Incorporation of the Northern I-820 Focus Study Input
 - Optimization of Alignments



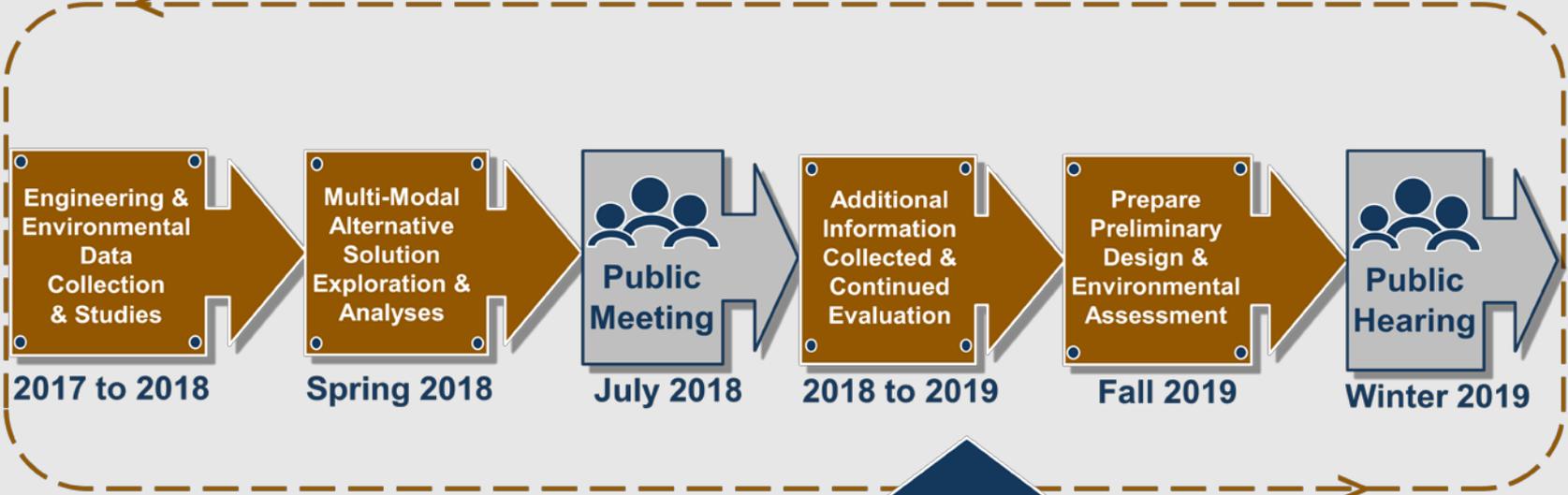
4. Current Status

- Schematic Development of Recommended Alternative
- Environmental Studies and Analyses
- Drainage Crossings Surveying
- Drainage Studies
- Traffic Analyses
- Initiate Interstate Access Justification Report
- Union Pacific Railroad Coordination
- Initiate Design-Visualization
- Continued Public Involvement and Outreach
- Newsletter



5. Next Steps

Public & Agency Involvement



National Environmental Policy Act (NEPA) Clearance
Spring 2020



Public & Agency Involvement



Public Input Continues

Future Public Hearing

On-Call Presentations

Town Hall Meetings

Meeting with Property Owners

Workshops



6. Other Issues / Discussion



Thank You for Attending!

Project Contact Information



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