



FORT WORTH DISTRICT

Southeast Connector Presentation at Transportation Town Hall

I-20, I-820 & US 287

August 16, 2018

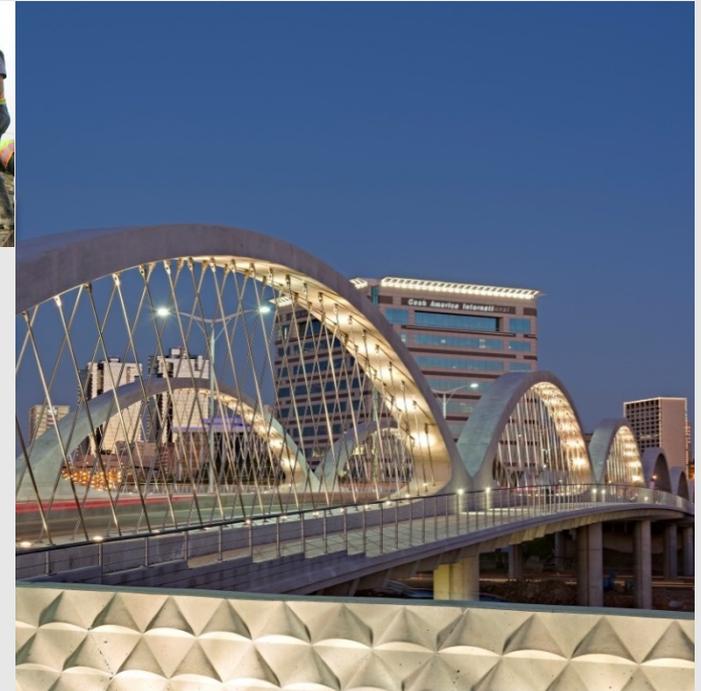


Photo by Liam Frederick



Agenda

- 1 Southeast Connector Study Area
- 2 Project History, Issues, and Constraints
- 3 Study Goals & Objectives
- 4 Recommended Alternative
- 5 Public / Agency Outreach & Involvement
- 6 July 19, 2018 Public Meeting – Stats & Feedback
- 7 Project Schedule
- 8 Project Contact Information
- 9 Questions



Southeast Connector Study Area

I-20

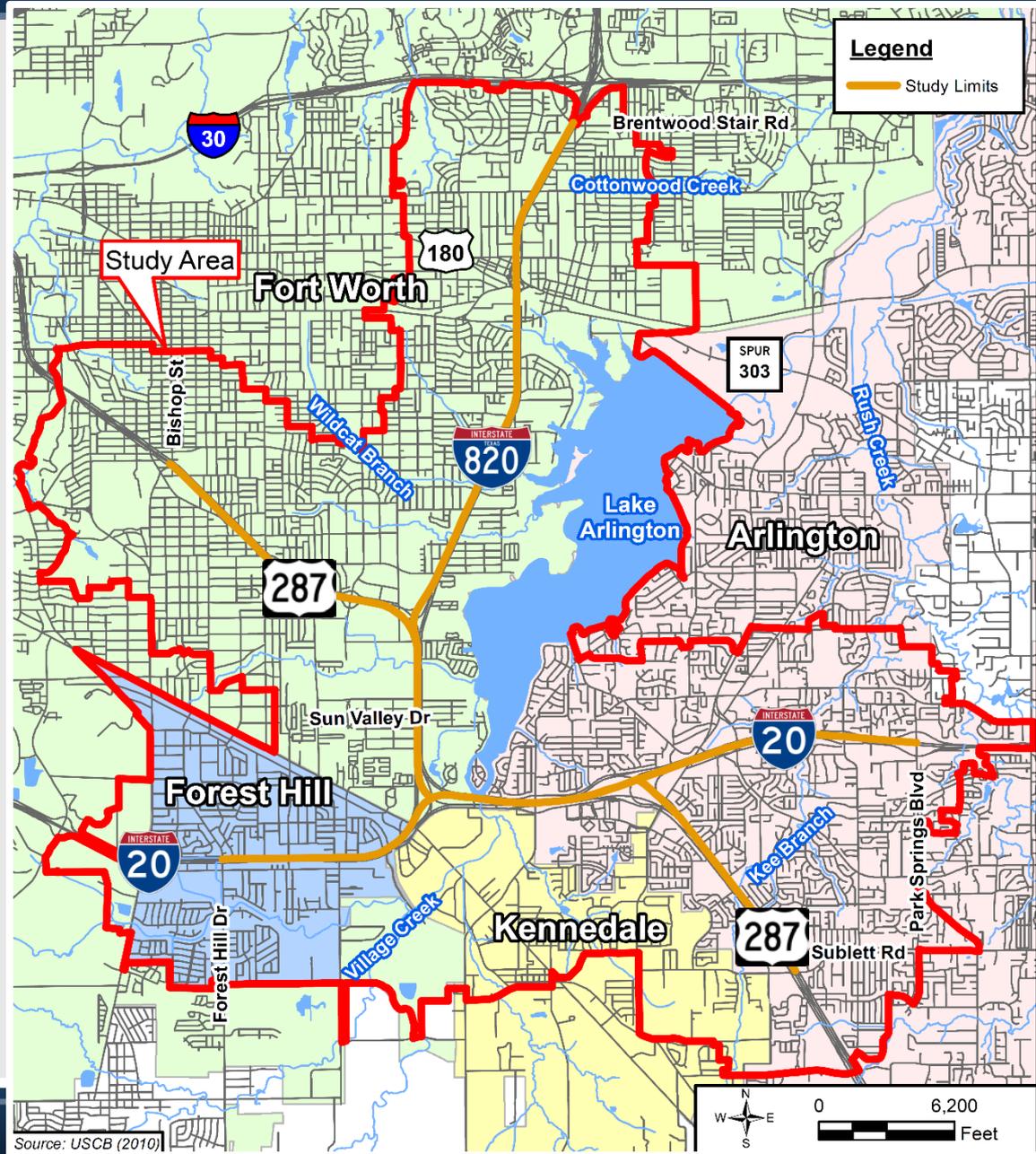
- From Forest Hill Drive To Park Springs Boulevard

I-820

- From I-20 To Brentwood Stair Road

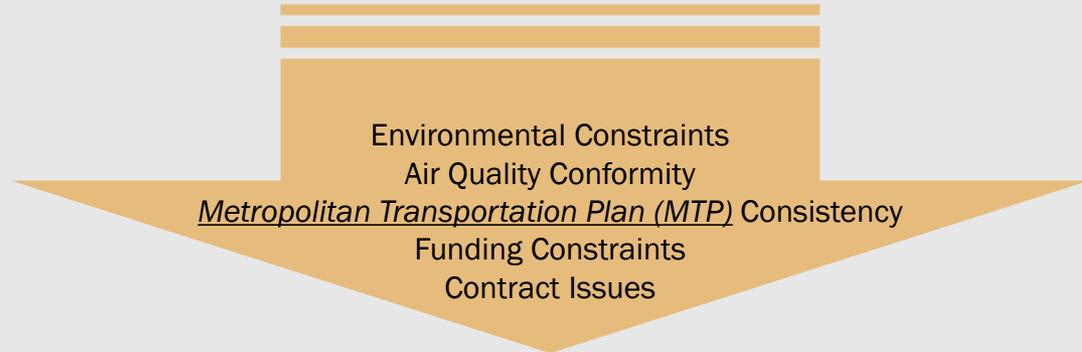
US 287

- From Bishop Street To Sublett Road





Project History



- April 2016 to Present Day
- Update Previous Study
 - Evaluate Alternatives
 - Analyze Future Traffic
 - Integrate with Mobility 2045
 - Produce Preliminary Design
 - Produce Environmental Assessment

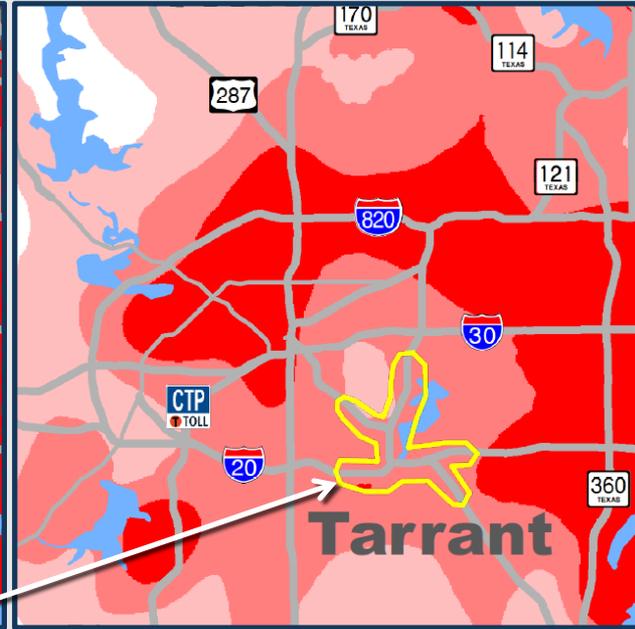
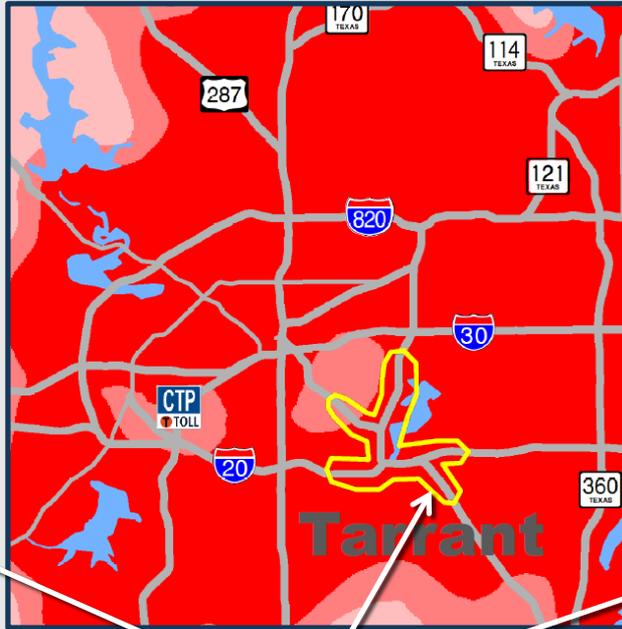
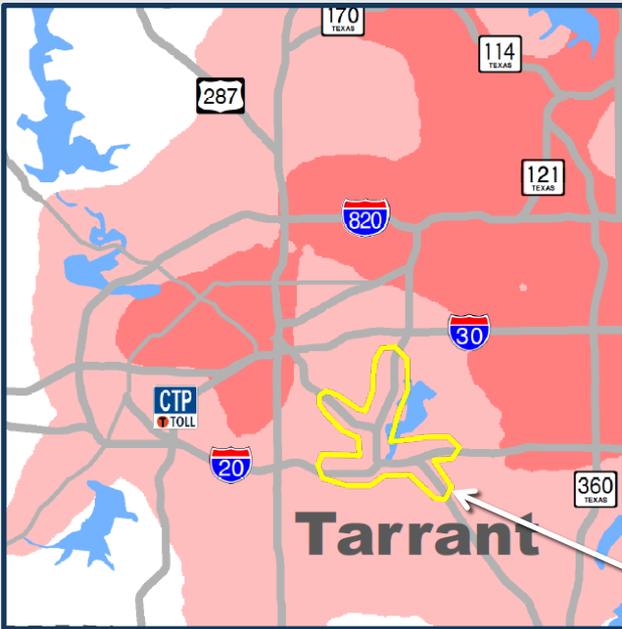


Congestion Levels / Delay

Year 2018
Current Congestion

Year 2045
No-Build Scenario

Year 2045
MTP¹ Build Scenario



¹ Metropolitan Transportation Plan (MTP)

Study Area

Congestion Index*

-  No Congestion
-  Light Congestion
-  Moderate Congestion
-  Severe Congestion

Source: MTP¹ Draft Mobility 2045, NCTCOG

* Congestion Index is based on a percent increase in travel time.

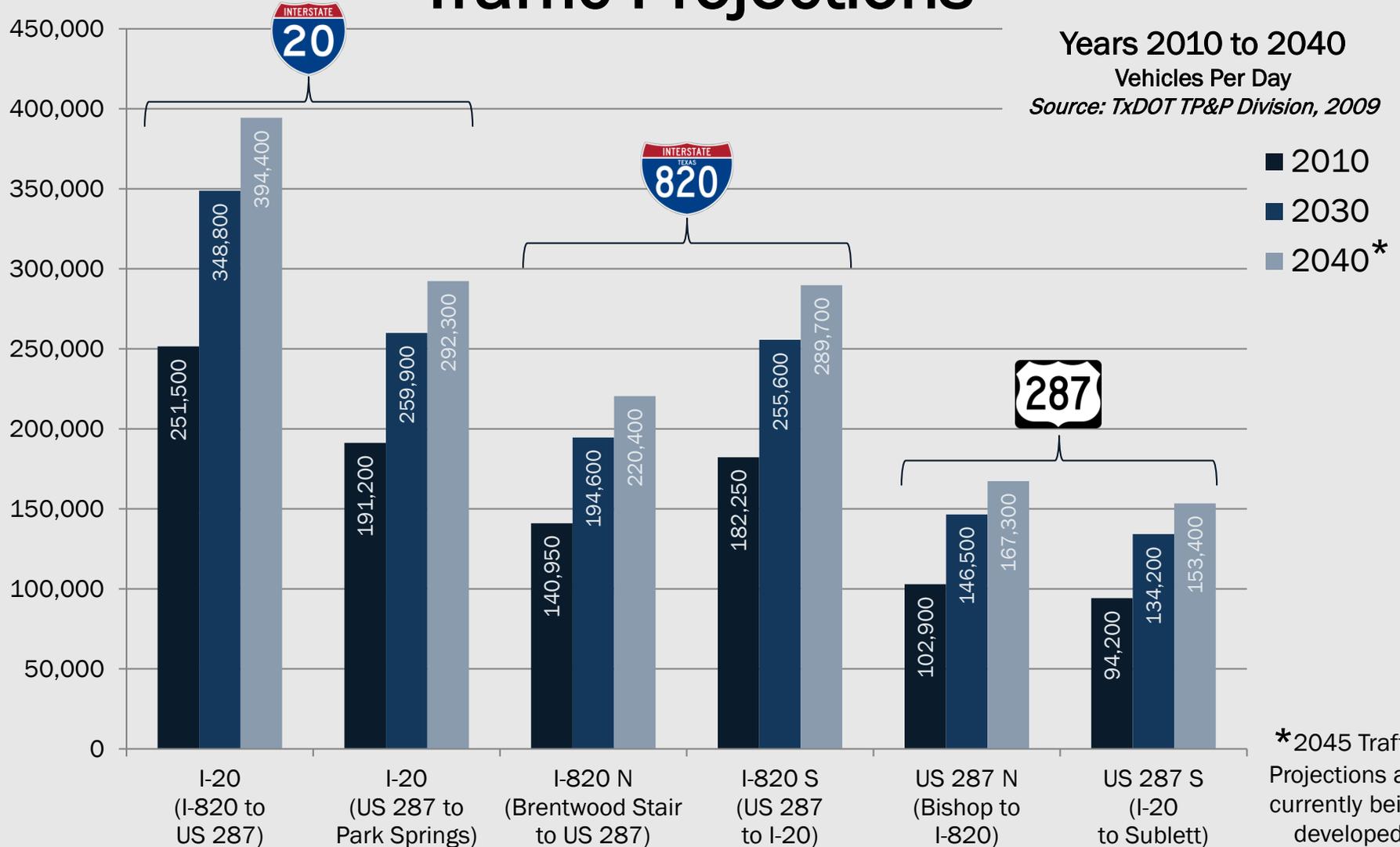


Traffic Projections

Years 2010 to 2040

Vehicles Per Day

Source: TxDOT TP&P Division, 2009



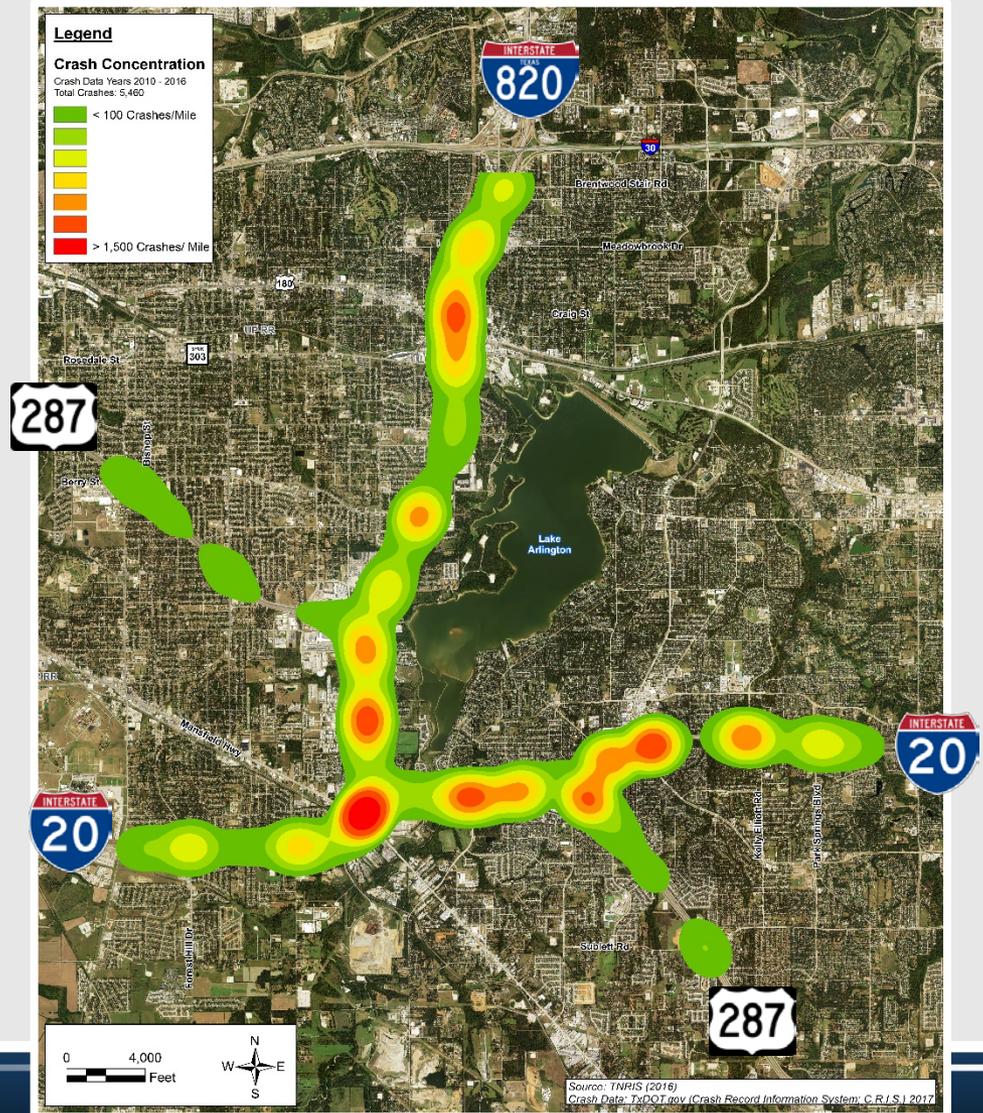
*2045 Traffic Projections are currently being developed



Crash History & Concentration

From 2010 to 2016

- 5,460 Crashes
- 47 Fatalities
- A main goal of the project is to improve safety

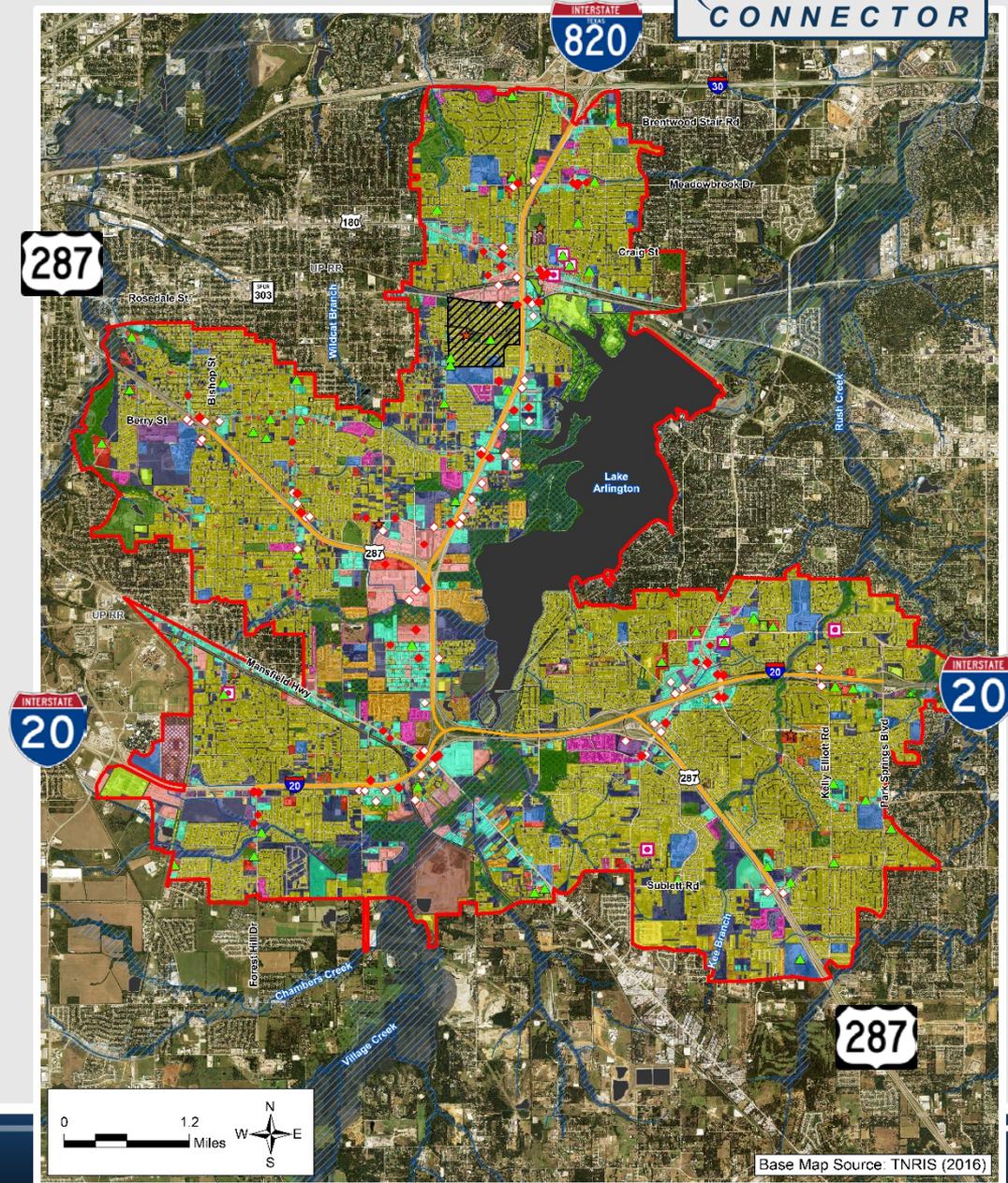




Study Area Constraints Map

LEGEND

- Project Limits
- Study Area
- Cemeteries
- Commercial
- Communication
- Education
- Group quarters
- Hotel/motel
- Industrial
- Institutional/semi-public
- Landfill
- Stadium
- Mobile home
- Multi-family
- Parks/recreation
- Railroad
- Ranch land
- Retail
- Single family
- Utility
- Vacant
- Woodland
- 100 Yr Flood Zone
- Carver Heights Local Historic District
- Stream
- Petroleum Storage Tank (PST)
- Leaking Petroleum Storage Tank (LPST)
- Places of Worship
- Recreation Center
- Historical Marker





Study Goals & Objectives

- Improve Safety
- Improve Mobility
- Improve Operations
- Provide Transportation Options
- Develop Safe Transportation Solutions
- Update and Modernize a Design from the early 1960s





Recommended Alternative

Along I-20 between I-820 and US 287



The alternative analyses recommend reconstructing existing interchanges and adding general purpose lanes and collector-distributor road systems to separate traffic movements and eliminate weaving.

Along I-820 between I-20 and US 287



The alternative analyses recommend widening and reconstruction of the general purpose lanes to align travel lanes and reduce major weaving movements.

Throughout the Project Study Limits



TxDOT is evaluating and recommending improvements promoting safety including reconstructing interchanges, widening or reconstructing general purpose lanes, modifying ramp locations to minimize weaving movements and eliminate left-hand exit ramps, reconstructing or providing additional frontage roads to incorporate shared use lanes that would accommodate vehicles and bicycles and adding sidewalks to accommodate pedestrians.



Recommended Alternative

The recommended alternative does not include tolled managed or express lanes.



**IMPROVE
SAFETY**



**IMPROVE
MOBILITY**



**IMPROVE
OPERATIONS**



**PROVIDE
TRANSPORTATION
OPTIONS**



**DEVELOP SAFE
TRANSPORTATION
SOLUTIONS**



Public / Agency Outreach & Involvement

Technical Work Group (TWG)

- **TWG Meetings**
- Representatives from Agencies, Local Governments, Chambers of Commerce, Schools:
 - TxDOT
 - FHWA
 - NTTA
 - Trinity Metro
 - NCTCOG
 - FWISD, AISD, KISD
 - Tarrant County
 - Cities (Arlington, Forest Hill Fort Worth, Kennedale, and Mansfield)
 - Southeast Tarrant Transportation Coalition
 - Others

Public Meeting & Hearing

- **Public Meeting** – Open House Format; Recommended Alternative and Alternatives Considered
- **Public Hearing** – Formal Presentation describing the Preferred Alternative and Environmental Studies
- **Advertisements**
- **Legal Notices**
- **Press Releases**
- **Public Input**
- **Public Comment**

Other Outreach

- Project Website through www.txdot.gov
Keyword Search:
Southeast Connector
- Newsletters
- Mailing List
- Postcards
- On-call Presentations / Briefings
- Meetings with Affected Property Owners (MAPOs)
- Workshops



July 19, 2018 Public Meeting – Quick Stats

- ▶ 273 Attended
 - ▶ 203 General Public
 - ▶ 14 Elected Officials
 - ▶ 56 Project Representatives

- ▶ 174 Written Comments
 - ▶ 33 Received at Meeting
 - ▶ 115 E-mailed
 - ▶ 26 Mailed





Public Input Continues

Future Public Hearing

On-Call Presentations

Town Hall Meetings

Meeting with Property Owners

Workshops



Comments in Study Area

A NB Entrance Ramp from Meadowbrook

– 88 Comments

B SB Entrance Ramp from Brentwood Stair

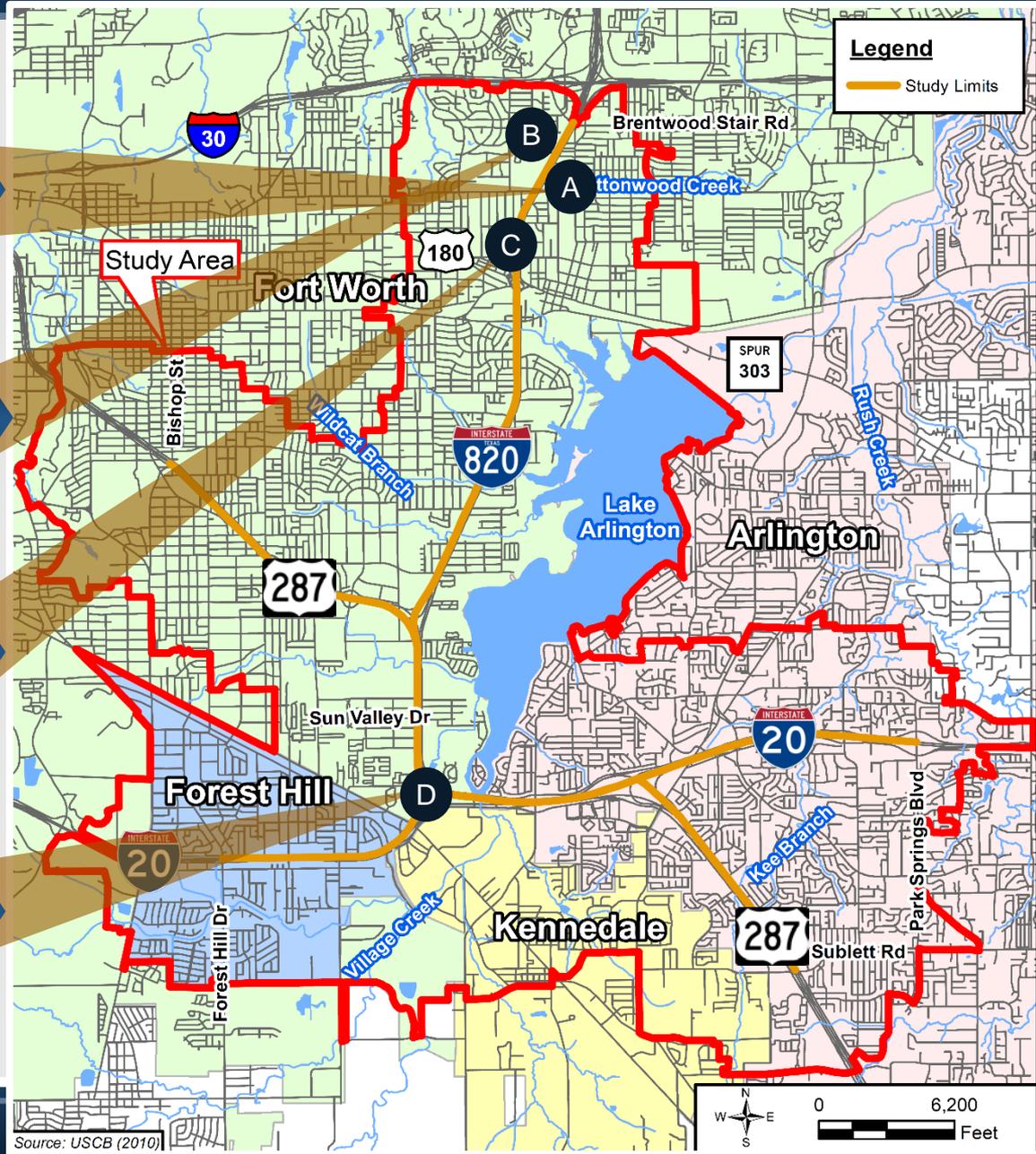
– 49 Comments

C Craig Street Bridge Access / Ped Bridge

– 25 Comments

D Noise Wall Study

– 51 Comments





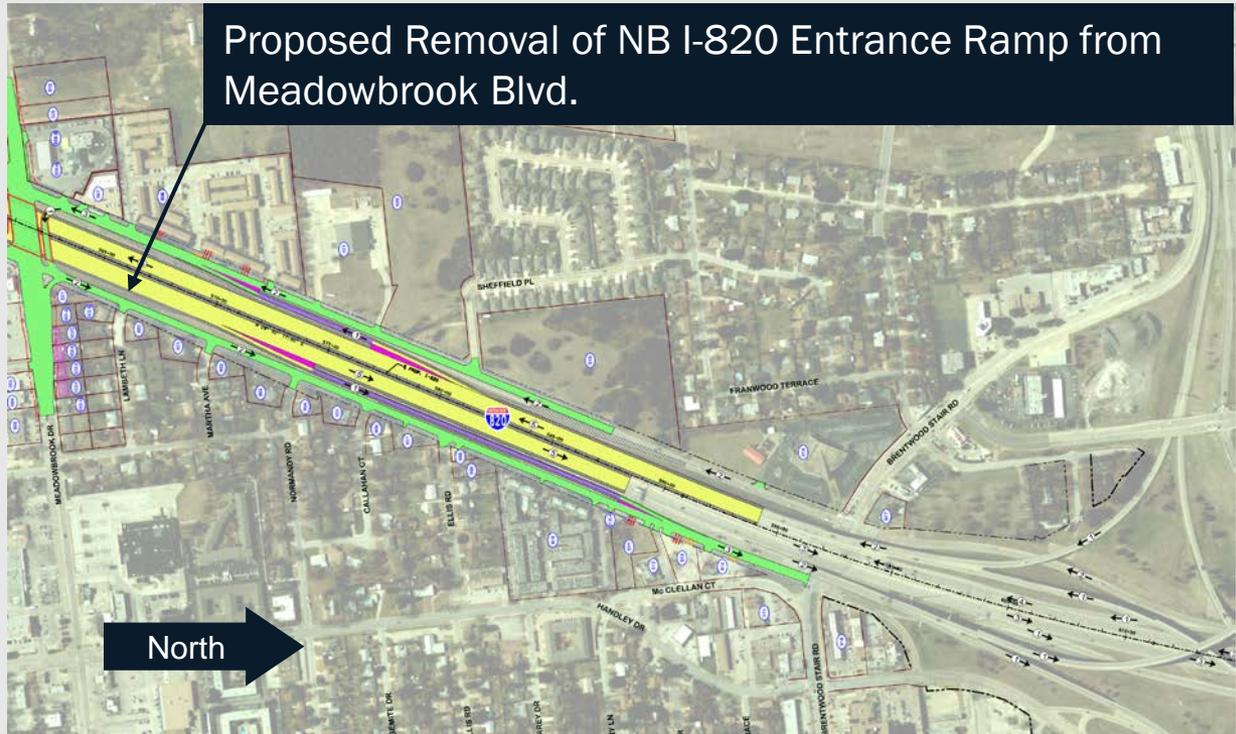
A NB Entrance Ramp from Meadowbrook

WHY?

- Conflicts with Exit to Brentwood Stair and I-30 Direct Connections causing heavily congested weaving movements
- Distance between ramps near minimum spacing lengths described in roadway design guidelines

ALTERNATE ROUTES

- Head south toward Lancaster Ave, use the improved U-turn lane, and take NB I-820 entrance ramp
- Head north through Brentwood Stair Rd intersection to go north on I-820 or east on I-30
- Head toward Brentwood Stair Rd and Bridgewood Dr to go west on I-30





A NB Entrance Ramp from Meadowbrook

ADDITIONAL ALTERNATIVES TO BE EVALUATED

- Adding entrance ramp from Meadowbrook Dr and exit ramp to Brentwood Stair Rd as they currently exist
- Adding entrance ramp from Meadowbrook Dr and removing the exit ramp to Brentwood Stair Rd
- Removing the exit ramp to Brentwood Stair Rd but adding a northbound bypass frontage road lane for Brentwood Stair Rd under Meadowbrook Dr bridge
- Adding braided ramps for entrance from Meadowbrook Dr and exit to Brentwood Stair Rd

Evaluate all alternatives for right-of-way impacts*, mobility, and traffic operations for safety.

***Most of these alternatives would require additional right-of-way.**



B SB Entrance Ramp from Brentwood Stair

WHY?

- Conflicts with I-30 Direct Connections and Exit Ramp to Meadowbrook Dr causing heavily congested weaving movements
- Distance between ramps near minimum spacing lengths described in roadway design guidelines

ALTERNATE ROUTE

- Head south through Meadowbrook Dr. and take improved entrance ramp to SB I-820

Proposed Removal of SB I-820 Entrance Ramp from Brentwood Stair Rd.





B SB Entrance Ramp from Brentwood Stair

ADDITIONAL ALTERNATIVES TO BE EVALUATED

- Adding entrance ramp from Brentwood Stair Rd with ramps as they currently exist
- Adding braided ramps (*ramps that are bridged, one over the other*) for entrance ramp from Brentwood Stair Rd and exit ramp to Meadowbrook Dr
- Removing entrance ramp from Brentwood Stair Rd but adding a southbound bypass frontage road lane under Meadowbrook Dr to access southbound entrance ramp

Evaluate all alternatives for right-of-way impacts*, mobility, and traffic operations for safety.

***Most of these alternatives would require additional right-of-way.**



C Craig Street Bridge Access / Ped Bridge

- Existing pedestrian bridge to be removed because of vertical clearance/height conflicts
- Potential conflicts with overhead/high mast power lines
- Potential pedestrian bridge meeting American with Disability Act (ADA) requirements expected to be very long and high over roadway
- Proposed Craig Street bridge would be wider with shared use lanes (cars and bikes) and wide sidewalks

Proposed Craig St. Bridge: 1 shared use lane in each direction with Sidewalks (see typical section below)

Existing Pedestrian Bridge to be removed





C Craig Street Bridge Access / Ped Bridge

ADDITIONAL ALTERNATIVE TO BE EVALUATED

- Plan for pedestrian facilities or bridge meeting ADA requirements

Evaluate alternative for right-of-way impacts*, mobility, and traffic operations for safety.

***This alternative would require additional right-of-way.**



D Noise Wall Study

- Noise Analyses would be conducted for entire study limits along I-20, I-820 and US 287
- Noise Abatement would be evaluated for all areas that have noise impacts



Project Schedule

Public & Agency Involvement



2017 to 2018 Spring 2018 July 19, 2018 2018 to 2019 Fall 2019 Winter 2019

National Environmental Policy Act (NEPA) Clearance [Finding of No Significant Impact (FONSI) anticipated]

Spring 2020

Public & Agency Involvement



Current Study Phase



Thank You for Attending!

Project Contact Information



www.txdot.gov

Search for: "Southeast Connector"



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