



Final Environmental Assessment

On Quintana Rd. from Harmon Ave. to McKenna Ave.

San Antonio, Bexar County, Texas

CSJ: 0915-12-520

June 2017

The environmental review, consultation, and other actions required by applicable Federal environmental laws for this project are being, or have been, carried-out by TxDOT pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 16, 2014, and executed by Federal Highway Administration and TxDOT. Submitted pursuant to 42 U.S.C. §4321, et seq. and 49 U.S.C. §303.

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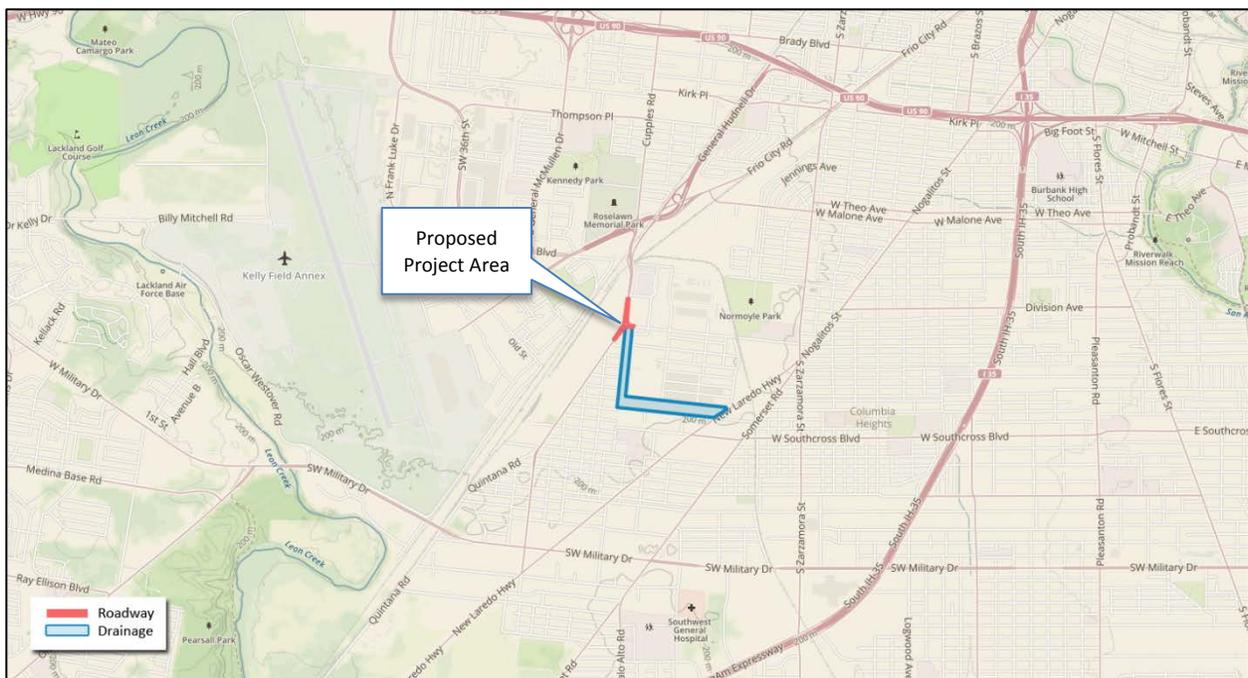
ACRONYMS

| | |
|----------|--|
| AADT | Average Annual Daily Traffic |
| AAMPO | Alamo Area Metropolitan Planning Organization |
| ACS | American Community Survey |
| APE | Area of Potential Effect |
| ASHTO | Association of State Highway and Transportation Officials |
| BEF | Biological Evaluation Form |
| BMP | Best Management Practices |
| CFR | Code of Federal Regulations |
| CoSA | City of San Antonio |
| CoSA-TCI | City of San Antonio Transportation and Capital Improvements Department |
| DHHS | Department of Health and Human Services |
| EA | Environmental Assessment |
| EKRP | East Kelly Railport |
| EIS | Environmental Impact Statement |
| EO | Executive Order |
| EPIC | Environmental Permits, Issues, and Commitments |
| FONSI | Finding of No Significant Impact |
| ISA | Initial Site Assessment |
| LEP | Limited English Proficiency |
| LWCF | Land and Water Conservation Fund |
| MAPO | Meeting with Affected Property Owner |
| MOU | Memorandum of Understanding |
| MPO | Metropolitan Planning Organization |
| MSAT | Mobile Source Air Toxic |
| MTP | Metropolitan Transportation Plan |
| NCHRP | National Cooperative Highway Research Program |
| NEPA | National Environmental Policy Act of 1969 |
| NOI | Notice of Intent |
| OWJ | Official with Jurisdiction |
| PCE | Tetrachloroethene |
| PM | Particulate Matter |
| PSA | Port San Antonio |
| RV | Recreational Vehicle |
| SW3P | Storm Water Pollution Prevention Plan |
| TCE | Trichloroethylene |
| TCEQ | Texas Commission on Environmental Quality |
| TERP | Texas Emissions Reduction Plan |
| THC | Texas Historical Commission |
| TIP | Transportation Improvement Program |
| TPDES | Texas Pollutant Discharge Elimination System |
| TPWD | Texas Parks and Wildlife Department |
| TxDOT | Texas Department of Transportation |
| TXNDD | Texas Natural Diversity Database |
| VPD | Vehicles Per Day |
| WPAP | Water Pollution Abatement Plan |

1.0 INTRODUCTION

Port San Antonio (PSA), in cooperation with the City of San Antonio Transportation and Capital Improvements Department (CoSA-TCI) and the Texas Department of Transportation (TxDOT), propose to improve traffic operation on Quintana Road by realigning the roadway at the Dunton Avenue intersection. Proposed drainage improvements will address existing flooding issues at the intersection by channeling water to a series of proposed channels and detention ponds to the south and east. The project is located in an urban area with mixed residential, commercial, and institutional uses. The general location of the proposed improvements is shown in **Appendix A** and below. Photographs of the project area are included in **Appendix B**.

The purpose of this Environmental Assessment (EA) is to study the potential environmental consequences of the proposed project and to determine if the consequences warrant the preparation of an Environmental Impact Statement (EIS). This EA was prepared to comply with environmental review rules established by both TxDOT and the National Environmental Policy Act (NEPA). It will be made available for public review after TxDOT considers all comments submitted regarding the proposed project. If it is determined that there are no significant adverse effects, TxDOT will prepare and sign a Finding of No Significant Impact (FONSI), which will be available for public viewing.

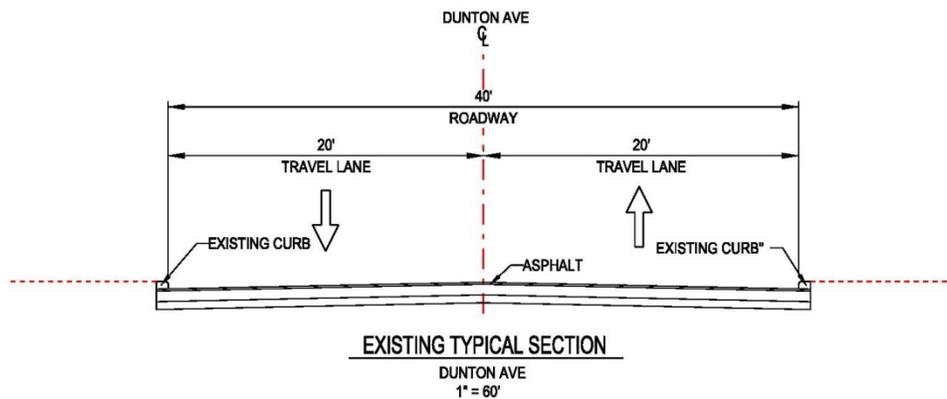
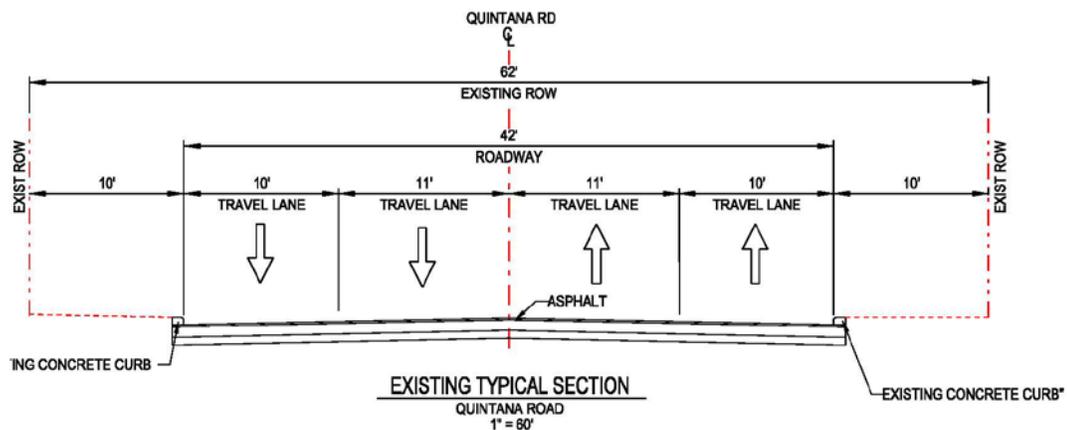


2.0 PROJECT DESCRIPTION

2.1 Existing Facility

Within the proposed construction limits, the existing design on Quintana Road consists of concrete curb, 10 feet of right-of-way on each side, and two 10 to 11 foot travel lanes in each direction. Dunton Avenue has two concrete curbs and one 20-foot travel lane in each direction. There are currently no pedestrian accommodations on Dunton Avenue and only five-foot sidewalks on the east side of Quintana Road, south of the Quintana/Dunton Intersection. Project schematics are included in **Appendix C**. The existing typical sections are shown below and in **Appendix D**.

Existing Typical Sections

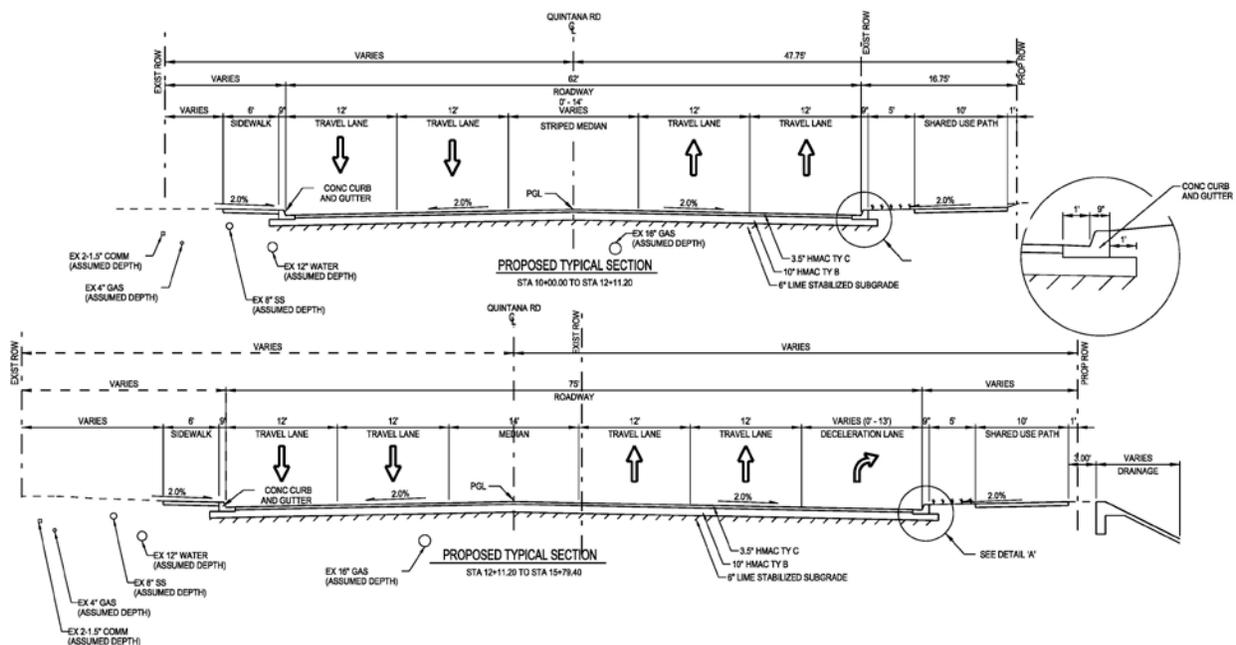


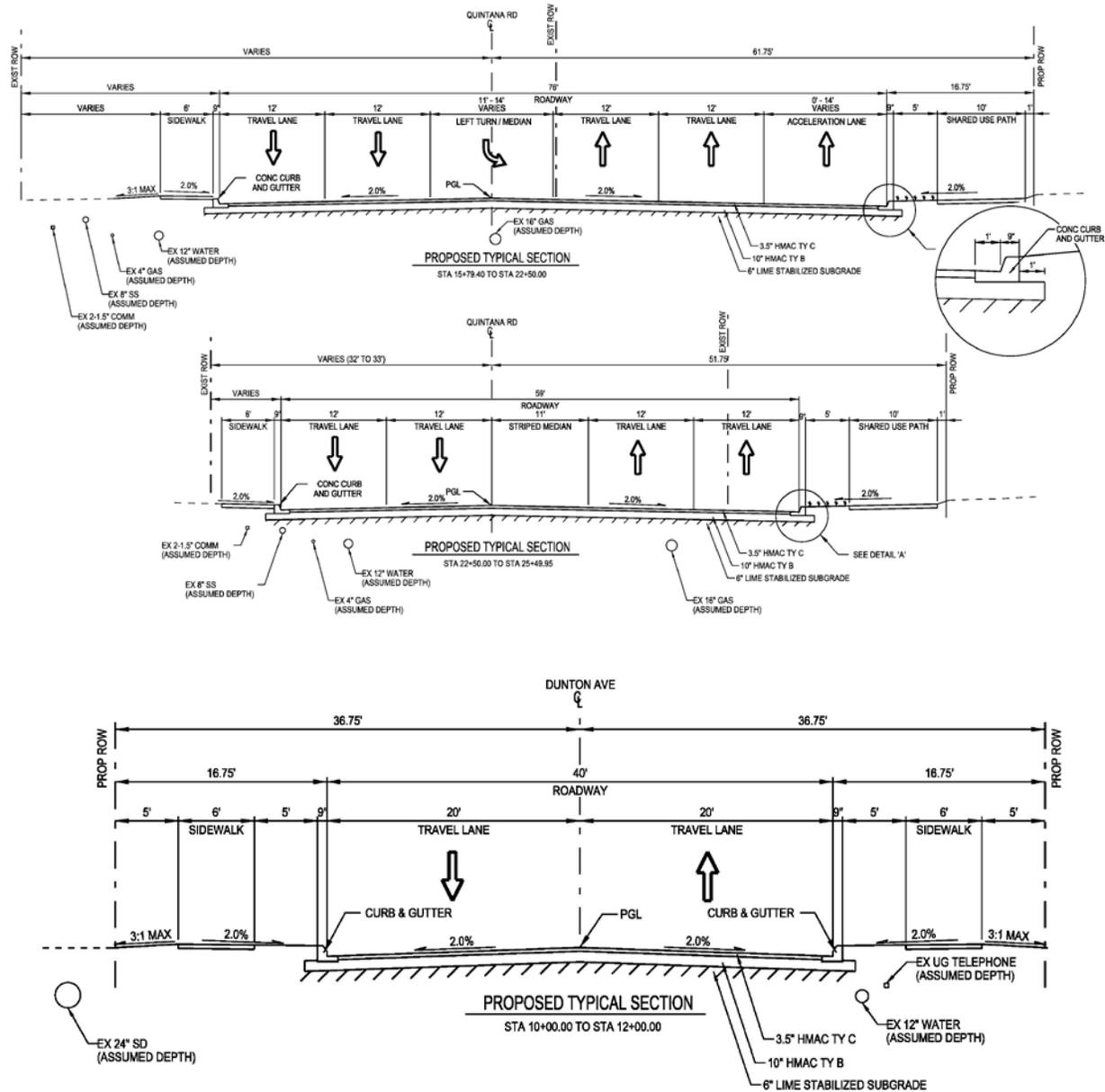
2.2 Proposed Project

The project has independent utility, and logical termini are from Harmon Avenue to McKenna Avenue (approximately 1,730 feet). The project would realign the curb line on the eastern side of Quintana Rd south of Harmon Avenue, so that northbound travel lanes within the project limits would properly line up with northbound travel lanes on the bridge north of Harmon Ave. As shown in the following proposed typical sections, Quintana Road will be widened along the east right-of-way line. Southbound and northbound travel lanes will be widened to 12 feet (from 10-foot outside, 11-foot inside widths). The project includes a dedicated right-turn lane from northbound Quintana Road onto Dunton Avenue; right turn lane from westbound Dunton Avenue onto northbound Quintana Road; left turn lane from southbound Quintana Road onto Dunton Avenue; and a curbed median on Quintana Road at the Dunton Avenue intersection.

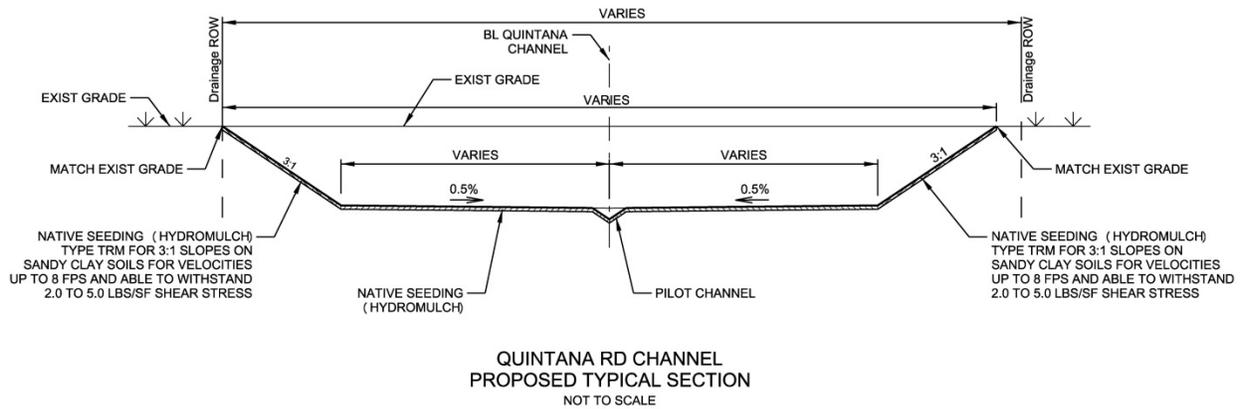
In addition, to preserve property access, uncurbed, unpainted medians will be used along Quintana Road. The project will improve traffic operation at this intersection by preserving traffic flow on Quintana Road main lanes. The traffic signals will be reconstructed at the Dunton Avenue intersection to accommodate the added lane movements. The project will include curbs, six-foot sidewalks on the west side of Quintana Road and on both sides of Dunton Avenue, a 10-foot wide shared-use path on the east side of Quintana Road, and driveways. Street excavation is estimated to be a maximum of 18 inches with an average of 12 inches. The proposed roadway typical sections are shown below and in **Appendix D**.

Proposed Typical Sections





To address flooding problems near the Quintana/Dunton intersection, the project will convey storm water into a proposed detention pond and channel beginning between Quintana and Lester roads. The improvement will then extend south, parallel to Lester Avenue, for about 2,400 linear feet within Port San Antonio property. Property between Quintana and Lester roads (two businesses and one residence), will be acquired to facilitate the detention pond and intersection widening. The remaining detention pond and channel will be within Port San Antonio property. The proposed typical drainage channel section is shown in the following graphic and in **Appendix D**.



The channel section will expand between Palestine Avenue and Fay Avenue to provide additional stormwater detention storage. A channel will extend parallel to Fay Avenue (+/- 3,100 linear feet) and expand where right-of-way is not constrained to provide additional detention storage. Where determined to be necessary, overflow structures will be constructed within the channel to provide stepped detention ponds so as to maximize detention storage within the available right-of-way. Stormwater from the project site will combine with other stormwater runoff and together outfall into an existing 72 inch diameter pipeline located just northeast of the intersection of Fay Avenue and New Laredo Highway.

The proposed project consists of 80 percent federal and 20 percent state funding. This project is listed in the Alamo Area Metropolitan Planning Organization's (AAMPO) *Mobility 2040* transportation plan and the Fiscal Year (FY) 2015-2018 Transportation Improvement Plan (TIP). As of July, 2016, the project will cost \$7.8 Million and be funded with Category 7 (STP MM) funds. The proposed letting date is July 2018. Construction is estimated to begin in October 2018 and would be completed by January 2020.

Projects in the TIP that are proposed for federal or state funds were initiated in a manner consistent with federal guidelines in *Section 450 of Title 23 Code of Federal Regulations (CFR)* and *Section 613.200, Subpart B, of Title 49 CFR*. Energy, environment, air quality, cost, and mobility considerations are addressed during TIP programming. Relevant Metropolitan Transportation Plan (MTP) and TIP pages are included in **Appendix E**.

3.0 PURPOSE AND NEED

3.1 Need

The proposed project is needed because the current roadway geometry will not accommodate anticipated traffic volumes and larger vehicles that are expected as a result of an increase in East Kelly Railport (EKRP) activity related to rail trans-loading operations. With this future growth, PSA anticipates that a greater number of large commercial vehicles will access the Quintana Road and Dunton Avenue intersection while traveling between U.S. Highway 90 and EKRP. The current intersection's design does not allow the efficient movement of such vehicles through the intersection, and PSA anticipates a considerable increase in congestion at the intersection. Additionally, the roadway intersection floods during frequent storm events. This poses a hazard for area motorists including residents, large commercial truck traffic, and St. Philip's College students and staff. Inadequate drainage in the project area also results in flooding on surrounding neighborhood streets.

3.2 Supporting Facts and Data

Traffic projections show an increase in Average Annual Daily Traffic (AADT) from 20,400 vehicles per day (VPD) (2016) to 28,600 VPD (2036). In addition, poor drainage at the intersection has resulted in considerable roadway flooding as shown in the September 2010 photos to the right.



3.3 Purpose

The purpose of the project is to address operational deficiencies caused by the current roadway geometry, and to address drainage and flooding issues occurring at the intersection. The project would provide protected turning movements at the intersection, Association of State Highway and Transportation Officials (ASHTO)-compliant realigned roadway curves, and protected pedestrian crossings.



4.0 ALTERNATIVES

4.1 Build Alternative (Preferred)

The Preferred Alternative (**Appendix C**) is a variation of Alternative 4 described in **Section 4.3** below, and similarly minimizes right-of-way requirements, the number of displacements, and adverse impacts to environmental justice populations.

4.1.1 Roadway

The roadway would be realigned, and the section located north of the Quintana/Dunton intersection would be expanded to allow for wider lanes and an additional acceleration lane for traffic turning right onto Quintana Road from Dunton Avenue. A median will be constructed, separating northbound and southbound traffic. A 10-foot wide shared use path will be constructed on the east side of Quintana Road with a six-foot sidewalk constructed on the west side, providing pedestrian access to the area. South of the intersection, a right-turn lane will be added for traffic moving from Quintana Road to Dunton Avenue. Dunton Avenue will remain two lanes with the addition of six-foot sidewalks on both sides. Traffic turning right will flow directly onto Quintana Road via a short turn lane with a yield.

4.1.2 Drainage

The proposed drainage component includes a channel and detention pond beginning immediately south of the Quintana Road and Dunton Avenue intersection along the western property line of PSA's EKRK property. It extends east along the southern boundary of the EKRK to an existing storm sewer outfall located near the intersection of Fay Avenue and New Laredo Highway. Drainage extending south of the intersection is the most direct route to the planned detention storage just north of Fay Avenue, which is a component of the drainage master plan for the redevelopment of EKRK property by PSA. The proposed channel size, alignment, and detention pond configuration are based on required outfall capacity and a need to store drainage flows as far upstream within the watershed and EKRK property as can be accommodated, while reserving space and capacity in the southern portion of EKRK for detention storage in consideration of future EKRK property development. 1.15 acres of existing PSA property currently in use as Quintana Park, will be converted to drainage use. See **Section 5.9** for discussion on the park. At PSA's request, the proposed drainage alignment also avoids impacting groundwater removal systems related to previous operations at Kelly Air Force Base, which are protected by deed restriction. The proposed drainage design better incorporates factors described, which were not considered during the PER, or were not considered to the degree needed to meet current project requirements.

The preferred alternative would require a total of 24.9236 acres of land (2.8 acres new right-of-way; 22.124 acres permanent easement) from ten parcels (See **Table 1**). It would cost an estimated \$7.8 Million to construct. This option is carried forward as the Build Alternative and is discussed in comparison to the No-Build Alternative throughout this EA.

4.2 No-Build Alternative

Under the No-Build Alternative, Quintana Road will not be improved and related drainage problems will not be addressed. The roadway intersection will operate as it currently does with no dedicated turn

lanes at the Dunton Avenue Intersection. There will be no new sidewalks or shared use path. New right-of-way will not be needed and there will be no residential or commercial displacements. The former RV park identified as “Quintana Park” will not be reduced in size. The intersection and adjacent neighborhood streets will continue to flood after heavy rain events.

The No Build Alternative will not address the identified need and purpose. The No-Build Alternative is carried forward in this document to provide a baseline for comparison to the Build Alternative.

4.3 Preliminary Alternatives Considered but Eliminated from Further Consideration

Two previous documents discussed proposed roadway alternatives, a single drainage option, and a No Build option. These documents include:

- *Preliminary Engineering Report (PER) - Quintana Road Paving and Drainage Improvements* (CDM Smith, July 2013). This document described three roadway design options (Alternatives 1 through 3) and a “fourth leg of intersection” option (CDM 2013).
- *Quintana Rd Widening and Drainage Project (CSJ: 0915-15-520), Bexar County, Texas – Constraints Technical Memo* (Pape-Dawson Engineers, June 20, 2014). This document analyzed the environmental, cultural, and socioeconomic impacts and “to select the least impactful, or preferred alternative,” with a particular emphasis on minimizing adverse impacts on environmental justice populations (P-D 2014). The constraints analysis discussed the three roadway alternatives from the PER, and introduced roadway Alternative 4. Consistent with the PER, the constraints analysis described several roadway alternatives and a single drainage option, which is described as follows:
 - Upstream end includes an earthen trapezoidal channel approximately eight feet deep, six foot bottom width, 4:1 side slopes (H:V), and 70-foot top width.
 - At the downstream end (where it ties into proposed detention facility), the channel depth transitions to approximately 12 feet, with a top width of 100 feet.
 - Proposed detention facility that would store approximately 32 acre-feet of volume.
 - A single proposed 24-inch reinforced concrete pipe (RCP) outfall structure would tie into an existing downstream 72-inch RCP outfall, which eventually drains to Six Mile Creek.

Roadway alternatives described in the PER and constraints analysis are summarized in the following bullets:

- Alternative 1: This alternative included a minimum 700-foot centerline radius coincident with the existing roadway centerline. It required right-of-way from both the east and west sides of Quintana Road. Standard 12-foot lane widths will increase the existing roadway width by two to four feet. This alternative would require 1.26 acres from 21 parcels.
- Alternative 2: This alternative incorporated a minimum 700-foot centerline radius, but held the existing east curb line to avoid penetration of existing properties on the east side. This alternative minimized impacts on individual properties while maintaining minimum design controls. This alternative would require 1.31 acres from 15 parcels.
- Alternative 3: This alternative incorporates a 1,200-foot center line radius. This alternative held the existing east curb line to avoid penetration of existing properties on the east side. This alternative would require 1.42 acres from 15 parcels.

- New street (“Fourth Leg”) of Quintana/Dunton Intersection: A new street (“fourth leg”) of the Quintana & Dunton Intersection was reviewed to provide a better point of access to Quintana Road for properties on the west side of the roadway. The street would also provide a safer and more convenient access without disrupting traffic operations along Quintana Road. This concept was considered based on (a.) existing land use on the west side of Quintana Road, which included old residential/commercial uses, vacant parcels, and parcels with unused structures and (b.) the goal of the 2010 “Kelly/S. San PUEBLO Community Plan Update,” which was to “Update the Community Commercial land use on Quintana between Frio City Road and Dunton in order to allow for more compatible land use around St. Philip’s College.” The PER’s fourth leg option was extensively studied and ultimately determined to be outside the scope of the funding of the project.
- Alternative 4: The environmental constraints analysis introduced Alternative 4, which was based on the three alternatives discussed in the PER. This alternative minimized right-of-way requirements, displacements, and adverse impacts to environmental justice populations.

Following the PER and *Constraints Technical Memo*, a draft schematic was created by the project engineer, reviewed by TxDOT, and further refined to incorporate TxDOT comments. This schematic describes the Build Alternative, which is presented in **Section 4.1** above.

5.0 AFFECTED ENVIRONMENT AND ENVIRONMENTAL CONSEQUENCES

In support of this EA, the following reports were prepared and approved by TxDOT Environmental Affairs Division:

- Air Quality Technical Report (RKEI 2016a)
- Archeological Background Study (RKEI 2016b)
- Biological Evaluation Form (RKEI 2016c)
- Community Impacts Technical Report (RKEI 2016d)
- Hazardous Materials Initial Site Assessment (ISA) (RKEI 2016e)
- Historical Studies Research Design (Meade and Hunt 2016a)
- Report for Archeological Survey (RKEI 2016f)
- Report for Historical Studies Survey (Meade and Hunt 2016b)
- Traffic Noise Technical Report (RKEI 2016g)
- Water Resources Technical Report (RKEI 2016h)

Based on the above technical studies, scoping, and thorough analysis, it was determined that the proposed project would have no impact on the following resource topics: air quality (permanent impacts), navigable waters; General Bridge Act/Section 9 waters, Section 303(d) waters, Section 404 Waters of the U.S. including wetlands, Bald and Golden Eagle Protection Act, Fish and Wildlife Coordination Act, farmlands, threatened or endangered species, and traffic noise. The reports may be inspected and copied upon request at TxDOT's San Antonio District Office.

The following sections summarize technical studies and topics as outlined in TxDOT's *Environmental Handbook: Preparing an Environmental Assessments* (TxDOT 2016b) and *Environmental Assessment Outline* (TxDOT 2016c).

5.1 Right-of-Way and Displacements

A total of 24.9236 acres of right-of-way is required for this project (**Table 1**), which includes land owned by an individual, an institution (Alamo Community College District), and the project sponsor (PSA). Two adjacent businesses will be displaced, including a muffler shop (accessed from Quintana Road) and a restaurant (accessed from Quintana Road and Lester Avenue). These commercial properties are located directly south of the Quintana/Dunton intersection. One residential lessee at 203 Lester Avenue will also be displaced. The properties requiring displacement (203 Lester Avenue, 1110 Quintana Road, and 1106 Quintana Road) are currently leased by a single owner to two commercial and one residential occupant. A summary of a meeting with the affected property owner (MAPO) is included in **Section 8.1**. PSA and TxDOT will comply with the Uniform Act to ensure that the owners are treated fairly, consistently, and equitably. New right-of-way requirements are summarized in **Table 1**.

Table 1. Right-of-Way Summary

| BCAD PARCEL ID | ADDRESS | TOTAL PARCEL SIZE (ACRE) | LAND PROPOSED AS | TOTAL ROW REQUIRED | % OF PARCEL REQUIRED | USE | OWNER |
|----------------------|--------------------|--------------------------|---|----------------------|----------------------|---|----------------------------------|
| 423065 ^a | 1206 Quintana Rd. | 0.1398 | New ROW | 0.1398 | 100% | Residential (lease) | Private (single owner) |
| 423064 ^a | 1306 Quintana Rd. | 0.1533 | New ROW | 0.1533 | 100% | Residential (lease) | |
| 423071 | 203 Lester Ave. | 0.0346 | New ROW | 0.0346 | 100% | Residential (lease) | |
| 423072 | 203 Lester Ave. | 0.1213 | New ROW | 0.1213 | 100% | Vacant | |
| 423061 | 1110 Quintana Rd. | 0.3953 | New ROW | 0.3953 | 100% | Muffler Shop | |
| 423060 | 1106 Quintana Rd. | 0.0884 | New ROW | 0.0884 | 100% | Restaurant | |
| 423063 | 203 Lester Ave. | 0.1607 | New ROW | 0.1607 | 100% | Residential (lease) | |
| 423062 | 203 Lester Ave. | 0.4362 | New ROW | 0.4362 | 100% | Residential (lease) | |
| 1192958 ^b | Hubert Harmon Ave. | 50.6620 | New ROW | 0.5381 | 1% | St. Philip's College | Alamo Community College District |
| 1107668 | 438 N. Tayman St. | 335.807 | New ROW (0.7319 ac.); Permanent Easement (22.124 ac.) | 22.8559 ^c | 7% | Multiple (East Kelly Railport tenants, Quintana Park) | Port San Antonio |
| Totals | | 387.9986 | | 24.9236 | | | |

a. Leased as part of residence at 203 Lester Ave.

b. Right-of-way to be donated by ACCD.

c. 1.15 acres of Quintana Park will be used for the project, and is included in this acreage (See **Section 5.9.3**).

As shown in **Table 1**, Alamo Community College District will donate 0.5381 acre of land along the southwestern perimeter of the St. Philip's College campus. As shown in the table above, 22.8559 acres of land from Port San Antonio will be used for the project as follows:

- 0.7319 acre of PSA-owned land through which Dunton Avenue crosses, would be dedicated to the project for intersection improvements.
- 22.124 acres of PSA-owned land will be used for the majority of the drainage channel and detention pond improvements, and will become a permanent drainage easement granted by PSA to CoSA.

PSA is currently reviewing the need for a potential temporary construction easement on the west side of Quintana Road, south of Harmon Avenue, at the primary entrance to an H.E.B. construction facility. The work at this location will involve offsite drainage improvements to direct stormwater into the proposed underground drainage system to prevent excess drainage from entering the roadway and is covered under the reviews conducted. The need, precise location, and dimensions are currently under review.

The No Build Alternative would not require the conversion of residential, commercial, and institutional land to transportation and drainage use.

5.2 Land Use

As shown in **Appendix F-1**, the proposed project (Quintana Road from Harmon Avenue to McKenna Avenue) is located in a mixed commercial, industrial, residential, recreational, and institutional area. It is between properties occupied by the former Kelly Air Force Base, currently under redevelopment by Port San Antonio. The project will require 2.0677 of acres of commercial, residential, and institutional land (**Table 1**) to be converted to transportation use, which will be owned and operated by CoSA.

Under the No-Build Alternative no adjacent land would be acquired. No impact (adverse or beneficial) would be expected.

5.3 Farmlands

Based on a project scoping analysis, it was determined that neither the Build Alternative nor the No-Build Alternative would have an impact on farmlands.

5.4 Utilities and Emergency Services

The proposed project would require the relocation of several underground and overhead utilities, including San Antonio Water System (SAWS) water and sewer lines, City Public Service (CPS) gas lines and overhead electricity lines, and potential impact to transmission lines in the proposed detention pond area. VIA Metropolitan Transit's (VIA) bus route 524 (shown in the inset graphic provided by VIA) is located in the project area, and there are four nearby bus stops. VIA has and will continue to attend utility review meetings, and the project sponsor would add a bus pad at an existing bus stop.

The proposed project would not permanently limit access for emergency services to residences, businesses, or public establishments in the project area. During construction, at least one lane in each direction would be maintained to allow continued access for emergency response vehicles.

Under the No-Build Alternative utility relocations and upgrades would not be required and no emergency services would be affected.

5.5 Bicycle and Pedestrian Facilities

To comply with the U.S. Department of Transportation Policy Statement on Bicycle and Pedestrian Accommodation Regulations and Recommendations, as well as TxDOT's policy for bicycle and pedestrian accommodation, the project would include six-foot sidewalks on the west side of Quintana Road and on both sides of Dunton Avenue. It would also include a 10-foot wide shared-use path on the east side of Quintana Road.

PSA coordinated with VIA in October 2015, and VIA provided the location of routes and stops in the project area. VIA Metropolitan Transit's (VIA) bus route 524 is located in the project area, along with four nearby bus stops. VIA will continue to attend utility review meetings and PSA will add a bus pad at an existing bus stop.

The No Build Alternative would not result in changes of temporary increases in travel times. There would be no addition of sidewalks or a shared-use path on the east side of Quintana Road.

5.6 Community Impacts

5.6.1 Community Profile

Based on the results of a Community Impacts Technical Report (RKEI 2016d), it was determined that within the Quintana Community Neighborhood Association boundaries, there are seven public schools, a charter school, a designated public city park, a recreational area, a pre-school/daycare, eight places of worship, a fire station, a community center, and one healthcare facility. Occupied housing in the project area ranges between approximately 87 percent and 100 percent and is similar to the comparison areas (census tracts). A Community Features Map is included in **Appendix F-2**.

5.6.2 Travel Patterns and Access

Lester Avenue would be partially closed as part of the intersection improvements. This street currently extends from Dunton Avenue south to Griffin Avenue (+/- 980 linear feet). The section from Dunton to McKenna (+/- 625 linear feet) currently allows only one-way traffic to the south, and would be permanently closed. With the exception of the renter at 203 Lester Ave. who would be displaced, other residents would continue to have primary access via Quintana Road.

Although construction could potentially result in temporary impacts to local businesses (e.g., reduction in patrons that may be deterred by construction activity), access to businesses would be maintained throughout construction. The project is not expected to result in considerable long-term changes to access or travel patterns in the vicinity (i.e., it is not on new location, would not add additional travel lanes, and would not result in major horizontal or vertical modifications to existing roadways or intersections)(RKEI 2016d).

The No Build Alternative would not have the potential to disrupt travel patterns and access.

5.6.3 Community Cohesion

The proposed improvements would use a portion of a former RV park, informally referred to as "Quintana Park," which is located adjacent to Lester Avenue and Dunton Avenue. According to a letter from PSA (property owner) dated April 5, 2016, the site remained vacant and unused for over 20 years. In 2014 the site was equipped with picnic tables, a park sign was installed, and it was opened to the community for recreation. The accessible portion of the site is approximately 4.7 acres in size¹ (from near Griffin Ave. north to Dunton Ave.), and it consists largely of open space. There are no defined goals or official plans for recreational facilities, or immediate plans to transfer it to the City of San Antonio's Parks and Recreation Department. The drainage improvements are designed to retain as much continued recreational access as possible after project completion. Public access to the property would be limited or restricted during construction for safety reasons. Based on the limited amenities available, the partial closure of this recreational area is not expected to significantly affect community cohesion (RKEI 2016d).

¹ The park is not separately platted and is part of a larger tract owned by PSA, which is identified in **Table 1** as BCAD Parcel ID 1107668.

The No Build Alternative would not have the potential to disrupt community cohesion.

5.6.4 Environmental Justice

An environmental justice analysis was completed in accordance with Executive Order (EO) 12898 “Federal Actions to Address Environmental Justice in Minority Populations and Low-Income Populations.”

2016 U.S. Department of Health and Human Services (DHHS) poverty guidelines were reviewed. The median household incomes in two census tracts within the project area are below the 2016 poverty threshold (\$24,300 for a family of four). These same census tracts have a higher percentage of persons below the poverty level when compared to the comparison area (San Antonio). In addition, racial and ethnic composition of the study area and comparison areas was assessed. Minority populations in the study area blocks range between 0 percent and 100 percent, and are present at a scale similar to the comparison area Block Groups (DHHS 2016). No disproportionately high and adverse impacts are anticipated from the proposed project. See **Section 5.6.2** for planned measures to include Environmental Justice populations in the public involvement process (RKEI 2016d).

One residential and two business displacements (all renting from a single property owner) would occur, and right-of-way acquisition would be in compliance with the Uniform Act. Although a portion of Lester Avenue would be permanently closed, with the exception of the single residential displacement (203 Lester Avenue), most commercial/residential access within the project limits is from Quintana Road; therefore, no substantial changes to access would occur. The project consists of intersection improvements (dedicated turn lanes to improve operation) and drainage improvements to reduce flooding; therefore, due to the nature of the project, travel patterns would not be significantly altered.

After reviewing income and race/ethnic data for the project area and larger comparison areas, impacts described above are not considered to be disproportionately high and adverse. See the following section for planned measures to include minority populations with limited English-speaking ability in the public involvement process.

The No Build Alternative would not have the potential to cause disproportionately high and adverse effects on minority populations or low-income populations.

5.6.5 Limited English Proficiency

Based on data from the 2010–2014 American Community Survey (ACS) for project area block groups, there is a significant percentage of the area population that may be considered persons with Limited English Proficiency (LEP). To ensure full and fair public participation, all public outreach activities will include measures to include Spanish-speaking populations in the decision-making process. This will include publishing meeting/hearing notices in English and Spanish, and making Spanish-language translation available during these outreach activities (RKEI 2016d).

5.7 Visual/Aesthetic Impacts

The proposed roadway improvements would be located primarily within existing public roadway right-of-way and land owned by PSA. Proposed new pavement, curbs, sidewalks, and a 10-foot wide shared-

use path may be considered by some to be an aesthetic improvement over the current roadway condition. A new drainage channel would be constructed through Quintana Park, approximately between Dunton Avenue and Griffin Avenue, which may be considered by some to be an adverse visual impact (i.e. replacing a portion of public open space with a drainage feature). The remainder of the drainage channel would largely replace existing flat terrain that is fenced in, regularly mowed and maintained, and inaccessible to the public.

Under the No-Build Alternative, there would be no impact (adverse or beneficial) to the visual aesthetics of the area.

5.8 Cultural Resources

5.8.1 Archaeology

An intensive pedestrian survey was conducted on July 8, 2016 under Texas Antiquities Permit 7708. The Area of Potential Effect (APE) encompassed approximately 43.19 acres bounded by Quintana Road, Dunton Road, Offutt Road, and New Laredo Highway. No previously recorded archaeological sites were located within the APE. No new archaeological sites were recorded as a result of this survey.

Much of the APE had been previously disturbed by the installation of utilities and the construction of asphalted and/or concreted surfaces. While conducting the survey, it appeared that much of the APE had been graded to create a level landscape in proximity to the airfield. Prior to fieldwork, twenty-two (22) shovel tests were planned. Due to the location and extent of asphalt-covered surfaces and the multitude of utility lines within the project area, the number of shovel tests initially proposed was reduced to ten (10) in order to investigate areas that exhibited fewer disturbances.

Shovel tests excavated in the southern portion of the APE encountered caliche road base to approximately 25 cm below surface. The soil encountered below the road base consisted of mottled clay mixed with caliche and gravels. Shovel tests excavated within the Commander's House property revealed less-disturbed soils, but no buried historic or prehistoric artifacts were encountered in the shovel test units. Shovel tests excavated within Quintana Park exhibited uniform soils. One shovel test, located at the northern edge of Quintana Park, produced a single artifact: a lithic core fragment. A lithic core fragment is a piece of chert (flint) that has been modified by humans during the process of manufacturing stone tools. The significance of the find is that it is the only artifact recovered during the survey that indicated a prehistoric human occupation of the area. However, no additional artifacts were noted in the shovel tests or in units excavated nearby. Therefore, this artifact was determined to be an isolated find.

No backhoe trenches were excavated within the APE due to the lack of locations that would have the potential for producing deeply buried cultural deposit (i.e., no proximity to streams). A review of historic aeriels and observations during the survey indicated that no streams or natural water drainages were present within the APE that would have deposited sediments on cultural features.

Results of the pedestrian survey indicate that there is a low potential for encountering buried, intact, significant cultural deposits. There are no historic cemeteries located within the APE; therefore none will be impacted. Since no cultural deposits and/or intact features have been identified during the survey, and much of the APE appears to have been graded and levelled to create a flat high-visibility landscape

in the vicinity of the airfield, it is unlikely that intact cultural deposits exist within the project APE. Therefore, it was recommended that no further archaeological investigations were warranted within the APE, and that the improvements should proceed as planned. Results of the pedestrian survey and recommendations were submitted to the TxDOT Archeological Division for review, and TxDOT coordinated with the Texas Historical Commission (THC) on September 13, 2016. The THC concurred on September 15, 2016 (**Appendix G**). Should changes be made to the project APE, further archaeological investigations may be required.

All project-generated documentation, and the single artifact collected during the survey, will be permanently curated at the University of Texas at San Antonio-Center for Archaeological Research.

Under the No-Build Alternative, there would be no potential to affect archaeological resources within the project limits.

5.8.2 Historic Properties

A reconnaissance survey was performed within the project area for potential effects within the APE, 150 feet beyond the proposed ROW. Mead & Hunt initiated consultation with multiple consulting parties: CoSA's Office of Historic Preservation via CoSA-TCI on May 31, 2016; the Port of San Antonio on May 31, 2016; and the Bexar County Historical Commission on June 27, 2016. In compliance with the Section 106 PA, TxDOT historians determined project activities will not affect historic properties. Individual project coordination with SHPO is not required. There is one historic property in the APE, #29A which is not affected by the proposed project.

The project APE included the Commander's House property. A standing structure survey of the property conducted several years ago found the property not to be eligible for listing on the National Register of Historic Places. A revisit and new assessment of the property in its current condition found no reasons to change the original assessment of the property (Meade and Hunt 2016b).

The reconnaissance survey identified no additional historic resources in the APE beyond those already determined eligible for the National Register. There is no potential for effects to historic properties.

Under the build alternative, the project poses no adverse effect to historic properties.

5.9 DOT Act Section 4(f), LWCF Act Section 6(f), and PWC Chapter 26

5.9.1 Section 4(f), U.S. Department of Transportation Act

Section 4(f) of the 1966 U.S. Department of Transportation Act is codified in the United States Code (USC) in 23 USC 138 and 49 USC 303. It protects (a.) publicly-owned, significant and accessible parks, recreation areas, and wildlife and waterfowl refuges and (b.) significant historic and archeological sites.

The proposed improvements would use a portion of Quintana Park, which is located adjacent to Lester Avenue and Dunton Avenue entirely within PSA-owned property. The site is a former RV park that PSA equipped with picnic tables and signage in 2014, and has been available to the community for recreation. It is approximately 4.7 acres in size. Approximately 1.15 acres of the park would be converted to drainage use.

TxDOT intends to pursue a de minimis 4(f), and on March 28, 2017, a Public Hearing was held that addressed the park, and allowed the public an opportunity to provide input on the proposed use of the park. The hearing met notification requirements for Section 4(f).



During the public hearing, several public commenters expressed concern over the park. Specifically, they commented about the potential to enhance the park by adding wellness activities (e.g. walking/biking trails), proving regular maintenance, opportunities for public events, lighting, and dedicated park features (e.g. playground equipment), and asking that project not “de-emphasize it as a park.” Although new park improvements/features are not part of the scope under approved MPO funds, several measures to mitigate park impacts are proposed. The walking/jogging track will be restored to maintain a continuous loop. The park will be accessible by a new sidewalk and shared use path along Quintana Road and Dunton Avenue. The project will incorporate a walkway over the detention pond at McKenna Avenue to provide direct access to the park from the adjacent community. PSA will continue advocating for the neighborhood to identify ways the park can be improved, and helping to identify and secure the necessary resources. See **Section 7.4**.

Under the No-Build Alternative, there would be no potential to affect Section 4(f) properties within the project limits.

5.9.2 Section 6(f), Land and Water Conservation Fund Act

The Land and Water Conservation Fund Act (LWCF Act) of 1965 established a funding source that assists states and federal agencies in meeting present and future outdoor recreation demands and needs (Texas Parks and Wildlife Department [TPWD] 2014f). Once land has been purchased or developed, partially or entirely, with LWCF assistance, it is considered a Section 6(f) property, and no Section 6(f) property shall be wholly or partly converted to a use other than public outdoor recreation uses without the approval of the National Park Service (TPWD 2014f). State park/recreation agencies are responsible for complying with LWCF provisions, and in Texas the TPWD’s Recreation Grants Branch maintains the current list of LWCF-assisted properties. According to a list of local park grant recipients (including LWCF grants) provided by the Recreation Grants Branch on August 14, 2016, Quintana Park is not listed (**Appendix G**); therefore, there will be no Section 6(f) properties affected.

Under the No-Build Alternative, there will be no potential to affect Section 6(f) properties within the project limits.

5.9.3 Chapter 26 of the Parks and Wildlife Code (PWC)

Chapter 26 of the Parks and Wildlife Code applies to any project that requires the use or taking of any public land designated and used (prior to the arrangement of the project) as a park, recreation area, scientific area, wildlife refuge, or historic site. According to TxDOT, “if the property has some formal

designation as one of the five listed land uses, and some basis for concluding the property is being used in accordance with that designation, Chapter 26 applies” (TxDOT 2016d). Park signage, a walking path and mile markers, and picnic tables are in place at Quintana Park; therefore, Chapter 26 applies.

TxDOT determined that there is no feasible and prudent alternative to the use or taking of Chapter 26 protected land, and the Quintana Road Project includes all reasonable planning to minimize harm to the land as a park, resulting from the use or taking. The proposed project requires 1.15 acres of the approximate 4.7-acre park (See footnote in **Section 5.6.3**). Approximately 3.56 acres of the park area would remain available for recreational use upon completion of the drainage channel. On March 28, 2017, a public hearing, publicly-advertised in accordance with PWC Chapter 26 requirements, was held that addressed the park, and allowed the public an opportunity to provide input on the proposed use of the park. See **Section 7.4**.

Under the No-Build Alternative, there will be no potential to affect PWC Chapter 26 properties within the project limits.

5.10 Water Resources

The following sections summarize the results of a Water Resources Technical Report (RKEI 2016h).

5.10.1 Clean Water Act Section 404

Based on a project scoping analysis, it was determined that neither the Build Alternative nor the No-Build Alternative will impact waters subject to Section 404 of the Clean Water Act.

5.10.2 Clean Water Act Section 401

Based on a project scoping analysis, it was determined that neither the Build Alternative nor the No-Build Alternative will require a Section 404 permit; therefore, compliance with the Texas Commission on Environmental Quality’s (TCEQ) Water Quality Certification Program, established under Section 401 of the federal Clean Water Act, is not required.

5.10.3 Executive Order 11990 Wetlands

Based on a project scoping analysis, it was determined that neither the Build Alternative nor the No-Build Alternative will impact wetlands; therefore, compliance with Executive Order 11990, Protection of Wetlands, is not required

5.10.4 Rivers and Harbors Act

Based on a project scoping analysis, it was determined that neither the Build Alternative nor the No-Build Alternative will impact waterways protected by the Rivers and Harbors Act.

5.10.5 Clean Water Act Section 303(d)

One Section 303 (d) listed threatened or impaired water is present within a five mile radius of the project (Sixmile Creek, Segment ID 1911E); however, the project will not discharge directly into this listed water body. Based on a project scoping analysis, it was determined that neither the Build Alternative nor the No-Build Alternative will impact threatened or impaired waterways.

5.10.6 Clean Water Act Section 402

Section 402 of the Clean Water Act sets forth the National Pollutant Discharge Elimination System (NPDES) program, which, in Texas, is administered by TCEQ under the Texas Pollutant Discharge Elimination System (TPDES) program. The proposed project will disturb approximately 20 acres of land. Therefore, construction activities must comply with TCEQ's TPDES General Permit for Construction Storm Water Discharges (TXR150000, issued March 5, 2013). Since it is considered a "large construction activity" (greater than five acres), the project will obtain coverage by preparing and implementing a Storm Water Pollution Prevention Plan (SWP3), posting a construction site notice, submitting a Notice of Intent (NOI) and associated fee to TCEQ, and otherwise complying with the permit terms.

The proposed project is located within the boundaries of a regulated Municipal Separate Storm Sewer System (MS4). Specifically, the proposed project is located within the Phase I San Antonio MS4 and will comply with the applicable MS4 requirements. The Phase I MS4 Permit (TPDES Permit No. WQ0004284000) is held and administered by The City of San Antonio Transportation and Capital Improvements Department, San Antonio Water System, and TxDOT and allows each of the MS4 operators (i.e., permittee) to discharge storm water from the MS4 to waters of the State.

Under the No-Build Alternative, there will be no potential to impact Section 402 waters.

5.10.7 Floodplains

Based on a project scoping analysis, it was determined that neither the Build Alternative nor the No-Build Alternative will impact floodplains.

Under the No-Build Alternative, there will be no potential to impact floodplains.

5.11 Biological Resources

The following sections summarize the results of a Biological Evaluation Form (RKEI 2016c), which was completed by a qualified biologist as defined in the 2013 Memorandum of Understanding (MOU) between TxDOT and the TPWD.

5.11.1 Vegetation

No riparian vegetation will be impacted, but trees will be removed within the project limits during drainage channel construction. A review of Texas Natural Diversity Database (TXNDD) data and the Texas Conservation Action Plan did not indicate adverse impacts to remnant vegetation. In order to avoid impacts caused by invasive species, and to provide beneficial landscaping under the Build Alternative, disturbed areas will be reseeded according to TxDOT specifications and in compliance with

EO 13112 where practicable. Trees will be removed within the project limits during drainage channel construction.

Under the No-Build Alternative, there will be no potential to affect vegetation within the project limits.

5.11.2 Wildlife

The proposed project does not have the potential to impact Bald or Golden Eagles. There is potential for nesting birds to be present in the project area during construction. While trees in the project area will be removed to construct the drainage channel, Bird BMPs outlined in the Best Management Practices Programmatic Agreement between TxDOT and TPWD under the 2013 MOU will be incorporated to protect migratory bird nests (See **Section 8.0**).

5.11.3 Threatened and Endangered Species

Based on the Biological Evaluation Form (BEF), it was determined that neither the Build Alternative nor the No-Build Alternative will have an impact on any habitat for listed threatened or endangered species, or species of greatest conservation need.

5.11.4 TPWD Coordination Triggers

Coordination with TPWD under the 2013 MOU was not required because no coordination conditions or MOU triggers were met.

5.12 Air Quality

This section Potential air quality impacts were analyzed in detail and are documented in an Air Quality Technical Memo (RKEI 2016a).

The project is located in an area in attainment or unclassifiable for all national ambient air quality standards (NAAQS); therefore, the transportation conformity rules do not apply. Under the Build Alternative, the proposed project is not expected to cause a meaningful change in traffic volume. Carbon monoxide levels will not be exceeded and Mobile Source Air Toxic (MSAT) levels will remain the same with the exception of temporary impacts from construction activities, which could include temporary increases in particulate matter (PM) and MSAT. The potential impacts of PM emissions will be minimized by using fugitive dust control measures contained in standard specifications, as appropriate. In addition, the Texas Emissions Reduction Plan (TERP) provides financial incentives to reduce emissions from vehicles and equipment. TxDOT encourages construction contractors to use this and other local and federal incentive programs to the fullest extent possible to minimize diesel emissions. Considering the temporary and transient nature of construction-related emissions, the use of fugitive dust control measures, the encouragement of the use of TERP, and compliance with applicable regulatory requirements; it is not anticipated that emissions from construction of this project will have any significant impact on air quality in the area.

Under the No-Build Alternative, there will be no change in air quality impacts (adverse or beneficial) relative to the existing condition.

5.13 Hazardous Materials

Based on a Hazardous Materials Initial Site Assessment (ISA) (RKEI 2016e), 15 features of potential concern were identified within TxDOT-recommended search distances. These included the former Kelly Air Force Base, historical dry cleaning facilities, and several petroleum storage tank facilities (some of which are reported with a history of leaking). A map of potential hazardous materials concerns is included in **Appendix F-3**.

Under the Build Alternative, due to depths of excavation and proposed new right-of-way acquisition, there is the potential to encounter hazardous materials related to some of these findings. TxDOT recommended conducting sub-surface investigation activities, contingent on further file research and review of potential impacts and excavation depths, as follows:

- Near the leaking petroleum storage tank findings adjacent to and within the project area where design indicates that excavations will occur below a depth of five feet.
- In the vicinity of the two dry cleaners identified in the ISA where design indicates that excavations will occur below five feet.
- In the channel and detention pond area to ascertain whether a Trichloroethylene (TCE) plume may be encroaching on the project right-of-way, and to assess potential vapor impacts related to this plume.
- In the channel and detention pond area to ascertain whether historical land use and operational activities may have impacted the surface and subsurface soil in the project right-of-way.

A detailed subsurface investigation plan to address potential contamination issues outlined above will be finalized after file research and review, and the type, locations, and depths of utility excavations are determined. The subsurface investigation will occur prior to construction.

Two monitoring wells and seven recovery wells that are part of a groundwater recovery system referred to as the "South Bank" system are located within the proposed drainage area north of Faye Avenue. The wells were installed in April 2000 as part of the TCEQ-approved remedy for TCE and Tetrachloroethene (PCE) groundwater contamination originating from the former Kelly Air Force Base. Some of these wells were inactivated in 2003 and the remaining wells were placed in stand-by mode in 2006. The recovery system can be re-activated to address cleanup requirements, if needed. The project will not impact this groundwater recovery system.

Since the project involves building demolition, asbestos inspections would be required.

Under the No-Build Alternative, there will be no potential to encounter hazardous materials related to construction or property acquisition, and no need to affect the existing groundwater recovery system.

5.14 Traffic Noise

Traffic Noise impacts were analyzed in detail and are documented in a Traffic Noise Technical Report (RKEI 2016g).

Existing and predicted traffic noise levels were modeled at receiver locations (**Appendix F-4**) that represent the land use activity areas adjacent to the proposed project that might be impacted by traffic noise and potentially benefit from feasible and reasonable noise abatement. The proposed project will not result in a traffic noise impact once completed, but could increase noise levels during construction activities. Provisions will be included in the plans and specifications that require the contractor to make every reasonable effort to minimize construction noise through abatement measures such as work-hour controls and proper maintenance of muffler systems.

Under the No-Build Alternative, there will be no potential to impact adjacent noise receivers from a project.

5.15 Induced Growth

TxDOT's *Induced Growth Indirect Impacts Decision Tree* (TxDOT 2014e) was followed to determine the need for an induced growth analysis. This analysis was not required based on the following:

- The proposed project is not intended to spur economic development in the Purpose and Need and is not intended to serve a specific development.
- Economic development or new opportunities for growth/development are not cited as benefits of the project.
- The proposed project does not add capacity, and will not substantially increase access or mobility in the project area.

5.16 Cumulative Impacts

CEQ regulations define cumulative impacts as:

"...an impact on the environment which results from the incremental impact of the action when added to other past, present and reasonably foreseeable future actions regardless of what agency (Federal or non-Federal) or person undertakes such other actions. Cumulative impacts can result from individually minor but collectively significant actions taking place over a period of time" (40 CFR §1508.7).

The regulatory requirement for addressing cumulative effects, distinction between types of effects (direct, indirect, and cumulative), and key principles are outlined in TxDOT's Cumulative Impacts Analysis Guidelines (TxDOT 2014b). Two key principles include:

- Since each resource has a limited capacity to sustain effects and can withstand only so many additional effects before it fails, analyzing cumulative effects addresses a resource's sustainability (TxDOT 2014b).
- It is important in cumulative effects analysis to consider "effect" as change in the trend of a resource as opposed to impact in static terms (TxDOT 2014b).

TxDOT's *Cumulative Impacts Decision Tree* (2014d) was followed to determine the need for a cumulative impacts analysis. The analysis was not required based on the following:

- The proposed project will not have substantial direct or indirect impacts on any resource.
- No resources in the project area are in poor or declining health.

5.17 Construction Phase Impacts

Temporary construction-related impacts may occur and could include dust and noise related to heavy machinery moving and earth-moving activities. BMPs will be implemented to mitigate temporary construction-related air quality and noise impacts (**Section 8.0**). The conceptual construction sequence will initially involve constructing the drainage outfall. The next step will include a two-phase roadway construction sequence beginning with the west side of Quintana Road, followed by the east side. One travel lane in each direction will be maintained throughout roadway construction. Public access to Quintana Park will be limited or restricted during construction for safety reasons.

Under the No-Build Alternative, there will be no impacts related to construction activities and Quintana Park will not be temporarily inaccessible.

6.0 AGENCY COORDINATION

Based on project scoping and the resource reviews conducted for this project, no resource agency coordination is required; however, see **Section 5.9** for discussion of coordination with the OWJ (property owner) related to Quintana Park. As discussed in **Section 5.5**, PSA will continue to coordinate with VIA regarding their bus routes.

7.0 PUBLIC INVOLVEMENT

TxDOT's *Environmental Handbook: Public Involvement* (TxDOT 2015c) was used as a guide during this assessment.

7.1 Meeting with Affected Property Owner

A meeting with the affected property owner (MAPO) subject to right-of-way acquisition was held on June 9, 2016, to inform him about the project and how his property would be impacted. The owner noted that he has lived at this location since the 1950's and is aware of many years of traffic accidents at the intersection. He suggested that the roadway should be shifted to the west side to flatten the curvature of the roadway to make it safer. The City responded that this option was explored, and that the project team deemed the current plan to be of the least impact to the community as a whole. The owner stated that the proposed design makes it more dangerous and expressed his opposition to the project and its impacts to his property.

7.2 Quintana Association Informational Meeting

A Quintana Neighborhood Association meeting was held on June 14, 2016. Approximately 16 residents attended, and another 10 to speak or present. Port San Antonio representative, Paco Felici, discussed the Quintana Road project, and representatives Juan Contreras and Veronica Barefield attended on behalf of CoSA. Mr. Felici announced the Quintana public meeting scheduled for June 30, 2016. One resident asked when construction may begin. CoSA staff responded that it will be constructed in late 2017². The President of the association asked whether the design was set or if there was an opportunity to change the design depending on the comments submitted. Port San Antonio responded that comments will be considered. City staff noted that the public will have the opportunity to ask additional questions and provide comments at the upcoming public meeting.

7.3 Public Meeting

The proposed plan was presented to the community and residents during an open house style public meeting on June 30, 2016, from 6:00 pm to 8:00 pm. The meeting was held at The Alamo Colleges Workforce Center of Excellence at 203 Norton Street, just northeast of the project area. A project location map and schematics were available for review at the reception area of the Workforce Center of Excellence from June 20, 2016 until the meeting date.

Approximately 50 people attended the public meeting, two of which were Public Officials (Edward Mungia of District 4 – Saldana and Richard D. Garcia of District 5 – Gonzales). The meeting was translated in Spanish by Mr. Paco Felici from Port San Antonio. Presenters included David McBeth (CoSA-TCI), Pete Rodriguez (CoSA-TCI), and Veronica Barefield (CoSA-TCI). The purpose of the meeting was to:

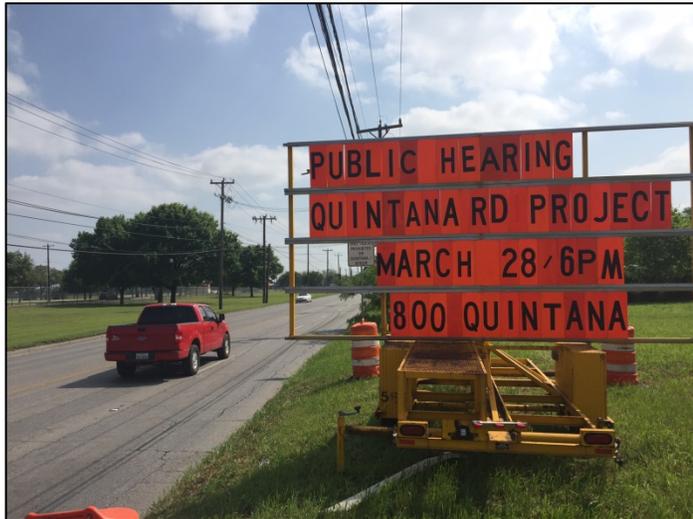
² This construction timeframe has been updated per **Section 2.2**, and the public will be updated during a future public hearing.

- Communicate the project’s vision and plans through an opportunity for the public to view various schematic alternatives that were developed for the proposed project
- Receive meaningful insight, comments, and public input (in writing or orally)
- Answer questions in face-to-face dialogue.

A total of 16 comments were received verbally during the meeting and two written comments were submitted. Some comments included concern about accessibility to Qui ntana Park and it was noted that the Port will look into offering more amenities; however, access will be limited during construction. A site visit was requested by the public to receive a direct visual of project improvements. In response, a site visit will be scheduled and interested people will be able to sign up for the opportunity. Concern of environmental issues was also brought to attention. It was explained that no construction will take place until the environmental process, including subsurface sampling in areas of concern, is completed within the project area. A problem with an increase in traffic speed down the ramp from Cupples onto Quintana Road was brought up as another concern. One recommendation noted was to add a traffic light at the bottom of Cupples Bridge for pedestrian and traffic safety. Other concerns brought up include drainage issues, how it will affect traffic, the objectives of the project, the use of McKenna Road, truck traffic, and house/yard flooding. Written comments included much of the same concerns brought up verbally in the meeting. Comments and responses are summarized in the Comment-Response Matrix in **Appendix H**.

7.4 Public Hearing

A public hearing was held on Tuesday, March 28, 2017, at the same venue as the prior public meeting. The hearing included display viewing at 6:00 p.m. followed by a formal presentation beginning at 7:00 p.m., which included a public comment period. The hearing was published in English and Spanish in the San Antonio Express-News and La Prensa. Public notices were sent to citizens and public officials with an interest in the project. Notices included direct mailings, e-mail, PSA website, and roadside message signs. Publication notification methods included the following:



- Newspaper advertisement in the San Antonio Express News (March 8, 15, and 21),
- Newspaper advertisement in La Prensa (March 8, 15, and 22),
- Mailed notices to 396 property owners, including all adjacent properties
- Mailed and/or e-mailed notices to public officials/groups with possible interest in the project, including:
 - City Council Districts 4 and 5
 - Keep South San Proud Neighborhood Association
 - Quintana Neighborhood Association
 - South San Antonio ISD
 - South San Board of Trustees

- St. Philip's College and Alamo Colleges' Workforce Center of Excellence
- PSA also posted a web page with public hearing information and relevant documents.

A bilingual English/Spanish interpreter was on-site to provide live Spanish translation during the hearing. A court reporter was on-hand to record the hearing minutes. Multiple informational and graphic boards were displayed at the venue's entrance, as well as within the hearing room. In addition to project staff (consultants, TxDOT staff, and CoSA-TCI staff), approximately 46 people, including citizens and representatives of City Council District 4 and 5 offices, attended the hearing. Councilwoman, Shirley Gonzalez, representing City Council District 5 attended.



During the course of the hearing and subsequent comment period, a total of eight individuals provided comments, either verbal or written. Councilwoman Gonzalez expressed concern that citizens might not have an opportunity to receive responses to their comments. Other questions centered on the possibility of extending the project beyond the project limits, concerns with future traffic and access, possible bottleneck effect, moving drainage issues downstream, property acquisition, closing of Lester Avenue, and opportunities to enhance the park and provide additional amenities. A complete list of all comments received during the hearing and subsequent comment period, and official responses, are documented in Documentation of Public Hearing, which is on file at TxDOT Environmental Affairs offices.

8.0 IMPACTS, AVOIDANCE, MINIMIZATION, AND MITIGATION

Several measures designed to protect and/or enhance the environment will be implemented for this project. These measures are summarized in the table below and will be included in the Environmental Issues, Permits, and Commitments (EPIC) sheet for this project.

Table 2. Summary of Impacts, Avoidance, Minimization, and Mitigation

| Topic | Impact | Avoidance, minimization, and mitigation measures |
|--------------------------|---|---|
| Air Quality | Temporary increases in PM and MSAT from construction activities | Fugitive dust control measures contained in standard specifications will be applied, as appropriate. TxDOT encouragement of TERP financial incentives and other local and federal incentive programs to minimize diesel emissions. |
| Archaeological Resources | Ground disturbing activities | In the event that unanticipated archeological deposits are encountered during construction, work in the immediate area will cease, and COSA archeological staff will be contacted to initiate post-review discovery procedures. |
| Biological Resources | Potential for nesting birds to be present in the project area. | The following BMPs will be incorporated to protect migratory bird nests. These will include: <ul style="list-style-type: none"> • Not disturbing, destroying, or removing active nests, including ground nesting birds, during the nesting season; • Avoiding the removal of unoccupied, inactive nests, as practicable; • Preventing the establishment of active nests during the nesting season on TxDOT, COSA, and PSA owned and operated facilities and structures proposed for replacement or repair; • Not collecting, capturing, relocating, or transporting birds, eggs, young, or active nests without a permit. |

Table 2 (Continued). Summary of Impacts, Avoidance, Minimization, and Mitigation

| | | |
|------------------------------|---|--|
| Hazardous Materials | The project is in close proximity to historic dry cleaning facilities, LPSTs, and TCE plumes, which are associated with the Union Pacific Rail Yard to the west and the former Kelly Air Force Base to the north and northeast. | <p>If any hazardous materials, contaminated media and/or petroleum contamination (i.e., soil, groundwater, surface water, sediment, building materials, etc.) are encountered during construction, work will cease in the immediate area and the Engineer should be contacted immediately.</p> <p>Under the Build Alternative, due to depths of excavation and proposed new right-of-way acquisition, there is the potential to encounter hazardous materials related to some of these findings. TxDOT recommended conducting subsurface investigation activities contingent on further review of potential impacts and excavation depths as follows:</p> <ul style="list-style-type: none"> • Near the leaking petroleum storage tank findings adjacent to and within the project area where design indicates that excavations will occur below a depth of five feet. • In the vicinity of the two dry cleaners identified in the ISA where design indicates that excavations will occur below five feet. • In the channel and detention pond area to ascertain whether a TCE plume may be encroaching on the project right-of-way and to assess potential vapor impacts related to this plume. • In the channel and detention pond area to ascertain whether historical land use and operational activities may have impacted the surface and subsurface soil in the project right-of-way. <p>A detailed subsurface investigation plan to address potential contamination issues outlined above will be finalized after file research and review, and the type, locations, and depths of utility excavations are determined. The subsurface investigation will occur prior to construction.</p> <p>Since the project involves building demolition, asbestos inspections would be required.</p> |
| Traffic Noise | Temporary construction noise associated with heavy machinery will be generated. | Provisions will be included in the plans and specifications that require the contractor to make every reasonable effort to minimize construction noise through abatement measures such as work-hour controls and proper maintenance of muffler systems. |
| Utilities/Emergency Services | Several underground and overhead utilities, including water and sewer lines, gas lines, and overhead electricity lines will be relocated. There is a potential to impact transmission lines in the proposed detention pond. | Coordination with SAWS and CPS Energy (gas and electric), is currently underway and will be completed prior to construction beginning. Utility relocations and adjustments will be accomplished with the minimum practicable disruption in service to customers. |
| Vegetation | Vegetation will be disturbed during construction. | Disturbed areas will be re-vegetated according to TxDOT's Specification, 164-006 Seeding Native Grasses for Erosion Control. |

| | | |
|---------------|--|--|
| Water Quality | The project will disturb more than five acres of land. | Since the project will disturb approximately 20 acres of land, a TPDES permit will be required, along with a NOI to the TCEQ. The plans and specifications will include a SW3P. BMPs will be incorporated in the construction plans to minimize potential sedimentation effects in the stormwater system. These measures will be in place before construction begins and will be inspected on a regular basis. |
|---------------|--|--|

9.0 CONCLUSION

An EA is prepared when the significance of the environmental impact of a proposed project is not clearly established. To determine significance, the severity of the impact was examined in terms of the type, quality, and sensitivity of the resources involved; the location of the proposed project; the duration of the effect (short-term/long-term); and other “context” considerations. If a significant impact is determined for the proposed project, the preparation of an EIS would be recommended.

This EA and supporting technical reports analyzed the potential direct impacts that may result to the environmental, social, and cultural environments surrounding and within the proposed Quintana Road project. Indirect and cumulative effects were not analyzed in detail, as the need for these analyses was eliminated by following TxDOT’s decision trees for these types of effects.

This EA indicates that the proposed project will not result in a significant impact on the human or natural environment; therefore, a FONSI is recommended.

10.0 REFERENCES

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- Raba-Kistner Environmental, Inc. 2016g. *Traffic Noise Technical Memo: On Quintana Rd. from Harmon Ave. to McKenna Ave. CSJ: 0915-12-520*. November 30, 2016.

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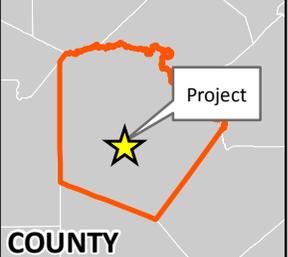
APPENDICES

Appendix A
Project Location

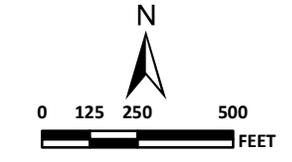


LEGEND

- Proposed Intersection Improvements
- Proposed Detention Pond and Channel



PROJECT LOCATION
 ON QUINTANA RD. FROM HARMON AVE.
 TO MCKENNA AVE.
 San Antonio, Bexar County, Texas
 CSJ: 0915-12-520



Appendix A

SOURCE: Basemap from Environmental Systems Research Institute (ESRI)

Appendix B
Project Photographs



Photo 1. On Quintana Road facing north towards Dunton Avenue intersection.



Photo 2. Restaurant adjacent south of Quintana Road and Dunton Ave. to be displaced; facing south.



Photo 3. View of muffler shop (former fuel station) 1110 Quintana Road to be displaced; facing south.



Photo 4. View of jogging track at Quintana Park; facing south.



Photo 5. "Commander's House" located within proposed drainage area southeast of Tampa Ave; facing south.



Photo 6. View of proposed drainage area along Fay Ave; facing east.



Photo 8: A horizontal recovery well on the southern side of the proposed drainage area; facing north.

Appendix C
Project Schematics

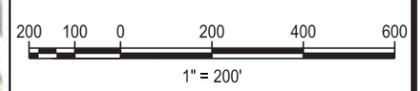
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PLAN VIEW LEGEND

- EXISTING LOT LINES
- - - EXIST ROW
- - - PROP ROW
- ▭ PROP DETENTION POND & CHANNEL
- ▭ PROP ROADWAY

NOTE:
 THIS EXHIBIT DEPICTS PROPOSED ROADWAY AND DRAINAGE FEATURES THAT WILL BE FURTHER DEVELOPED DURING THE DESIGN PHASE. LIMITS, LOCATION, AND SIZING WILL BE CONFIRMED DURING DESIGN.



INTERIM REVIEW
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 ON 2/11/2016

ON QUINTANA ROAD PROJECT
 HARMON AVE TO MCKENNA AVE

CEC 11550 IH 10 WEST, SUITE 395
 SAN ANTONIO, TEXAS 78230-1037
 TEL: (210) 641-9999 | FAX: (210) 641-6440
 TBPE REGISTRATION NO.: F-2214
 TBPLS REGISTRATION NO.: 100410-00
 CEC PROJECT NUMBER: E0517900

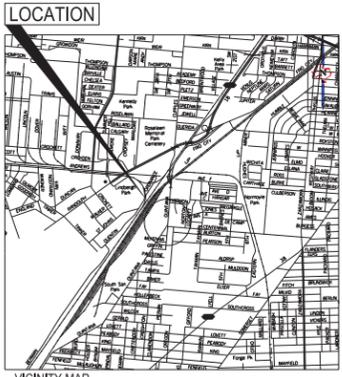
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| TEXAS | SAT | BEXAR |
| CONT. | SECT. | JOB |
| 915 | 12 | 520 |
| HIGHWAY / STREET | | |
| QUINTANA RD | | |



SCHEMATIC LAYOUT ON QUINTANA ROAD

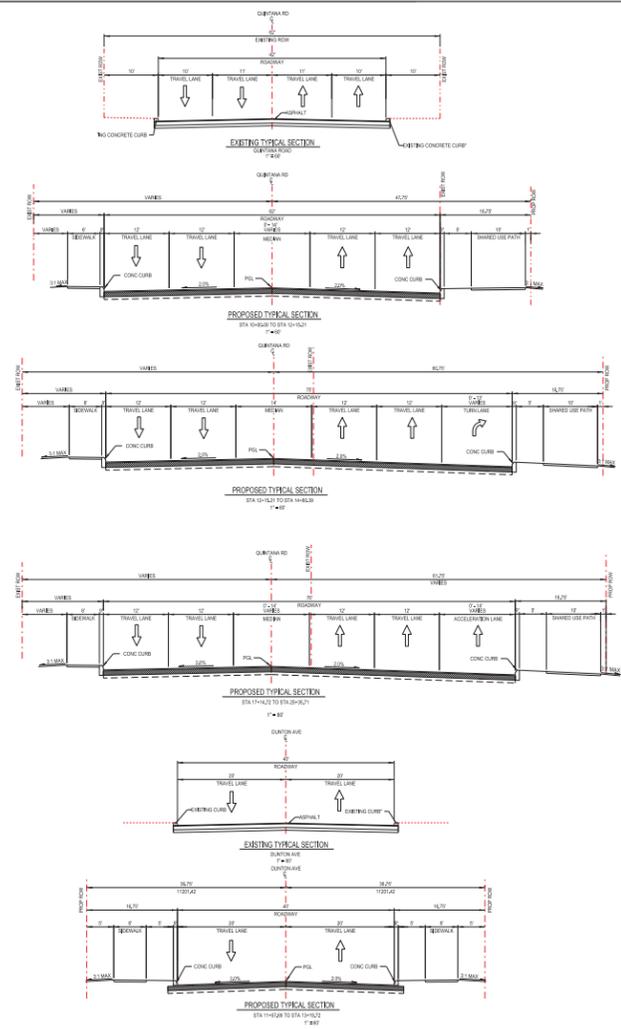
LIMITS FROM: HARMON AVE.
TO: MCKENNA AVE.
CSJ: 0915-12-520
LENGTH: 0.5 MI
DESIGN SPEED: 45 MPH

FUNCTIONAL CLASSIFICATION: Arterial
PROPOSED DESIGN STANDARDS (STRUCTURES): TXDOT Hydraulic Manual
PROPOSED DESIGN STANDARDS (ROADWAY): TXDOT Roadway Design Manual, Chapter 6, Section 3
PROPOSED DESIGN STANDARDS (TRAFFIC): TMUTCD & Applicable Standards



PRELIMINARY

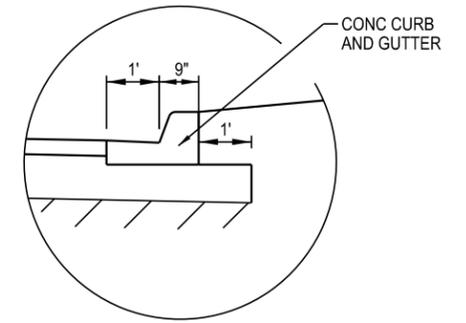
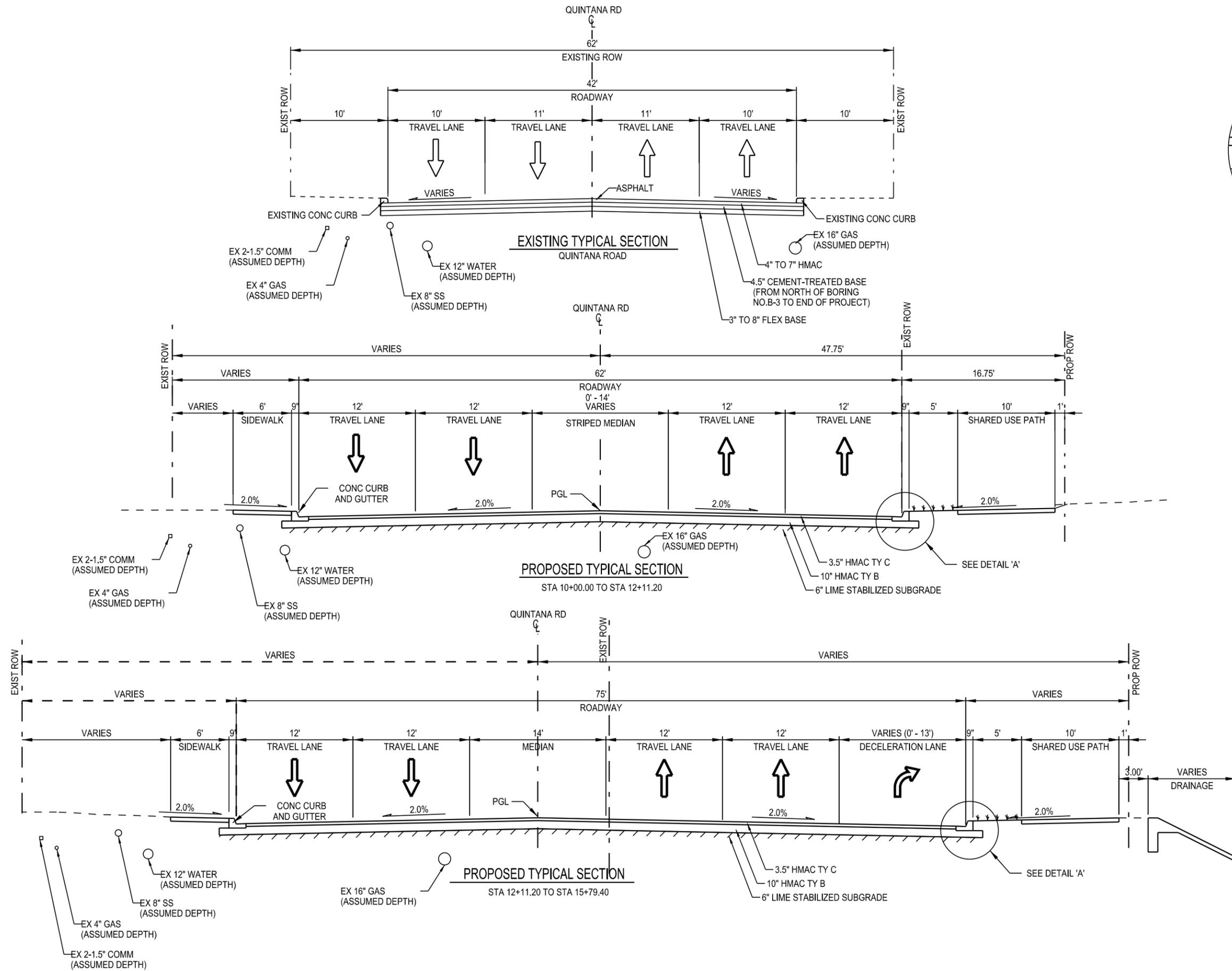
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Appendix D

Typical Sections

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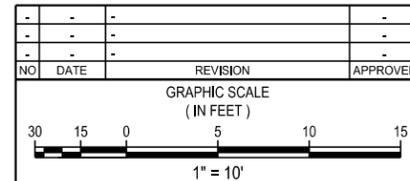


DETAIL 'A'

NOTES:

REFER TO GEOTECHNICAL ENGINEERING STUDY (ARIAS JOB NO. 2012-747) FOR DETAILS ON EXISTING PAVEMENT STRUCTURE. CEMENT TREATED BASE OR POSSIBLY CONCRETE PAVEMENT IS PRESENT NORTH OF BORING NO. B-3. CONTRACTOR MUST VERIFY LIMIT OF CEMENT TREATED BASE IN FIELD PRIOR TO BEGINNING EXCAVATION.

NEW UTILITY POLES TO BE PLACED BETWEEN THE SHARED USE PATH AND THE CURB AND GUTTER.



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 SAN ANTONIO, TEXAS 78230-1037
 TEL: (210) 641-8999 FAX: (210) 641-6440
 CIVIL ENGINEERING CONSULTANTS DON DURDEN, INC. TBPE REGISTRATION NO.: F-2214
 TBPLS REGISTRATION NO.: 100410-00

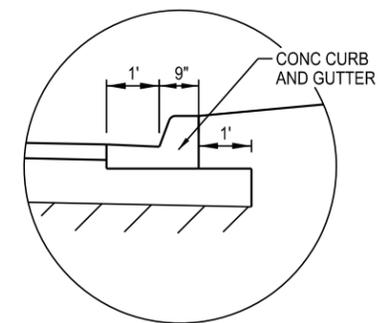
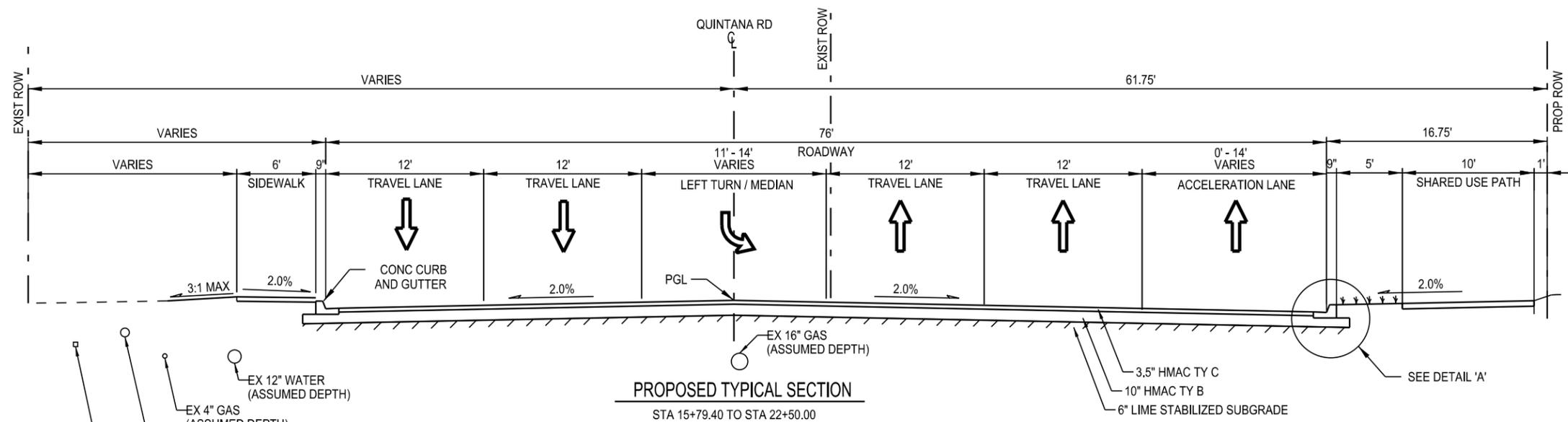
QUINTANA ROAD REALIGNMENT

QUINTANA ROAD TYPICAL SECTIONS

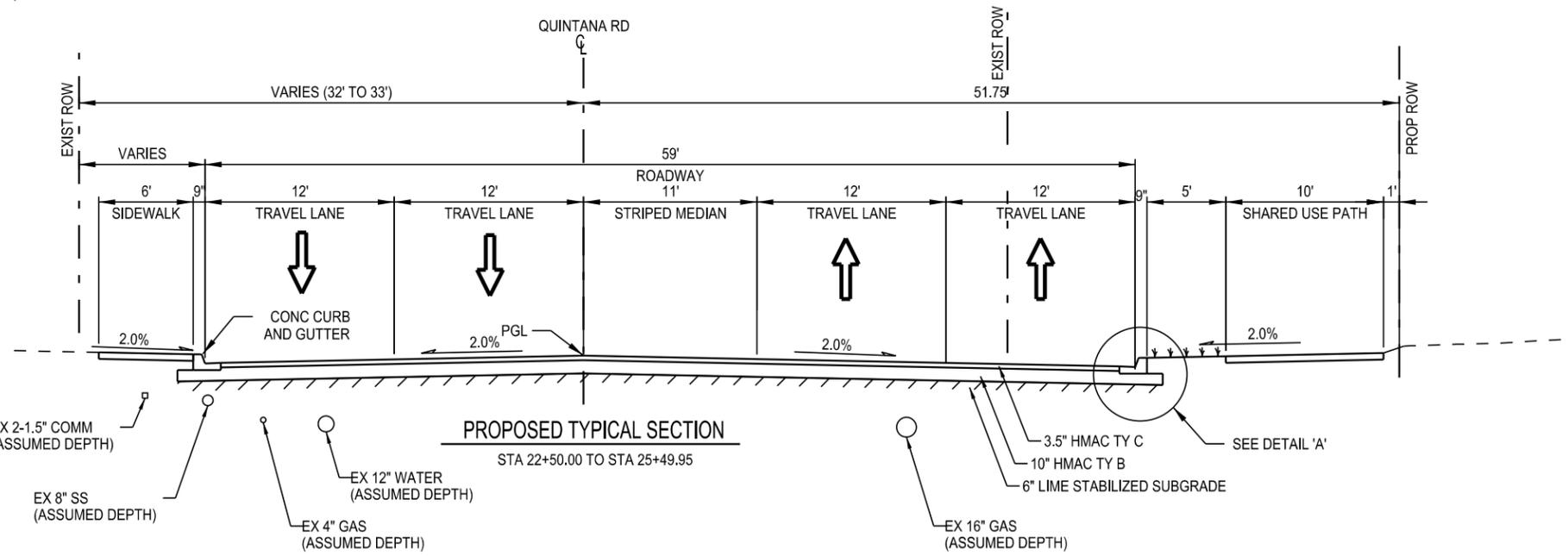
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PORT SAN ANTONIO

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| CONT. | SECT. | JOB | HIGHWAY / STREET |
| 915 | 12 | 520 | QUINTANA RD |



- EX 12" WATER (ASSUMED DEPTH)
- EX 4" GAS (ASSUMED DEPTH)
- EX 8" SS (ASSUMED DEPTH)
- EX 2-1.5" COMM (ASSUMED DEPTH)



- EX 2-1.5" COMM (ASSUMED DEPTH)
- EX 8" SS (ASSUMED DEPTH)
- EX 12" WATER (ASSUMED DEPTH)
- EX 4" GAS (ASSUMED DEPTH)
- EX 16" GAS (ASSUMED DEPTH)

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GRAPHIC SCALE (IN FEET)

1" = 10'

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 CIVIL ENGINEERING CONSULTANTS DON DURDEN, INC. TBPE REGISTRATION NO.: F-2214
 TBPLS REGISTRATION NO.: 100410-00

QUINTANA ROAD REALIGNMENT

QUINTANA ROAD TYPICAL SECTIONS

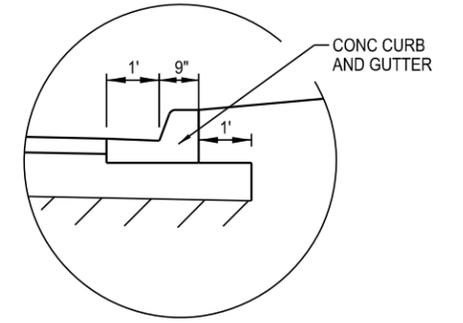
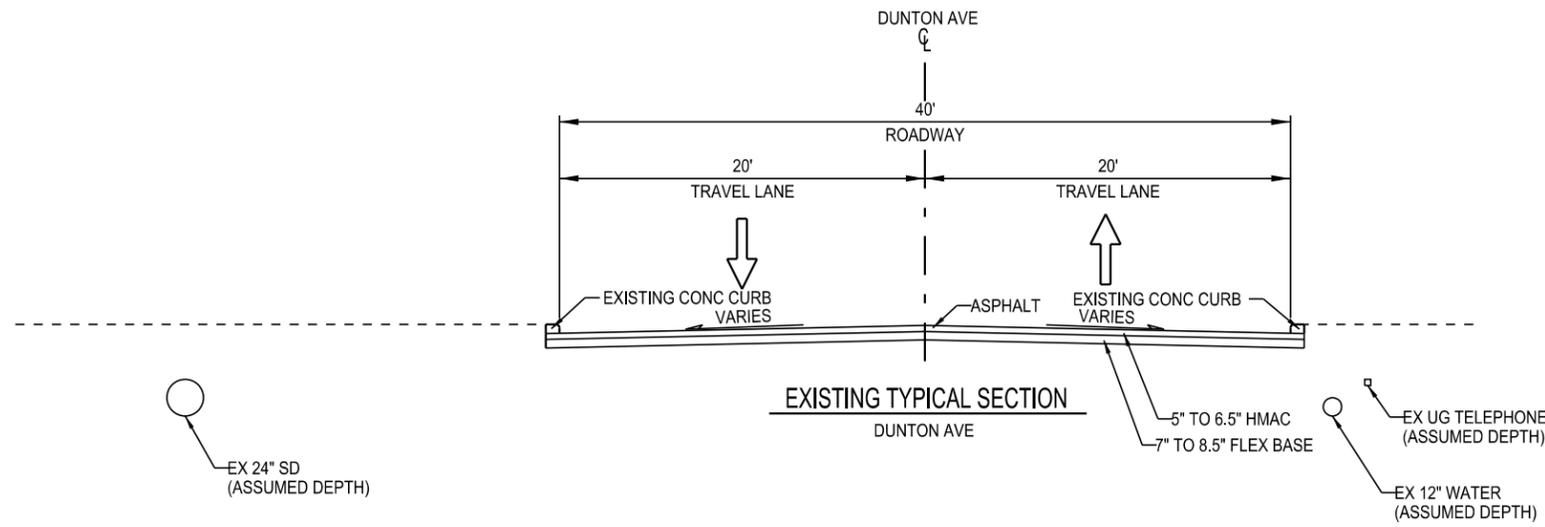
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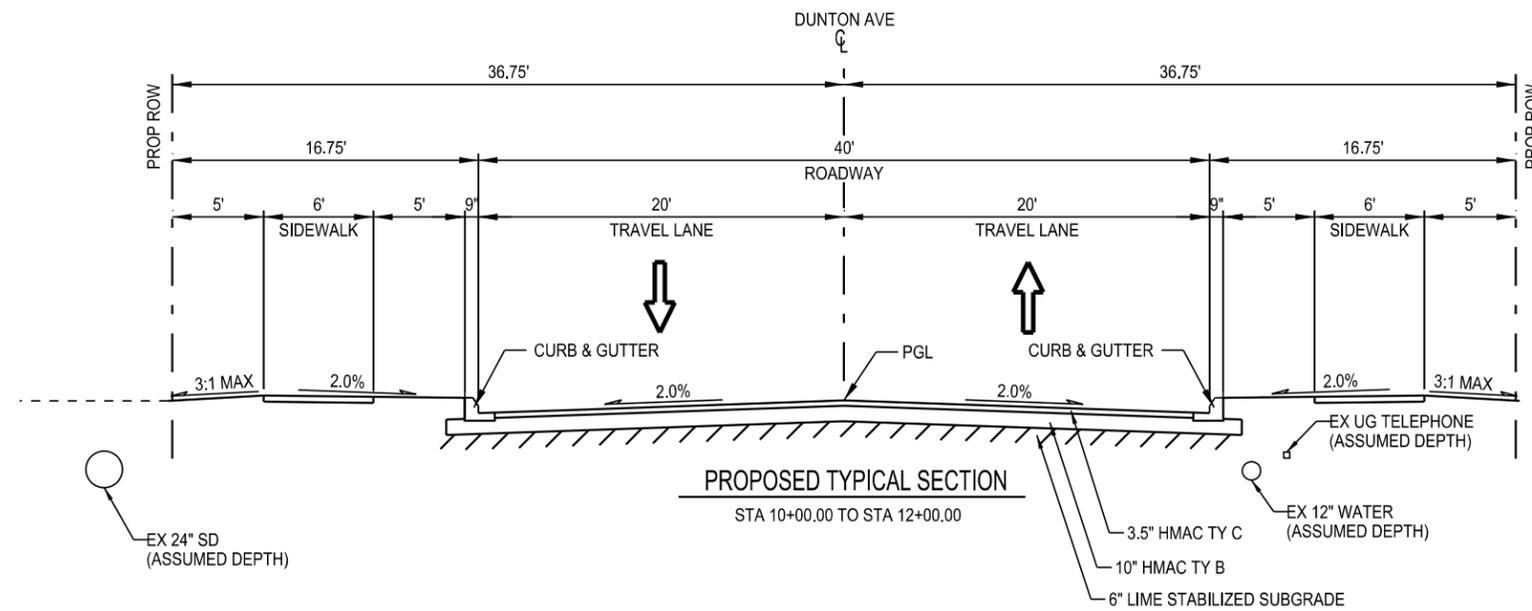
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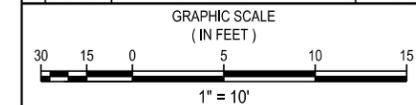


DETAIL 'A'

NOTES:
 DUNTON AVE IS A PRIVATE ROAD AND DOES NOT HAVE EXISTING RIGHT OF WAY.



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 CIVIL ENGINEERING CONSULTANTS DON DURDEN, INC. TBPE REGISTRATION NO.: F-2214
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QUINTANA ROAD REALIGNMENT

DUNTON DR. TYPICAL SECTIONS

SHEET 3 OF 3
 PORT SAN ANTONIO

| FED. RD. DIV. NO. | FEDERAL AID PROJECT NO. | PLAN SH. NO. | |
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| CONT. | SECT. | JOB | HIGHWAY / STREET |
| 915 | 12 | 520 | QUINTANA RD |

ADDITIONAL NOTES

ALL EXISTING MANHOLE COVERS, VALVE COVERS, ETC. ARE TO BE ADJUSTED TO FINISH GRADE ELEVATION BY CONTRACTOR.

CONTRACTOR SHALL BRACE ANY CPS ELECTRIC POLE IF THEY ARE WITHIN 5 FT OF ANY TRENCH EXCAVATION OPERATION.

PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL OBTAIN ALL REQUIRED STORM WATER PERMITS, FEES, AND APPROVALS. NO CONSTRUCTION OR FABRICATION SHALL BEGIN UNTIL THE CONTRACTOR HAS RECEIVED AND THOROUGHLY REVIEWED ALL PERMITS REQUIRED FOR CONSTRUCTION IN DRAINAGE EASEMENTS, RIGHT OF WAYS, AND FLOODPLAINS.

THE CONTRACTOR SHALL NOTIFY STORM WATER ENGINEERING AT LEAST 24 HOURS PRIOR TO THE INSTALLATION OF ANY DRAINAGE FACILITY WITHIN A DRAINAGE EASEMENT OR STREET RIGHT OF WAY NOT INDICATED ON THE CONSTRUCTION PLANS.

THE CONTRACTOR IS RESPONSIBLE FOR PROTECTING EXISTING DRAINAGE FACILITIES FROM DAMAGE. ANY DAMAGE TO EXISTING DRAINAGE SYSTEMS, WHETHER OR NOT SHOWN ON THE PLANS, SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO REPAIR AT HIS EXPENSE. THE CONTRACTOR SHALL NOTIFY STORM WATER ENGINEERING AT 210-207-8052 AS SOON AS CONFLICTS WITH UTILITIES ARE ENCOUNTERED OR ANY DRAINAGE SYSTEM IS DAMAGED DURING CONSTRUCTION.

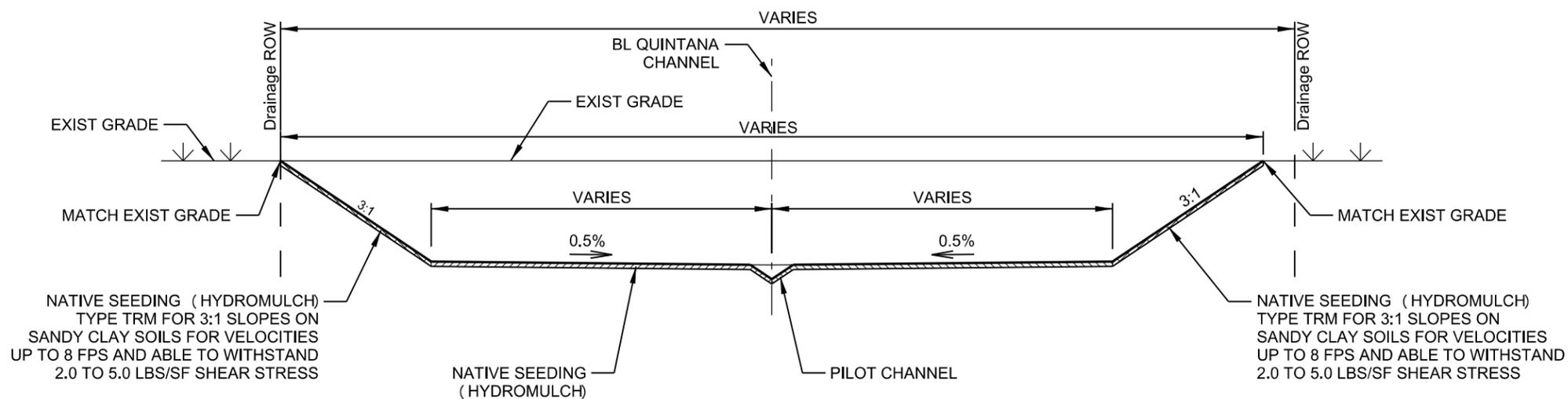
CONSTRUCTION SPOILS WILL NOT BE ALLOWED TO BE DEPOSITED ANYWHERE WITHIN THE RIGHT OF WAY OR FLOODPLAIN WITHIN THE LIMITS OF THE PROJECT AND SHALL BE DISPOSED OFFSITE IN COMPLIANCE WITH CURRENT APPLICABLE REGULATIONS.

NO STRUCTURE, FENCES, WALLS, LANDSCAPING, OR OTHER OBSTRUCTIONS THAT IMPEDE DRAINAGE SHALL BE PLACED WITHIN THE LIMITS OF THE DRAINAGE EASEMENTS SHOWN ON THE CONSTRUCTION DOCUMENTS.

UPON COMPLETION OF TRENCHING, THE AREA WILL BE BACKFILLED AND COMPACTED TO ITS ORIGINAL CONDITION. TRENCHES/BORE PITS TO BE OPEN AND UNATTENDED LONGER THAN 24 HOURS SHALL BE PROTECTED TO WITHSTAND ALL HYDRODYNAMIC AND HYDROSTATIC FORCES AND PREVENT DOWNSTREAM IMPACTS. TRENCHES/BORE PITS TO BE OPEN LONGER THAN 30 DAYS AFTER STARTING EXCAVATION SHALL BE BACKFILLED WITH A SEMI-PERMANENT REPAIR BACKFILL.

IMPROVED SECTIONS OF EARTHEN CHANNELS AND/OR WATERWAYS WILL BE VEGETATED BY SEEDING OR SODDING. EIGHTY FIVE PERCENT OF THE CHANNEL SURFACE AREA MUST HAVE ESTABLISHED VEGETATION BEFORE THE CITY OF SAN ANTONIO WILL ACCEPT THE CHANNEL FOR MAINTENANCE.

CONTRACTOR SHALL NOT PLACE WEEP HOLES IN PRECAST BOX CULVERTS



**QUINTANA RD CHANNEL
PROPOSED TYPICAL SECTION**
NOT TO SCALE

| | | | |
|----|------|----------|----------|
| NO | DATE | REVISION | APPROVED |
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| - | - | - | - |

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ON 6/29/2016

QUINTANA ROAD REALIGNMENT

**DRAINAGE
GENERAL NOTES &
TYPICAL DRAINAGE CHANNEL
SECTION**

SHEET 1 OF 1

MAESTAS & ASSOCIATES INC.
TBPE REGISTRATION No.: F-333
11550 I.H. 10 WEST SUITE 350
SAN ANTONIO, TEXAS 78230
(210) 366-1988

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| CONT. | SECT. | JOB | HIGHWAY / STREET |
| 915 | 12 | 520 | QUINTANA RD |

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Appendix E
Relevant MTP and TIP Pages

METROPOLITAN TRANSPORTATION PLAN "Mobility 2040"

Updated:
June 25, 2015

ALAMO AREA METROPOLITAN PLANNING ORGANIZATION

FY 2016

| TxDOT District | County | CSJ | Hwy | Phase | City | Project Sponsor | MPO Proj ID No. | Year of Expenditure Cost | |
|------------------|--------|--|-----|-------|-------------|-----------------|---------------------|--------------------------|--------|
| 15 - San Antonio | Bexar | 0915-12-513 | VA | C | San Antonio | AACOG | 4001.0 | \$267,000 | |
| Limits From: | | Alamo Area MPO: Bexar, Comal | | | | | Last Revision Date: | | 5/2014 |
| Limits To: | | Guadalupe & partial Kendall County | | | | | | | |
| Description: | | Alamo Area Commute Solutions Program (FY 2016) | | | | | | | |
| Project History: | | 4/12 - funded through STP-MM project selection process | | | | | | | |

| Total Project Cost Information (TxDOT %): | | | Type of Work: | Authorized Funding by Category/Share | | | | |
|---|-----------|-----------|--------------------|--------------------------------------|-------|----------|---------------|-----------|
| Cost of Approved Phases: | | | Funding Categories | Federal | State | Local | Local Contrib | Total |
| Preliminary Engineering | \$0 | \$267,000 | 7 - STP-MM | \$213,600 | \$0 | \$53,400 | \$0 | \$267,000 |
| ROW Purchase: | \$0 | | Other | \$0 | \$0 | \$0 | \$0 | \$0 |
| Construction Cost: | \$267,000 | | Other | \$0 | \$0 | \$0 | \$0 | \$0 |
| Construction Engineering | \$0 | | Other | \$0 | \$0 | \$0 | \$0 | \$0 |
| Contingencies: | \$0 | | Totals | \$213,600 | \$0 | \$53,400 | \$0 | \$267,000 |
| Indirect Costs: | \$0 | | | | | | | |
| Other Field | \$0 | | | | | | | |
| Total Project Cost: | \$267,000 | | | | | | | |

| TxDOT District | County | CSJ | Hwy | Phase | City | Project Sponsor | MPO Proj ID No. | Year of Expenditure Cost | |
|------------------|--------|---|-----|-------|-------|-----------------|---------------------|--------------------------|--------|
| 15 - San Antonio | Bexar | 0915-12-518 | CS | C | Kirby | Kirby | 4000.0 | \$509,196 | |
| Limits From: | | In Kirby in Friendship Park | | | | | Last Revision Date: | | 5/2015 |
| Limits To: | | From Old Seguin Road to Binz Engleman | | | | | | | |
| Description: | | Construction of a hike & bike trail through Friendship Park | | | | | | | |
| Project History: | | 4/15 - move from FY 2015 to FY 2016 and update limits; 4/12 - funded through STP-MM project selection process | | | | | | | |

| Total Project Cost Information (TxDOT %): | | | Type of Work: | Authorized Funding by Category/Share | | | | |
|---|-----------|-----------|--------------------|--------------------------------------|-------|-----------|---------------|-----------|
| Cost of Approved Phases: | | | Funding Categories | Federal | State | Local | Local Contrib | Total |
| Preliminary Engineering | \$24,951 | \$509,196 | 7 - STP-MM | \$407,357 | \$0 | \$101,839 | \$0 | \$509,196 |
| ROW Purchase: | \$0 | | Other | \$0 | \$0 | \$0 | \$0 | \$0 |
| Construction Cost: | \$509,196 | | Other | \$0 | \$0 | \$0 | \$0 | \$0 |
| Construction Engineering | \$32,487 | | Other | \$0 | \$0 | \$0 | \$0 | \$0 |
| Contingencies: | \$36,000 | | Totals | \$407,357 | \$0 | \$101,839 | \$0 | \$509,196 |
| Indirect Costs: | \$34,880 | | | | | | | |
| Other Field | \$0 | | | | | | | |
| Total Project Cost: | \$637,513 | | | | | | | |

| TxDOT District | County | CSJ | Hwy | Phase | City | Project Sponsor | MPO Proj ID No. | Year of Expenditure Cost | |
|------------------|--------|---|-----|-------|-------------|-----------------|---------------------|--------------------------|--------|
| 15 - San Antonio | Bexar | 0915-12-520 | CS | E,R,C | San Antonio | Port SA | 4005.0 | \$7,800,000 | |
| Limits From: | | On Quintana Road from Harmon Avenue | | | | | Last Revision Date: | | 2/2014 |
| Limits To: | | McKenna Avenue | | | | | | | |
| Description: | | Rehab and realign roadway, intersection and flood control improvements | | | | | | | |
| Project History: | | 1/14 - included PE and ROW as funded phases; 4/12 - funded through STP-MM project selection process | | | | | | | |

| Total Project Cost Information (TxDOT %): | | | Type of Work: | Authorized Funding by Category/Share | | | | |
|---|-------------|-------------|--------------------|--------------------------------------|-------|-------------|---------------|-------------|
| Cost of Approved Phases: | | | Funding Categories | Federal | State | Local | Local Contrib | Total |
| Preliminary Engineering | \$353,200 | \$5,846,800 | 7 - STP-MM | \$6,240,000 | \$0 | \$1,560,000 | \$0 | \$7,800,000 |
| ROW Purchase: | \$1,600,000 | | Other | \$0 | \$0 | \$0 | \$0 | \$0 |
| Construction Cost: | \$5,846,800 | | Other | \$0 | \$0 | \$0 | \$0 | \$0 |
| Construction Engineering | \$286,500 | | Other | \$0 | \$0 | \$0 | \$0 | \$0 |
| Contingencies: | \$267,200 | | Totals | \$6,240,000 | \$0 | \$1,560,000 | \$0 | \$7,800,000 |
| Indirect Costs: | \$282,400 | | | | | | | |
| Other Field | \$0 | | | | | | | |
| Total Project Cost: | \$8,636,100 | | | | | | | |

STATEWIDE TRANSPORTATION IMPROVEMENT PROGRAM
TIP FY 2015-2018
ALAMO AREA METROPOLITAN PLANNING ORGANIZATION
FY 2016

Updated:
 April 28, 2014

| TxDOT District | County | CSJ | Hwy | Phase | City | Project Sponsor | MPO Proj ID No. | Year of Expenditure Cost |
|------------------|--------|--|-----|-------|-------------|-----------------|----------------------------|--------------------------|
| 15 - San Antonio | Bexar | 0915-12-513 | VA | C | San Antonio | AACOG | 4001.0 | \$267,000 |
| Limits From: | | Alamo Area MPO: Bexar, Comal | | | | | Last Revision Date: 5/2014 | |
| Limits To: | | Guadalupe & partial Kendall County | | | | | | |
| Description: | | Alamo Area Commute Solutions Program (FY 2016) | | | | | | |
| Project History: | | 4/12 - funded through STP-MM project selection process | | | | | | |

| Total Project Cost Information (TxDOT %): | | | Type of Work: Rideshare | | | | | | |
|---|-----------|---|---------------------------|--------------------------------------|--------------|--------------|----------------------|--------------|-----------|
| Preliminary Engineering | \$0 | Cost of Approved Phases: \$267,000 | <u>Funding Categories</u> | Authorized Funding by Category/Share | | | | | |
| ROW Purchase: | \$0 | | | <u>Federal</u> | <u>State</u> | <u>Local</u> | <u>Local Contrib</u> | <u>Total</u> | |
| Construction Cost: | \$267,000 | | | 7 - STP-MM | \$213,600 | \$0 | \$53,400 | \$0 | \$267,000 |
| Construction Engineering | \$0 | | | Other | \$0 | \$0 | \$0 | \$0 | \$0 |
| Contingencies: | \$0 | | | Other | \$0 | \$0 | \$0 | \$0 | \$0 |
| Indirect Costs: | \$0 | | | Other | \$0 | \$0 | \$0 | \$0 | \$0 |
| Other Field | \$0 | | | Totals | \$213,600 | \$0 | \$53,400 | \$0 | \$267,000 |
| Total Project Cost: | \$267,000 | | | | | | | | |

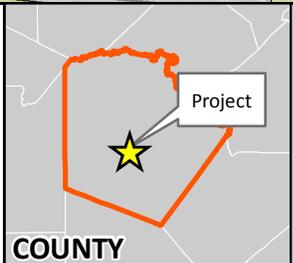
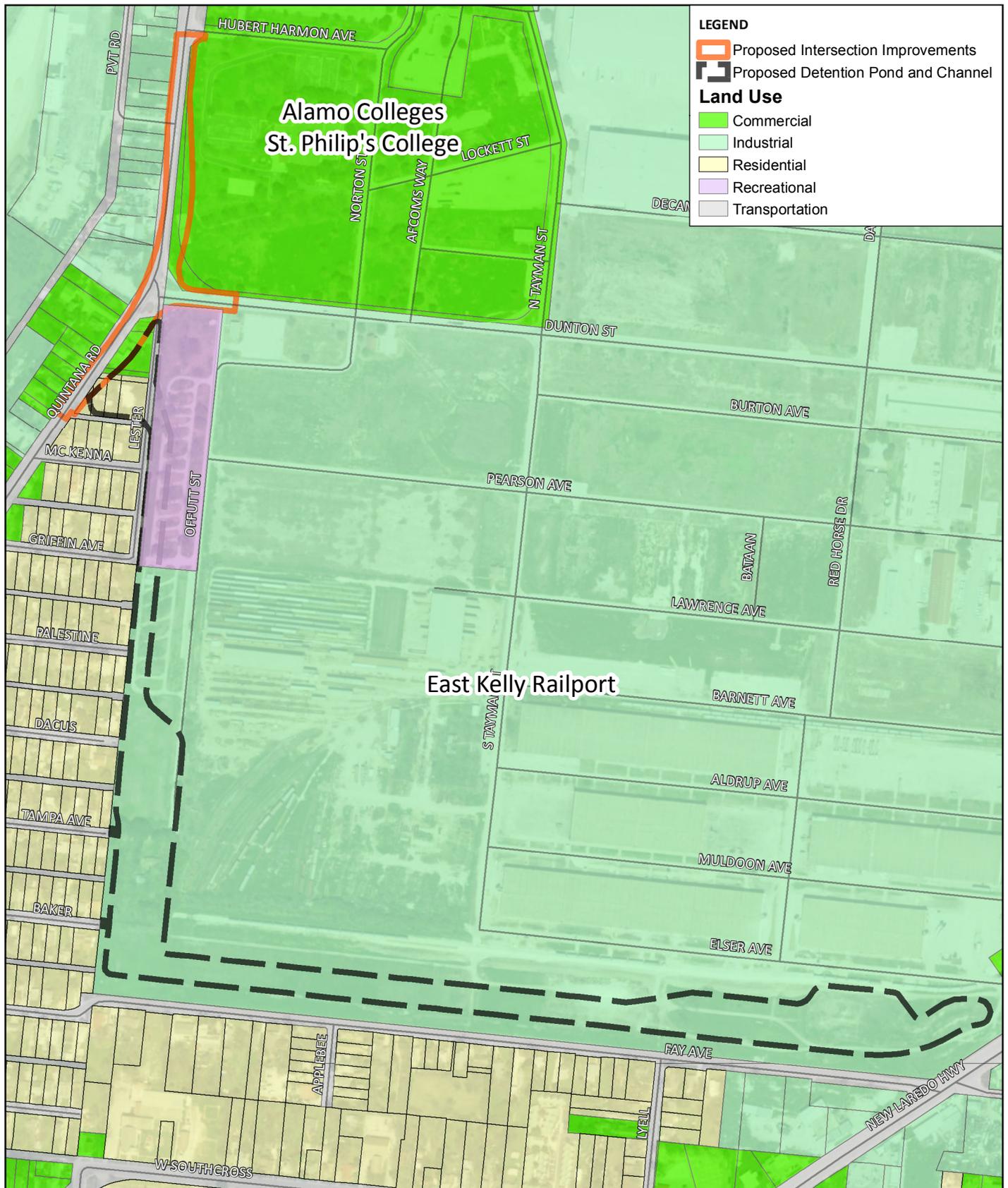
| | | | | | | | | |
|------------------|-------|---|----|-------|-------------|---------|----------------------------|-------------|
| 15 - San Antonio | Bexar | 0915-12-520 | CS | E,R,C | San Antonio | Port SA | 4005.0 | \$7,800,000 |
| Limits From: | | On Quintana Road from Harmon Avenue | | | | | Last Revision Date: 2/2014 | |
| Limits To: | | McKenna Avenue | | | | | | |
| Description: | | Rehab and realign roadway, intersection and flood control improvements | | | | | | |
| Project History: | | 1/14 - included PE and ROW as funded phases; 4/12 - funded through STP-MM project selection process | | | | | | |

| Total Project Cost Information (TxDOT %): | | | Type of Work: Maint/Rehab | | | | | | |
|---|-------------|---|---------------------------|--------------------------------------|--------------|--------------|----------------------|--------------|-------------|
| Preliminary Engineering | \$353,200 | Cost of Approved Phases: \$7,800,000 | <u>Funding Categories</u> | Authorized Funding by Category/Share | | | | | |
| ROW Purchase: | \$1,600,000 | | | <u>Federal</u> | <u>State</u> | <u>Local</u> | <u>Local Contrib</u> | <u>Total</u> | |
| Construction Cost: | \$5,846,800 | | | 7 - STP-MM | \$6,240,000 | \$0 | \$1,560,000 | \$0 | \$7,800,000 |
| Construction Engineering | \$286,500 | | | Other | \$0 | \$0 | \$0 | \$0 | \$0 |
| Contingencies: | \$267,200 | | | Other | \$0 | \$0 | \$0 | \$0 | \$0 |
| Indirect Costs: | \$282,400 | | | Other | \$0 | \$0 | \$0 | \$0 | \$0 |
| Other Field | \$0 | | | Totals | \$6,240,000 | \$0 | \$1,560,000 | \$0 | \$7,800,000 |
| Total Project Cost: | \$8,636,100 | | | | | | | | |

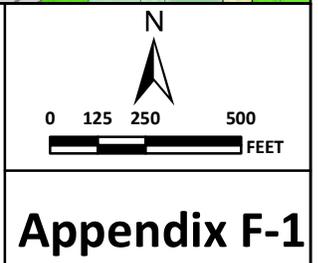
| | | | | | | | | |
|------------------|-------|--|----|---|---------|---------|----------------------------|-------------|
| 15 - San Antonio | Bexar | 0915-12-529 | CS | C | Helotes | Helotes | 4003.0 | \$1,336,212 |
| Limits From: | | FM 1560 | | | | | Last Revision Date: 7/2012 | |
| Limits To: | | SH 16 | | | | | | |
| Description: | | Construct 2 lane roadway on new location | | | | | | |
| Project History: | | 4/12 - funded through STP-MM project selection process | | | | | | |

| Total Project Cost Information (TxDOT %): | | | Type of Work: Added Capacity: Non - Toll | | | | | | |
|---|-------------|---|--|--------------------------------------|--------------|--------------|----------------------|--------------|-------------|
| Preliminary Engineering | \$65,474 | Cost of Approved Phases: \$1,336,212 | <u>Funding Categories</u> | Authorized Funding by Category/Share | | | | | |
| ROW Purchase: | \$0 | | | <u>Federal</u> | <u>State</u> | <u>Local</u> | <u>Local Contrib</u> | <u>Total</u> | |
| Construction Cost: | \$1,336,212 | | | 7 - STP-MM | \$1,068,970 | \$0 | \$267,242 | \$0 | \$1,336,212 |
| Construction Engineering | \$65,474 | | | Other | \$0 | \$0 | \$0 | \$0 | \$0 |
| Contingencies: | \$89,660 | | | Other | \$0 | \$0 | \$0 | \$0 | \$0 |
| Indirect Costs: | \$91,531 | | | Other | \$0 | \$0 | \$0 | \$0 | \$0 |
| Other Field | \$0 | | | Totals | \$1,068,970 | \$0 | \$267,242 | \$0 | \$1,336,212 |
| Total Project Cost: | \$1,648,351 | | | | | | | | |

Appendix F
Resource/Topic-Specific Maps



LAND USE
 ON QUINTANA RD. FROM HARMON AVE.
 TO MCKENNA AVE.
 San Antonio, Bexar County, Texas
 CSJ: 0915-12-520

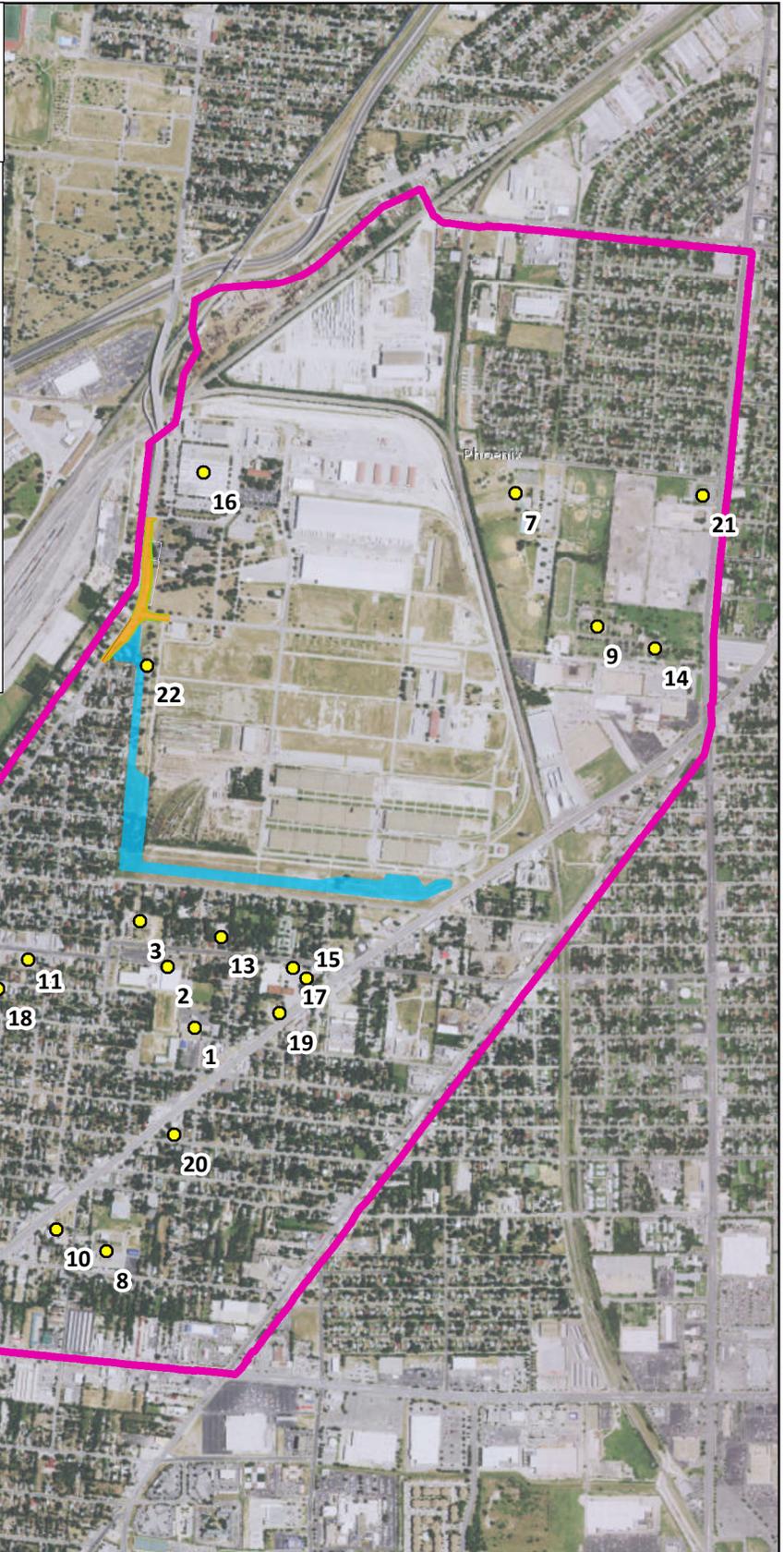


SOURCE: Land Use data derived from City of San Antonio Zoning data (Some zoning codes of similar type combined). Last updated: 08-15-13 msd

LEGEND

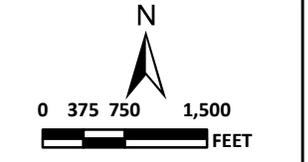
- Proposed Intersection Improvements
- Proposed Detention Pond and Channel
- Quintana Community (Neighborhood Assoc.)

| Map ID | Name | Type |
|--------|---|-------------------|
| 1 | Athens Avenue School | Public School |
| 2 | Dwight Middle School | Public School |
| 3 | Filadelfia Hispanic Baptist | Place of Worship |
| 4 | Iglesia Crisiana Emanuel | Place of Worship |
| 5 | Mensajeros Del Gran Rey | Place of Worship |
| 6 | Miguel Carrillo Junior Elementary School | Public School. |
| 7 | Normoyle Park | Park |
| 8 | Price Avenue School | Public School |
| 9 | San Antonio Can High School | Charter School |
| 10 | San Antonio Fire Station | Fire Station |
| 11 | South San Antonio Alternative | Public School |
| 12 | South San Community Center | Community Center |
| 13 | South San Transition Education Program | Public School |
| 14 | St Anthony Day Care/Learning | Day Care Center |
| 15 | St Joseph's Church | Place of Worship |
| 16 | St. Philips College Southwest Campus | Public School |
| 17 | St. Vincent de Paul Society | Place of Worship |
| 18 | Templo Amor Y Gracia | Place of Worship |
| 19 | Templo Victoria & Cristo | Place of Worship |
| 20 | The River Worship Centre | Place of Worship |
| 21 | Zarzamora Clinic - University Health System | Healthcare |
| 22 | Former RV Court (Quintana Park) | Recreational Area |



COMMUNITY BOUNDARIES & FEATURES

ON QUINTANA RD. FROM HARMON AVE.
TO MCKENNA AVE.
San Antonio, Bexar County, Texas
CSJ: 0915-12-520

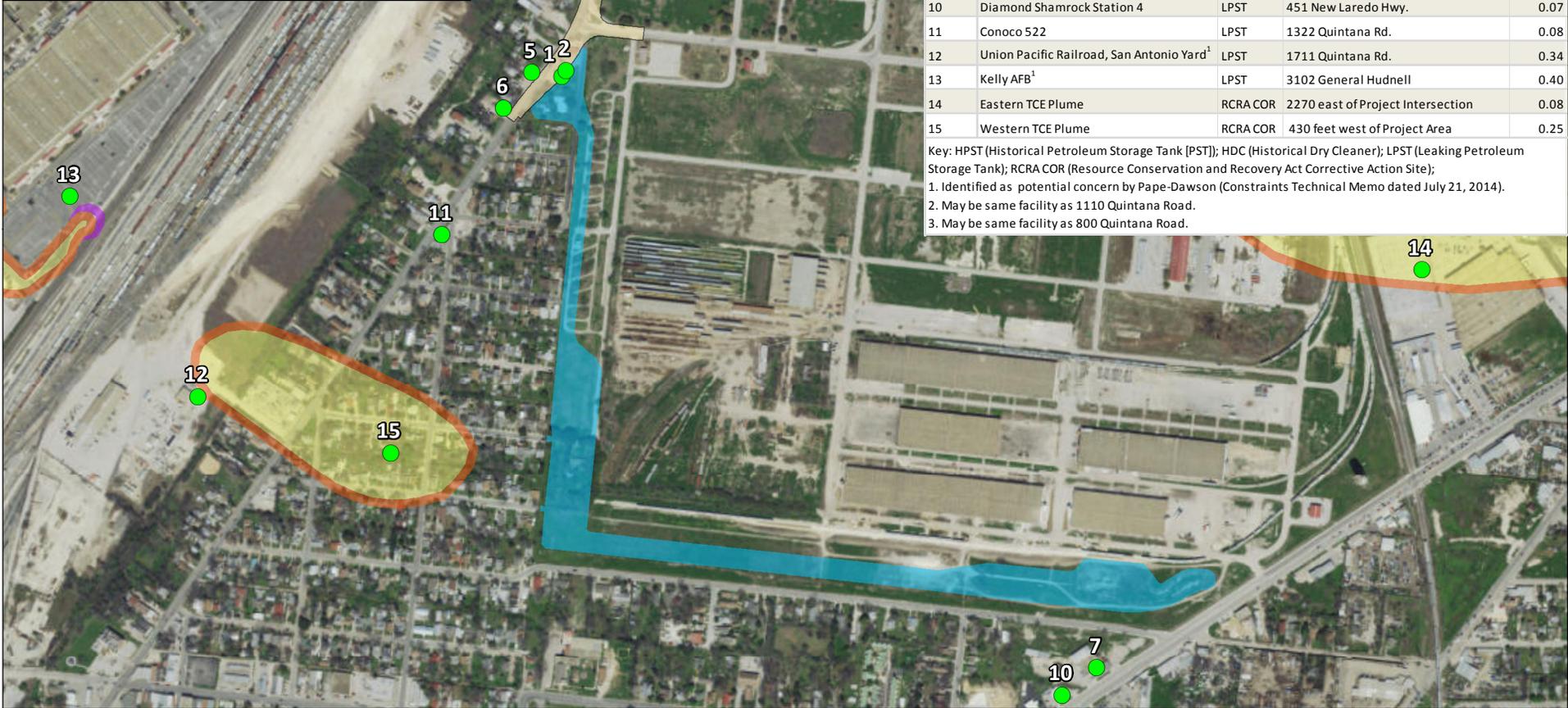


Appendix F-2

SOURCE: Basemap from Environmental Systems Research Institute (ESRI)

LEGEND

- Proposed Intersection Improvements (Footprint)
- Proposed Detention Pond and Channel (Footprint)
- ISA FEATURE
- Approx. TCE Plume (2015)
- Approx. PCE Plume (2015)

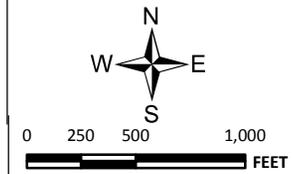


| Feature | Description | Type | Address | Distance |
|---------|---|----------|---------------------------------------|----------|
| 1 | Signor Shamrock Service Station ¹ | HPST | 1110 Quintana Rd. | 0.00 |
| 2 | South San Antonio Garage ^{1,2} | HPST | 1104 Quintana Rd. | 0.00 |
| 3 | Sunglo Station ³ | PST | 1011 Quintana Rd. | 0.01 |
| 4 | Quintana Laundry (Dry Cleaners) ¹ | HDC | 1007 Quintana Rd. | 0.01 |
| 5 | Flamingo Steam Cleaners ¹ | HDC | 1129 Quintana Rd. | 0.01 |
| 6 | East Kelly Automotive ¹ | HPST | 1227 Quintana Rd. | 0.03 |
| 7 | First Stop Stores ¹ | LPST | 433 New Laredo Hwy. | 0.04 |
| 8 | John Stapper ¹ | LPST | 800 Black Quintance Rd. (Quintana Rd) | 0.06 |
| 9 | John M Stapper Property ² | PST | 809 Quintana Rd. | 0.06 |
| 10 | Diamond Shamrock Station 4 | LPST | 451 New Laredo Hwy. | 0.07 |
| 11 | Conoco 522 | LPST | 1322 Quintana Rd. | 0.08 |
| 12 | Union Pacific Railroad, San Antonio Yard ¹ | LPST | 1711 Quintana Rd. | 0.34 |
| 13 | Kelly AFB ¹ | LPST | 3102 General Hudnell | 0.40 |
| 14 | Eastern TCE Plume | RCRA COR | 2270 east of Project Intersection | 0.08 |
| 15 | Western TCE Plume | RCRA COR | 430 feet west of Project Area | 0.25 |

Key: HPST (Historical Petroleum Storage Tank (PST)); HDC (Historical Dry Cleaner); LPST (Leaking Petroleum Storage Tank); RCRA COR (Resource Conservation and Recovery Act Corrective Action Site);
 1. Identified as potential concern by Pape-Dawson (Constraints Technical Memo dated July 21, 2014).
 2. May be same facility as 1110 Quintana Road.
 3. May be same facility as 800 Quintana Road.



HAZMAT ISA FEATURES
 ON QUINTANA RD. FROM HARMON AVE. TO MCKENNA AVE.
 SAN ANTONIO, BEXAR COUNTY, TEXAS
 CSJ: 0915-12-520

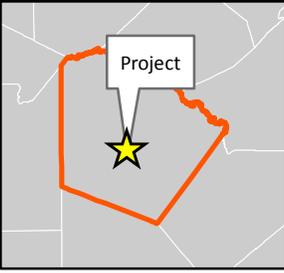


Appendix F-3

SOURCE: Aerial Photograph Obtained from the City of San Antonio (COSA) - 2013. Plume locations approximate - derived from Arias Limited geotechnical Report (Map from CB&I Federal Service) - Combines overlapping TCE plumes.

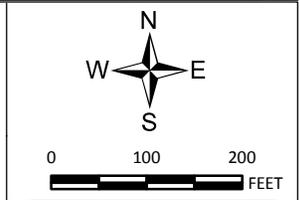
LEGEND

- NON-IMPACTED RECEIVER
- - - PROPOSED RIGHT-OF-WAY
- - - EXISTING RIGHT-OF-WAY



NOISE RECEIVER LOCATION MAP

ON QUINTANA RD. FROM HARMON AVE. TO MCKENNA AVE.
SAN ANTONIO, BEXAR COUNTY, TEXAS
CSJ: 0915-12-520



SOURCE: Aerial Photograph Obtained from Google Earth Pro - 2016

Appendix G
Agency Correspondence



April 5, 2016

To Whom It May Concern:

This letter is to acknowledge that Port San Antonio is the Official with Jurisdiction (OWJ) over a former recreational vehicle court, informally referred to as "Quintana Park," which is located adjacent to Lester Avenue and Dunton Avenue. This property is located within an area of the proposed Quintana Road Widening and Drainage project (CSJ # 0915-12-520). Port San Antonio is the OWJ and project sponsor.

The area in question was historically used as a recreational vehicle (RV) park by the former Kelly Air Force Base, which remained vacant and unused for over 20 years. In the spring of 2014 the Port equipped the site with picnic tables, installed a park sign and made the area available to the community as a recreational use. The Port retains ownership and control of this parcel, and there are no immediate plans to transfer the property to the City of San Antonio's Parks and Recreation Department. Since the site is largely open space with no defined goal or official plans for recreational facilities, Port San Antonio does not consider the site to be significant as a park.

Port San Antonio has designed proposed drainage improvements in a manner that will retain as much continued access to this open space as possible to continue serving as a recreational area upon completion of the drainage project. As the OWJ and project sponsor, Port San Antonio fully supports the proposed use of this space for the Quintana Road Widening and Drainage project.

Sincerely,

A handwritten signature in black ink that reads "John Farrow". The signature is written in a cursive style with a large, prominent initial "J".

John Farrow
Director of Real Estate Development
Port San Antonio

Sam Blanco

From: Dana Lagarde <Dana.Lagarde@tpwd.texas.gov>
Sent: Friday, August 14, 2015 3:59 PM
To: Sam Blanco
Subject: RE: LWCF 6(f) List
Attachments: Local Park Grants in Bexar County.xls

Here you go.

Dana Lagarde
Texas Parks & Wildlife
Recreation Grants Branch
Local Park Grants Manager
512-389-8175 (o)
512-571-7847 (m)

[Visit us online](#)

From: Sam Blanco [<mailto:sblanco@rkci.com>]
Sent: Friday, August 14, 2015 2:18 PM
To: Dana Lagarde
Subject: RE: LWCF 6(f) List

All parks in Bexar County if it's easy enough. It would be helpful for other projects as well. Thanks.

SAMUEL BLANCO, AICP
ASSOCIATE • SENIOR ENVIRONMENTAL PLANNER



Raba Kistner Environmental, Inc. • 12821 W. Golden Lane • San Antonio, TX 78249
P 210.699.9090 • D 210.694.3691 • F 210.699.6426 • sblanco@rkci.com



***Purpose:** To provide professional consulting services with passion and integrity, to help build a better world for our employees, their families, our clients, and the communities we serve.*

***Quality:** Raba Kistner, Inc. is committed to delivering our services right, the first time, on time, every time.*

TBPE Firm F-3257

This electronic communication and its attachments contain confidential information. They are forwarded to you without passing through our standard review process. Design data and recommendations included herein should not be used for final design. If you have received this information in error, please notify the sender immediately.

From: Dana Lagarde [<mailto:Dana.Lagarde@tpwd.texas.gov>]
Sent: Friday, August 14, 2015 2:16 PM
To: Sam Blanco
Subject: RE: LWCF 6(f) List

Sure. Anything that Bexar County owns or any parks within Bexar County?

Dana Lagarde
Texas Parks & Wildlife
Recreation Grants Branch
Local Park Grants Manager
512-389-8175 (o)
512-571-7847 (m)

[Visit us online](#)

From: Sam Blanco [<mailto:sblanco@rkci.com>]
Sent: Friday, August 14, 2015 2:14 PM
To: Dana Lagarde
Subject: LWCF 6(f) List

Dana, could you please provide the latest list for Bexar County? Thank you.

SAMUEL BLANCO, AICP
ASSOCIATE • SENIOR ENVIRONMENTAL PLANNER



Raba Kistner Environmental, Inc. • 12821 W. Golden Lane • San Antonio, TX 78249
P 210.699.9090 • D 210.694.3691 • F 210.699.6426 • sblanco@rkci.com



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Quality: Raba Kistner, Inc. is committed to delivering our services right, the first time, on time, every time.

TBPE Firm F-3257

This electronic communication and its attachments contain confidential information. They are forwarded to you without passing through our standard review process. Design data and recommendations included herein should not be used for final design. If you have received this information in error, please notify the sender immediately.

Local Park Grants Funded in Bexar County

Name

BEXAR COUNTY 15-000026 GREEN BRIGADE PROGRAM
 BEXAR COUNTY 20-000272 RODRIGUEZ PARK
 BEXAR COUNTY 20-000311 MISSION COUNTY PARK
 BEXAR COUNTY 48-000459 PADRE PARK
 BEXAR COUNTY 48-000656 MACARTHUR PARK
 BEXAR COUNTY 48-001012 PLETZ PARK
 BEXAR COUNTY 55-000008 Mission County Park II
 CONVERSE 20-000461 PARK NORTH EXPANSION
 CONVERSE 48-000348 CITY PARK
 CONVERSE 48-000956 CITY PARK II
 HOLLYWOOD PARK 20-000129 VOIGT PARK
 KIRBY 20-000070 HUGO LENTZ CITY PARK PHASE III (FRIENDSHIP PARK)
 KIRBY 48-000494 HUGO LENTZ CITY PARK
 KIRBY 48-000682 HUGO LENTZ CITY PARK (PHASE II)
 LEON VALLEY 48-000402 CITY PARK (RAYMOND RIMKUS PARK)
 LIVE OAK 48-000432 CITY PARK (Municipal)
 San Antonio 15-000005 BRACKENRIDGE PARK RENOVATION
 San Antonio 15-000012 REHABILITATE PRIORITY RA
 San Antonio 15-000020 BRACKENRIDGE REHAB
 San Antonio 15-000027 SAN ANTONIO ROVING LEADER OUTREACH
 San Antonio 15-000029 MISSIONS HIKE/BIKE TRAIL
 San Antonio 15-000040 LINCOLN PARK POOL & PLAYGROUND REHAB
 San Antonio 20-000087 TRAVIS PARK
 San Antonio 20-000167 EISENHOWER PARK
 San Antonio 20-000403 MILAM PARK
 San Antonio 20-000475 SOUTH NEW BRAUNFELS AVENUE PARK (PYTEL PARK)
 San Antonio 48-000114 EDGEWOOD AND 24TH STREET PARKS (VIDAURRI AND NAVARRO)
 San Antonio 48-000131 MONTERRY PARK
 San Antonio 48-000150 NORMOYLE PLAYFIELD
 San Antonio 48-000154 PITTMAN-SULLIVAN PARK
 San Antonio 48-000159 DRESSING ROOM FACILITIES - 5 POOLS
 San Antonio 48-000161 MCFARLIN TENNIS CENTER
 San Antonio 48-000185 SWIMMING POOL FILTERS
 San Antonio 48-000208 NORTHEAST PRESERVE (MCALLISTER PARK)
 San Antonio 48-000300 FRIEDRICH PARK
 San Antonio 48-000374 ACEQUIA RECREATION PARK
 San Antonio 48-000398 VALLEY HI COMMUNITY PARK (Millers Pond Community Park)
 San Antonio 48-000495 DAFOSTE NEIGHBORHOOD PARK
 San Antonio 48-000496 KENNEDY PARK ACQUISITION
 San Antonio 48-000497 JOHN JAMES PARK
 San Antonio 48-000550 CUELLAR PARK SWIMMING POOL
 San Antonio 48-000555 GARZA PARK SWIMMING POOL
 San Antonio 48-000561 MILLERS POND COMMUNITY PARK
 San Antonio 48-000572 KENNEDY PARK
 San Antonio 48-000689 ROSEDALE PARK
 San Antonio 48-000710 ALDERETE PARK
 San Antonio 48-000821 PARKS (BROOKS LEVI STRAUSS AND FRIESENHAHN)
 San Antonio 48-000998 (RAINBOW HILLS) PARK
 San Antonio 48-001032 MEDINA RIVER PARK (53-00007)
 San Antonio 50-000045 FRIEDRICH Park II
 San Antonio 53-000007 MEDINA RIVER PARK [48-01032]
 San Antonio 55-000003 Phil Hardberger Park (formerly Max & Minnie Voelcker)
 San Antonio 55-000014 Friedrich Wilderness Park III
 San Antonio 55-000020 Playground Replacements (3 sites)
 San Antonio 55-000022 Family Adventure Garden
 San Antonio 56-000002 Phil Hardberger Ecology Center
 San Antonio 56-000004 Jingu House Restoration

SAN ANTONIO MUD #1 48-000493 WILDLAKE DISTRICT PARK
san antonio river authority 48-000040 BRAUNIG LAKE PARK
san antonio river authority 48-000137 CALAVERAS LAKE PARK BOAT RAMP
Sports Outdoor & Recreation (SOAR) Inc. 50-000434 Morgan's Wonderland Park
UNIVERSAL CITY 20-000194 CITY PARK II
UNIVERSAL CITY 48-000611 CITY PARK
Windcrest 54-000095 Takas/Windy Hollow Park



125 EAST 11TH STREET, AUSTIN, TEXAS 78701-2483 | 512.463.8588 | WWW.TXDOT.GOV

September 13, 2016

Re: Transmittal of Raba Kistner Environmental, Inc. Draft Report: *Report for Archeological Survey: Quintana Road, CSJ 0915-12-520, Bexar County, San Antonio District; by Kristi Miller Nichols, Steve Tomka, Principal Investigator.* Texas Antiquities Permit Number 7708.

Ms. Pat Mercado-Allinger
Division Director/State Archeologist
Division of Archeology
Texas Historical Commission
P.O. Box 12276
Austin, TX 78711

Dear Ms. Mercado-Allinger:

Attached for your review is a draft report by Kristi Miller Nichols of Raba Kistner Environmental, Inc. *Report for Archeological Survey: Quintana Road, CSJ 0915-12-520, Bexar County, San Antonio District; by Kristi Miller Nichols; Steve A. Tomka, Principal Investigator.* Texas Antiquities Permit Number 7708. The City of San Antonio is proposing to widen and re-align Quintana Road and improve flood control. The project would also add mixed use paths. The estimated Area of Potential Effects (APE) is 43.19 acres. Approximately 40 acres of new right of way would be required. Estimated depths of impact are two feet along the roadways, six to 10 feet in isolated areas if utilities are relocated and up to 10 feet deep for storm-water facilities.

The records research identified Holocene-age alluvial soils in the APE; however, the survey encountered and documented extreme disturbance throughout the APE, mainly the result of urbanization and development. Ten shovel tests were excavated in areas with potential to contain intact deposits. Shovel Test 9 contained a lithic core. No other artifacts materials, prehistoric or historic, were encountered. The core was returned to the shovel test and is considered an isolated find.

For the present survey, Raba Kistner inspected the surface of the APE and excavated 10 shovel tests to a maximum depth of 60 cm. Additional shovel tests were not excavated because of observed disturbances within the APE, including widespread asphalt paving and landscape modifications. Based upon the project setting, the number of shovel tests complies with the Council of Texas Archeology/Texas Historical Commission guidelines for Intensive Archeological Survey. Soils were too disturbed for backhoe trenches. No archeological material was collected and no curation of artifacts is needed.

The report documents that no archeological sites exist within the project's area of potential effects. A TxDOT archeologist has reviewed the report and identified a few needed corrections. TxDOT concurs with the results and recommendations of the report. **TxDOT seeks THC concurrence that:**

OUR VALUES: People • Accountability • Trust • Honesty

OUR MISSION: Through collaboration and leadership, we deliver a safe, reliable, and integrated transportation system that enables the movement of people and goods.

An Equal Opportunity Employer

1. Per our MOU, no archeological historic properties (36 CFR Part 800.16(1) or State Antiquities Landmarks (13 TAC 26.12) are present within the 43.19 acre APE examined by Raba Kistner, and none will be affected by the proposed undertaking. There is little likelihood of significant or intact prehistoric or historical archeological sites within the APE and no further archeological investigations are warranted.
2. Since the survey was conducted under an individual THC Antiquities Permit, we are forwarding the draft for your review and processing in partial fulfillment of THC Antiquities Permit No. 7780. TxDOT finds the report acceptable as a draft and pending any final report review comments from your office, we request your concurrence that the report may proceed toward final production and that it provides sufficient documentation that the proposed undertaking will have no effect on an archeological historic properties or Antiquities Landmarks.

In the event that unanticipated archeological deposits are encountered during construction, work in the immediate area will cease and TxDOT archeological staff will be contacted to initiate post-review discovery procedures under the provisions of the PA (2005) and the Memorandum of Understanding between TxDOT and the THC.

Thank you for your consideration of this matter. If you have any questions regarding the report, please contact Principal Investigator Steve A. Tomka of Raba Kistner Environmental, Inc. at (210) 694-3633. Please contact me at (512) 416-2505 for all other matters.

Sincerely,

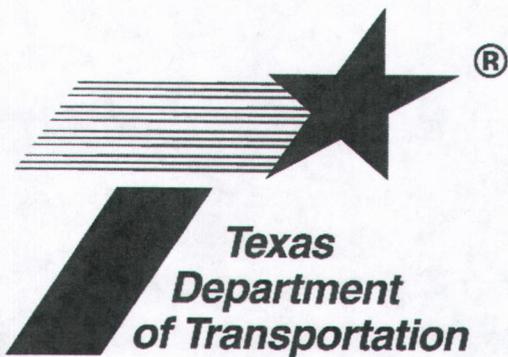


Eric Oksanen, Archeological Studies Program
Environmental Affairs Division

cc w/o attachments: ECOS Project File; Steve A. Tomka, Raba Kistner Environmental, Inc.

| | |
|--|-----------------|
| Concurrence By: | |
|  for: Mark Wolfe, Executive Director and SHPO Texas Historical Commission | 9-15-16 Date |

DRAFT



Report for Archeological Survey

**DRAFT REPORT
ACCEPTABLE**

by *Mark Wolfe*
for Mark Wolfe
Executive Director, THC
Date 9-15-14
Track# _____

Quintana Road, CSJ 0915-12-520,
Bexar County, San Antonio District

Principal Investigator, Steve A. Tomka, Antiquities Permit No.7708

The environmental review, consultation, and other actions required by applicable Federal environmental laws for this project are being, or have been, carried-out by TxDOT pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 16, 2014, and executed by FHWA and TxDOT.



CITY OF SAN ANTONIO
TRANSPORTATION & CAPITAL IMPROVEMENTS

February 20, 2017

John Farrow, Director of Real Estate Development
Port San Antonio
907 Billy Mitchell Blvd.
San Antonio, TX 78226

SUBJECT: Chapter 26 Public Hearing Notice
Quintana Road from Harmon Avenue to McKenna Avenue, Bexar County, Texas
CSJ: 0915-12-520

Dear Mr. Farrow:

A public hearing will be held to address requirements of Texas Parks and Wildlife Code Title 3 Chapter 26 related to proposed use of a publicly-owned recreation area, Quintana Park, which is located along Lester Avenue. Approximately 1.15 acres of the 4.71-acre park would be displaced by proposed drainage improvements, while the remainder of the park would remain publicly-accessible after construction. 3 PWC 26.002[a) requires that "at least 30 days before the date for the public hearing" a notice of hearing "must be given in writing to the person, organization, department, or agency that has supervision of the land proposed to be used or taken." As owner of the property subject to Chapter 26, this letter is to inform Port San Antonio that a public hearing will be held on Tuesday, March 28, 2017, at Alamo Colleges Workforce Center of Excellence - Building 8 on the St. Philips Southwest Campus addressed at 800 Quintana Road (also 203 Norton Street), San Antonio, Texas 78211. The facility is located immediately south of Harmon Street. The hearing will take place in the Atrium (Room 117). Displays will be available for viewing at 6:00 pm with the formal hearing starting at 7:00 pm. The purpose of the hearing is to present the proposed improvements and to receive public comment on the proposed project.

Environmental documentation or studies, maps showing the project location and design, tentative construction schedules, and other information regarding the project will be available for inspection at the hearing. Verbal and written comments from the public regarding the project are requested and may be presented at the hearing, or submitted in person or by mail to the City of San Antonio Transportation and Capital Improvements Department. Comments must be received on or before Wednesday, April 12, 2017, to be part of the official hearing record.

If you have any general questions or concerns regarding the proposed project or the hearing, please contact David Lopez, P.E., COSA TCI Project Manager with the City of San Antonio at (210) 207-5001 or david.lopez@sanantonio.gov.

The environmental review, consultation, and other actions required by applicable Federal environmental laws for this project are being, or have been, carried-out by TxDOT pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 16, 2014, and executed by FHWA and TxDOT.

Sincerely,

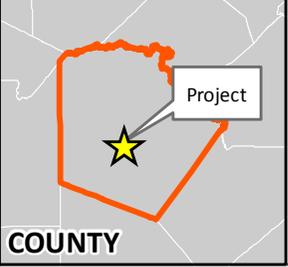
David Lopez, P.E., COSA TCI Project Manager
Transportation & Capital Improvements

Enclosures: Project Location Map; Public Hearing Location Map



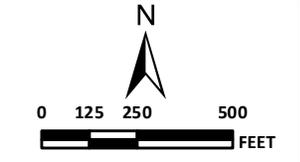
PUBLIC HEARING
 Alamo Colleges Workforce Center of Excellence - Building 8
 St. Philips Southwest Campus
 800 Quintana Road (also 203 Norton Street)
 San Antonio, Texas 7821

- LEGEND**
- Proposed Intersection Improvements
 - Proposed Detention Pond and Channel



PROJECT & PUBLIC HEARING LOCATION

ON QUINTANA RD. FROM HARMON AVE.
 TO MCKENNA AVE.
 San Antonio, Bexar County, Texas
 CSJ: 0915-12-520



SOURCE: Basemap from Environmental Systems Research Institute (ESRI)

Appendix H
Public Meeting Comment-Response Table

| Comment-Response Matrix - Quintana Road Public Meeting - June 30, 2016 | | | | |
|--|-----------|---------------|--|---|
| Comment Type (e.g. form, e-mail) | Date | Commenter | Comment (Paraphrased) | Response |
| Verbal | 6/30/2016 | Unnamed | Concerned about drainage issues, where is the water going? | David McBeth explained the construction and function of the drainage portion of the construction plans. |
| Verbal | 6/30/2016 | Unnamed | How long for drainage and how it will affect traffic? | David McBeth explained the traffic plan for the project and that two lanes will be open at all times. |
| Verbal | 6/30/2016 | Unnamed | What are the objectives of the project. | David McBeth explained drainage and function. Showed the lane specifics. |
| Verbal | 6/30/2016 | Unnamed | Concerned about truck traffic on Lester and McKenna roads. Will Lester road be closed? | In response to the truck traffic concerns, Paco Felici explained that most trucks do not go on Quintana and SAPD should enforce No-Thru zone. |
| Verbal | 6/30/2016 | Unnamed | Mosquito control problems are of concern to residents near drainage. | Explained the water should not stay in the drainage channels long enough for mosquitos to breed. |
| Verbal | 6/30/2016 | Unnamed | Is the project fully funded? | Yes. |
| Verbal | 6/30/2016 | Unnamed | Concern of environmental issues. Contamination and if the water has been tested. Will people have to evacuate because of a contamination at Kelly AFB. The plume is of concern. | Rick Klar and Sam Blanco explained the Hazmat ISA process. Rick Klar explained that no construction will begin until environmental process is complete. The environmental process will include subsurface sampling in areas of concern within the project area. |
| Verbal | 6/30/2016 | Unnamed | Concern over house/yard flooding. Will the drainage area dry out? Want of a deep channel or pond to catch water. | Explained the channel will be deep enough to retain water and will improve drainage for the adjacent neighborhoods. |
| Verbal | 6/30/2016 | Unnamed | If Lester is closed for the project will it open again? | A portion of Lester will be permanently closed. |
| Verbal | 6/30/2016 | Unnamed | Would McKenna be usable? | Yes. |
| Verbal | 6/30/2016 | Unnamed | Concern of park accessibility and use. Would like to look a opportunities to have access to the park. | Paco Felici noted the Port will look into it and may add amenities, however access may be an issue during construction. |
| Verbal | 6/30/2016 | Unnamed | Question of how far down sidewalks will go on West Quintana and if the sidewalks dead end. | David explained project limits and yes they will be stopped at the project boundary. |
| Verbal | 6/30/2016 | Unnamed | Question if anything will be done to slow traffic. Concern over wrecks and the speed of vehicles coming down the ramp from Cupples. | David McBeth noted they would look into what could be done to slow traffic. A traffic study will be done to determine if a light is warranted at the intersection of Hubert Harmon and Quintana Road. |
| Verbal | 6/30/2016 | Unnamed | Request to have a site visit so City can describe the improvements on the ground. | A site visit will be scheduled. Interested persons were asked to sign up. |
| Written | 7/2/2016 | David Sawyer | I have only one concern. My business is at 1111 Quintana Rd. That is the problem with cars speeding down Quintana Rd. This has been ongoing for 15 or more years. It starts with cars coming from the top of the bridge at St. Phillips College and at the curve in the road at Dunton and Quintana Rd. we have had the telephone poles hit and replace 8 or 10 time in just the past year, we have had our business signs damaged. Cars, houses, people injured and even killed in just that one spot. What needs to be done is have a red light installed at the bottom of the bridge in front of St. Phillips College. This will slow down the traffic before it reaches the curve on Quintana Rd. and Dunton. This will also help control the traffic flow into the campus. I hope someone has the insight to do something before anyone else gets hurt or killed. Please don't just take my word on this matter. Check it out for yourself and see. | A traffic study will be done to see if a light is warranted at the intersection of Hubert Harmon and Quintana Road. |
| Written | 7/8/2016 | Martha Galvan | Traffic Light: The vast majority of drivers pick up high speed coming down the Cupples bridge (towards SW Military Dr.). A traffic light is imperative at the bottom of the Cupples bridge for St. Phillips College students and pedestrians safety. Kelly AFB Contamination of Water and Soil -NOW- Port San Antonio Industrial Park: The Air Force acknowledged decades ago; contaminated groundwater in and around the base stemming from chemicals like trichloroethylene (TCE), a degreaser, and tetrachloroethylene (PCE). Officials later admitted that workers drained chemical waste for years directly into the ground, or dumped it into a nearby creek. In addition to TCE and PCE, Kelly workers also handled and dumped dangerous toxins like dichloromethane (DCE), benzene, vinyl chloride and thallium, known and suspected cancer-causing agents. In the late 1980s construction workers unearthed toxic fumes and collapsed when digging along Quintana Road. Many residents are concerned with the proposed construction on Quintana Road and would like to review the Environmental Analysis prior to implementing the project. Sidewalk and Bike Trails on West of Quintana Rd.: The NEW sidewalk and bike trails on the West side of Quintana will just dead end onto a dirt road. No current sidewalk exits along the west of Quintana Rd. for students, pedestrians and bicyclists. In closing these are just a few of the concerns that were addressed at the open house on June 30, 2016. I look forward to the Quintana improvements. | In response the traffic light request you've highlighted below it is currently not part of this project, however we will have our traffic division follow up on your request once construction has been completed to see if it is warranted. The Draft Environmental Assessment (EA) and supporting studies may be obtained directly from TxDOT once they have been reviewed and approved. The Draft EA will contain instructions on how the public can obtain these studies from TxDOT. Connection of the sidewalk would be another part of a future Bond project. Keep working with your council office for approval and future funding |

Appendix I
Public Hearing Comment-Response Table

Comment-Response Matrix - Quintana Road Public Hearing - March 28, 2017

| Comment | Comment Type | Date | Commenter | Comment (Paraphrased) | Response |
|---------|--|-----------|--------------------------------------|--|--|
| A | Verbal | 3/28/2017 | Shirley Gonzales, Council District 5 | <ol style="list-style-type: none"> 1. Concerned with the hearing format and that the public would not have the opportunity to have questions responded to. 2. Where will the responses be located? 3. When could the questions be answered? 4. When did Quintana project start? | <ol style="list-style-type: none"> 1. Opportunity for dialog and to have questions answered was provided during the 1 hour open house prior to the formal hearing. In addition, an informal Open House Meeting was held on June 9, 2016, during which the public was invited to interact with the project team and to provide comments or ask questions. During that meeting, a question and answer session took place. In addition, a field trip was held on November 16, 2016, with eight attendees available to make comments or ask questions. 2. Responses will be housed at the Port San Antonio office and on the City of San Antonio website. For location and viewing hours, those interested can contact Paco Felici (at Port San Antonio) 210-362-7805 or visit www.sanantonio.gov/Quintana 3. After the Public Hearing, City staff and Port staff stayed and spoke to residents and answered their questions. 4. August 24, 2009 |
| B | Written/Verbal Comment to Court Reporter after hearing was adjourned | 3/28/2017 | Felipa Lopez | <ol style="list-style-type: none"> 1. "PSA intent to develop Quintana Park for wellness activity - walking and bike trails" 2. "PSA initiative to prevent heavy duty / 18 wheeler trucks from using Harmon Street entry (<i>illegible</i>) exit" 3. "Any future plan to install a median or curb to prevent cars from making a U-turn at the entry at Harmon Street?" 4. (<i>From transcript</i>) In reviewing the design plan there doesn't seem to be any forethought for St. Philip's College Southwest campus which has an entry point at Harmon Street. That particular entry point on Harmon Street has a large amount of traffic. Not only heavy-duty vehicles like trucks; it is a bus route. There is no pedestrian walkway into the campus. The Harmon street becomes a easily U-turn for vehicles coming down the overpass and going up the overpass, so that becomes a dangerous safety point for all of the students. 5. (<i>From transcript</i>) Is there any future plan to make Dunton Street the primary entrance into that section of Port San Antonio simply because of the safety impact of all of the students that attend St. Philip's College Southwest Campus. 6. (<i>From transcript</i>) Alamo College has a bond that it's trying to pass, and if that bond passes that means that St. Philip's College Southwest campus will be building a new facility, which would allow for, at least, an additional increase in student enrollment; therefore, we need to think in the future what do we plan to do with Harmon street. Because St. Philip's College Southwest campus is a vital community campus for the southwest side of San Antonio. | <ol style="list-style-type: none"> 1. This project includes restoration of existing wellness activities along the perimeter of what is referred to as Quintana Park, this restoration being the replacement of disturbed asphalt with concrete walks. In addition the park area will be connected with by way of sidewalk to the new shared use path along Quintana Road and along Dunton Avenue. In response to community concerns, the project will also incorporate a walkway over the detention pond at McKenna Ave. 2. This is outside the project scope. The Port is not aware of trucks exiting from Harmon because Dunton provides better access. There are no plans in design at this time, because the Port does not have a history of truck usage showing this activity at this time. 3. This is not part of the project scope. There is no plan to add medians at this time. 4. This is not part of the project scope. It is an issue that should be taken up with Alamo Community College District (ACCD). This project actually is adding turn lane protection for Harmon for those traveling south on Quintana and entering the campus area. The project is also providing framework for the addition of sidewalk into the campus by others with the shared use path being placed along the entire project limits. Students will be able to use the new sidewalk at the Harmon intersection to access bus stops on Quintana. Additional pedestrian walkways beyond that provided in this project will need to be planned within another project, as part of campus improvements, and/or coordinated with ACCD planning and engineering staff. Improvements to the Harmon intersection and street are not a part of this project. 5. Dunton is the primary ingress and egress point for the East Kelly area owned by Port San Antonio and truck traffic is encouraged to use this as the access point. Improvements to the intersection will provide better ingress and egress, which will attract traffic to that intersection instead of Harmon. 6. This is not part of the project scope. It is an issue that should be taken up with ACCD. Improvements to the ACCD campus have been the subject of coordination meetings between the Port, COSA, and ACCD planning and their engineering consultant, and facilitating drainage needs that result from this campus expansion has been a Quintana Project objective since its inception. Alleviating the flooding at the Dunton intersection will provide a safer and more reliable ingress and egress point and better facilitate multimodal transportation in the area. As for Harmon Street, improvements provided on the Quintana project include the dedicated turn lane for southbound Quintana traffic and addition of a shared use path extending to Harmon along the Quintana project's entire eastern boundary. The Port and their design consultant will be happy to continue to work with ACCD planning staff and their engineering consultants to make sure the Quintana Project facilitates ACCD needs along the limits of this project. ACCD will be required to coordinate their development plans with the City and mitigate any adverse effects to public infrastructure. |

Comment-Response Matrix - Quintana Road Public Hearing - March 28, 2017

| Comment | Comment Type | Date | Commenter | Comment (Paraphrased) | Response |
|---------|----------------|-----------|-----------------|--|---|
| C | Written/Verbal | 3/28/2017 | Al Rocha | <p>1. Concerned of property owners whose land will be taken for project. Hopes they are informed and process is fair.</p> <p>2. <i>(From transcript)</i> Looks like there's an opportunity to bottleneck going north and south, or mainly, coming down south. So, hopefully, there is a solution. There is a bond draft proposal for sidewalks and drainage; however, there's going to be a big gap between there, so asking for District 5 to "hear us out" that this project would be in the best interest for the community to continue this project widening the street all the way down to Military. You're widening the streets, and then going to narrow them down just south of Dunton, so it's going to not -- what I estimate is going to be a bottleneck in some -- in some instances. You're going to have a median and then all of a sudden you're not going to have a median. We're going to have sidewalks, then you're not going to have sidewalks continuing down all the way to Military.</p> <p>3. Sees opportunity for Quintana Park to "turn into a regular park with regular maintenance and regular opportunities for public events." Hopes this "in no way kind of de-emphasizes it as a park." With Port of San Antonio's assistance, sees opportunities to beautify and increase the park. Suggests adding lights and more dedicated park features.</p> <p>4. "What disruptions to existing lanes are proposed? "Any plans to minimize congestion while construction is taking place?"</p> | <p>1. Port San Antonio property will be primarily used for the project. Land will also be donated by Alamo Community Colleges. Land from a single private landowner is also required for the project, and the owner was informed of the intent to acquire his property during a June 9, 2016 meeting with the owner, during a Public Meeting held on June 30, 2016, and during this Public Hearing. Port San Antonio, the City of San Antonio, and TxDOT will comply with the Uniform Act to ensure that the owner is treated fairly, consistently, and equitably. Right-of-way information and TxDOT right-of-way staff were available at this public hearing to provide any information related to the property acquisition process and owners' rights.</p> <p>2. This not part of the project scope. Metropolitan Planning Organization (MPO) funding is only allowed to be used for the approved project scope: widening Quintana from McKenna to Harmon to improve traffic operations and correcting the existing flooding conditions for the Quintana/Dunton intersection. Road and sidewalk improvements south of McKenna and north of Harmon are not allowed as well as any park improvements in addition to restoring the existing conditions. This should be addressed separately relative to the City's intended use of the 2017 bond fund. Quintana is being widened within the limits allowed by current funding levels. If future funding becomes available, Quintana can be considered for additional improvements.</p> <p>3. Park improvements are not part of the project scope and will not be allowed due to the use of MPO funding. The Port has, for several years, been a neighborhood advocate assisting to identify ways in which the park can be improved. The Port will continue to help identify and secure necessary resources.</p> <p>4. Work will be done in phases so that traffic movement can be maintained in all directions. The project will maintain traffic in all current streets throughout construction including maintenance of 2 way traffic on Quintana at all times. Signage will be posted in advance of the project to alert the public and provide opportunity for alternative routes to be utilized.</p> |
| D | Written | 3/28/2017 | Linda Aguilar | <p>1. Would the one-way on Lester to McKenna remain open?</p> <p>2. Project would leave McKenna with one entrance from Quintana. We cannot U-turn to exit back to entrance. Have usage of back gates or entrance from Lester to McKenna. Alley way is used. We added asphalt to use as a driveway.</p> | <p>1. No. Lester will be closed and utilized for part of the drainage channel.</p> <p>2. Lester will remain open and be 2 way between McKenna and Griffin. Alley use will not change from its present use. There is no plan to close the alley between Lester and Quintana.</p> |
| E | Written | 3/28/2017 | Eliza Contreras | <p>1. Does not believe the project is beneficial to the community. Believes it would create a bottleneck effect for the residents. Drainage will cause flooding in other areas. Sinkhole concerns.</p> | <p>1. The community will benefit from this project by improved traffic movements at the Quintana/Dunton intersection, added left turn lane for southbound traffic at Quintana/Dunton intersection, and elimination of flooding on Quintana. There will be no increased flooding in other areas as a result of this project. The pavement collapse far south of the project area was caused by a deteriorating sanitary sewer main. The pavement collapse was not a naturally occurring event and is not expected to occur within these limits of Quintana.</p> |

Comment-Response Matrix - Quintana Road Public Hearing - March 28, 2017

| Comment | Comment Type | Date | Commenter | Comment (Paraphrased) | Response |
|---------|----------------|-----------|------------------|--|---|
| F | Written/Verbal | 3/28/2017 | Daniel Meza | <p>1. How much money the City of San Antonio is contributing?</p> <p>2. Concerned with safety of the area due to a number of complaints from citizens and businesses that the traffic is very, very fast coming north and south. Concerned with lack of prominent signs communicating to people to reduce speed.</p> <p>3. Citizens have requested sidewalks, expanded sidewalks. Suggests sidewalks should extend beyond Dunton and going down as far down south and north on Quintana as possible. Going north and south they're in a pretty deteriorated condition. There is "a major population" - student population at St. Philip's College - and "people really can't walk down Quintana Road." "Whatever moneys you can extend to improve that area" "is certainly needed." "The need is there . . . It's not based on a want... It is a need." Feels "that that's the maximum -- the best use of taxpayer dollars."</p> <p>4. Requests whatever can be done to improve the park "in terms of contributing to the lighting for community use or any other amenities that can be put in there to complement the jogging/walking/playground/park."</p> | <p>1. None. All funding is provided by MPO (federal), state and Port San Antonio.</p> <p>2. This is outside the project scope. This concern has been sent to City traffic engineers who will review traffic concerns. Funding for this project is limited to those methods that address the intersection and drainage improvements.</p> <p>3. This is outside the project scope. Sidewalks are being added within the limits of the project. Extension of sidewalks beyond this project will need to be completed within another project. This should be brought to the attention of the councilperson for this area and other community leaders. Quintana is being widened within the limits allowed by current funding levels. If future funding becomes available, Quintana can be considered for additional improvements.</p> <p>4. Park improvements are not part of the project scope and will not be allowed relative to the MPO funding. See response to Comment C-3.</p> |
| G | Verbal | 3/28/2017 | Gloria Hernandez | <p>1. Concerned with water drainage. "We don't have a problem in one place and then you move the problem to another place." "Be sure that this doesn't cause a problem for the neighbors over there on that end."</p> <p>2. "I think the project is probably a good project, because I work at St. Philip's College and I come to the southwest campus so I know that -- that area very well." Would like to see the project extended "a little bit more." "Sometimes just a little part isn't, you know, as effective as it -- it could be if we were to extend it." Extending the project down Quintana "would be a good for the area and probably a more efficient use of your money better, and we'd get -- get a good project -- get a good project done."</p> | <p>1. There will be no increased flooding in other areas as a result of this project. One of the project's design criteria is that it does not adversely impact properties located down-gradient of the proposed improvements. This is being done by adding ponds of sufficient capacity to store and release storm water over a period of time such that flows are not increased beyond the capacity of downstream systems.</p> <p>2. See response to Comment C-2.</p> |

Comment-Response Matrix - Quintana Road Public Hearing - March 28, 2017

| Comment | Comment Type | Date | Commenter | Comment (Paraphrased) | Response |
|---------|--------------|-----------|-----------------|--|---|
| H | Mail-in | 3/29/2017 | Meredith Wright | 1. She requested notice of street closures so she can inform customers. 2. Asked if night closures can be done after 7 pm and no later than 6 am and Sunday night | 1. Project team will notify residents of closure. 2. Notes will be added to plan set |

Appendix J
Port San Antonio BCAD Property Details and Maps

Bexar CAD

Property Search Results > 1107668 PORT AUTHORITY OF SAN ANTONIO for Year 2017

Property

Account

| | | | |
|---------------------------|---|--------------------|--|
| Property ID: | 1107668 | Legal Description: | NCB 11304 (PORT AUTHORITY OF SAN ANTONIO), BLOCK 2 LOT IRR N1496.20' OF 1 & NCB 8785 IRR 10.26, BY 79.47' OF TR 15 |
| Geographic ID: | 11304-002-0010 | Agent Code: | |
| Type: | Real | | |
| Property Use Code: | 5300 | | |
| Property Use Description: | EXEMPT - INDUSTRIAL BUILDING RESEARCH/ENGINEERING | | |

Location

| | | | |
|------------------|---|---------|-------|
| Address: | 438 N TAYMAN ST PORT SAN ANTONIO, TX 78211 | Mapsc0: | 649D4 |
| Neighborhood: | NBHD code12670 | Map ID: | |
| Neighborhood CD: | 12670 | | |

Owner

| | | | |
|------------------|---|--------------|-----------------|
| Name: | PORT AUTHORITY OF SAN ANTONIO | Owner ID: | 299691 |
| Mailing Address: | 907 BILLY MITCHELL RD SAN ANTONIO, TX 78226-1802 | % Ownership: | 100.0000000000% |
| | | Exemptions: | EX-XV |

Values

| | | | |
|---------------------------------------|---|-----|-----------------------|
| (+) Improvement Homesite Value: | + | N/A | |
| (+) Improvement Non-Homesite Value: | + | N/A | |
| (+) Land Homesite Value: | + | N/A | |
| (+) Land Non-Homesite Value: | + | N/A | Ag / Timber Use Value |
| (+) Agricultural Market Valuation: | + | N/A | N/A |
| (+) Timber Market Valuation: | + | N/A | N/A |
| ----- | | | |
| (=) Market Value: | = | N/A | |
| (-) Ag or Timber Use Value Reduction: | - | N/A | |
| ----- | | | |
| (=) Appraised Value: | = | N/A | |
| (-) HS Cap: | - | N/A | |
| ----- | | | |
| (=) Assessed Value: | = | N/A | |

Taxing Jurisdiction

Owner: PORT AUTHORITY OF SAN ANTONIO

% Ownership: 100.0000000000%

Total Value: N/A

| Entity | Description | Tax Rate | Appraised Value | Taxable Value | Estimated Tax |
|-----------------------------|--------------------------|----------|-----------------|---------------|---------------|
| 06 | BEXAR CO RD & FLOOD | N/A | N/A | N/A | N/A |
| 08 | SA RIVER AUTH | N/A | N/A | N/A | N/A |
| 09 | ALAMO COM COLLEGE | N/A | N/A | N/A | N/A |
| 10 | UNIV HEALTH SYSTEM | N/A | N/A | N/A | N/A |
| 11 | BEXAR COUNTY | N/A | N/A | N/A | N/A |
| 21 | CITY OF SAN ANTONIO | N/A | N/A | N/A | N/A |
| 57 | SAN ANTONIO ISD | N/A | N/A | N/A | N/A |
| 58 | SOUTH SAN ISD | N/A | N/A | N/A | N/A |
| CAD | BEXAR APPRAISAL DISTRICT | N/A | N/A | N/A | N/A |
| Total Tax Rate: | | N/A | | | |
| Taxes w/Current Exemptions: | | | | | N/A |
| Taxes w/o Exemptions: | | | | | N/A |

Improvement / Building

No improvements exist for this property.

Land

No land segments exist for this property.

Roll Value History

| Year | Improvements | Land Market | Ag Valuation | Appraised | HS Cap | Assessed |
|------|--------------|-------------|--------------|-----------|--------|----------|
| 2017 | N/A | N/A | N/A | N/A | N/A | N/A |
| 2016 | \$0 | \$0 | 0 | 0 | \$0 | \$0 |
| 2015 | \$0 | \$0 | 0 | 0 | \$0 | \$0 |
| 2014 | \$0 | \$0 | 0 | 0 | \$0 | \$0 |
| 2013 | \$0 | \$0 | 0 | 0 | \$0 | \$0 |
| 2012 | \$0 | \$0 | 0 | 0 | \$0 | \$0 |

Deed History - (Last 3 Deed Transactions)

| # | Deed Date | Type | Description | Grantor | Grantee | Volume | Page | Deed Number |
|---|-----------|------|-------------|---------|---------|--------|------|-------------|
|---|-----------|------|-------------|---------|---------|--------|------|-------------|

2017 data current as of Nov 7 2016 12:29AM.

2016 and prior year data current as of Oct 19 2016 10:27AM

For property information, contact (210) 242-2432 or (210) 224-8511 or email.

For website information, contact (210) 242-2500.

This year is not certified and ALL values will be represented with "N/A".

Property Identification #: 1107668

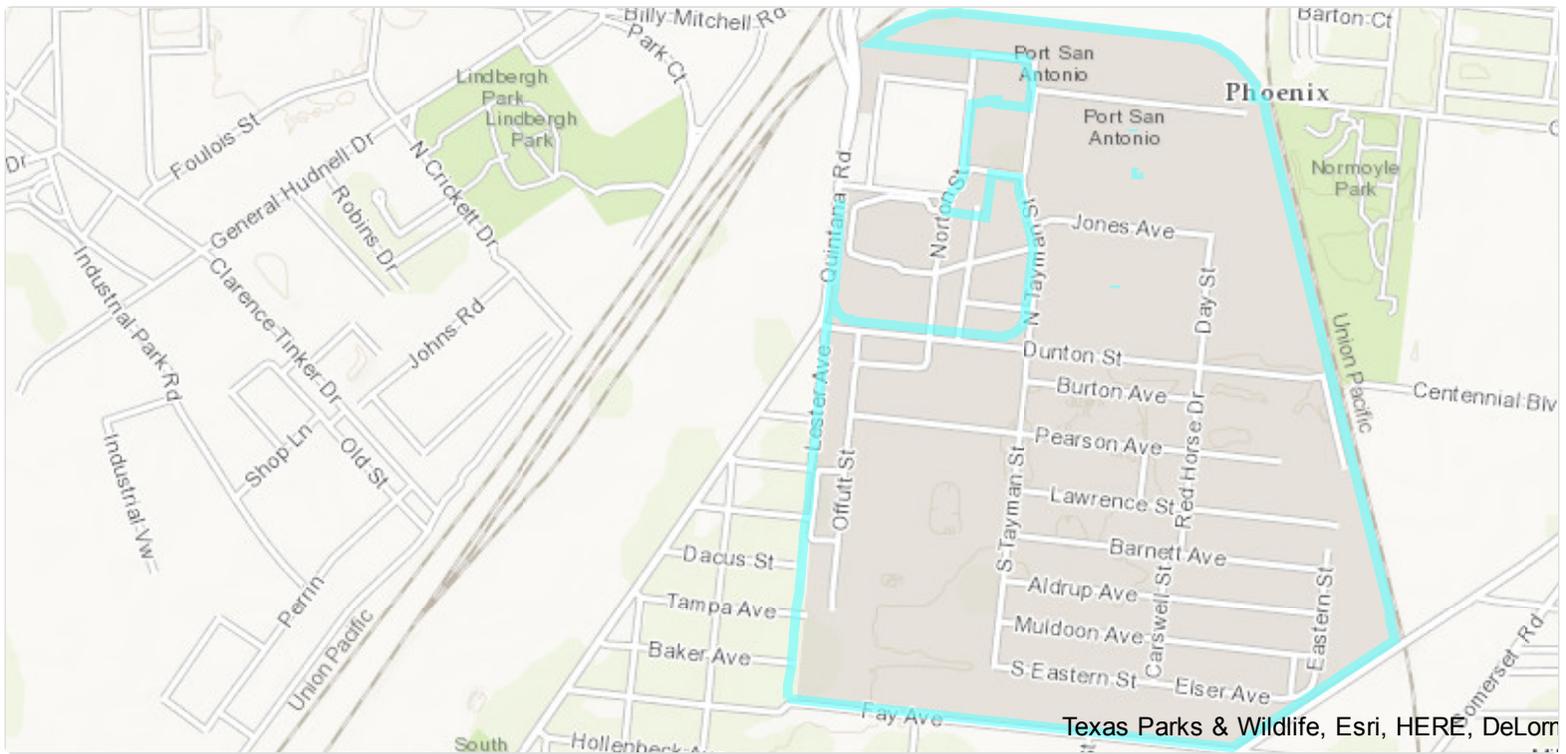
Geo ID: 11304-002-0010
Situs 438 N TAYMAN ST PORT SAN ANTONIO, TX 78211
Address: ANTONIO, TX 78211
Property Type: Real
State Code: Null

Property Information: 2017

Legal Description: NCB 11304 (PORT AUTHORITY OF SAN ANTONIO), BLOCK 2 LOT IRR N1496.20' OF 1 & NCB 8785 IRR 10.26, BY 79.47' OF TR 15
Abstract: S08785
Neighborhood: NBHD code12670
Appraised Value: N/A
Jurisdictions: 11, 58, 09, 06, 57, 10, 08, CAD, 21

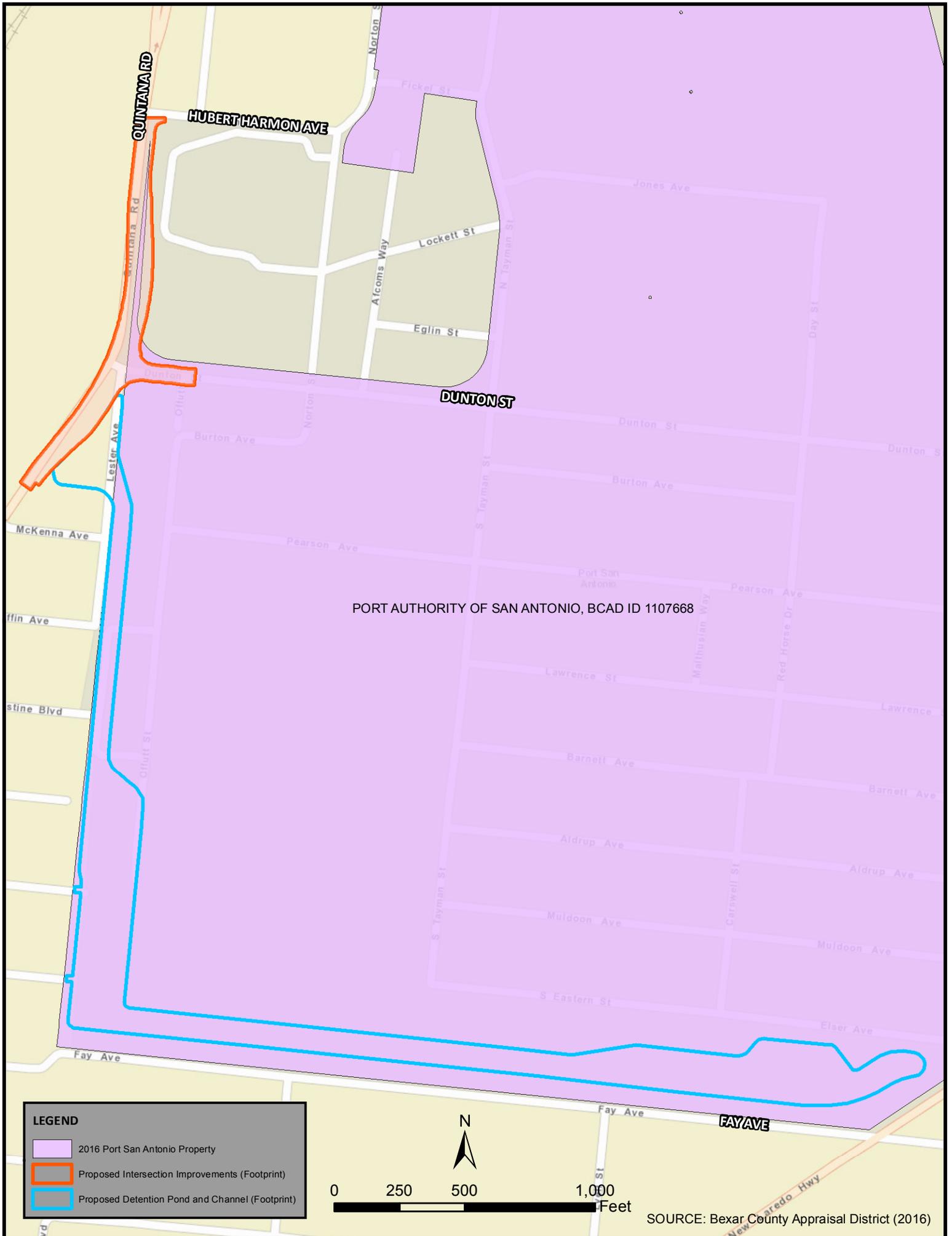
Owner Identification #: 299691

Name: PORT AUTHORITY OF SAN ANTONIO
Exemptions: EX-XV
DBA: LEASEHOLD PORT SAN ANTONIO-EAST PORT AUTHORITY



Bexar CAD Map Search

This product is for informational purposes only and may not have been prepared for or be suitable for legal, engineering, or surveying purposes. It does not represent an on-the-ground survey and represents only the approximate relative location of property boundaries. The Bexar County Appraisal District expressly disclaims any and all liability in connection herewith.



PORT AUTHORITY OF SAN ANTONIO, BCAD ID 1107668

LEGEND

- 2016 Port San Antonio Property
- Proposed Intersection Improvements (Footprint)
- Proposed Detention Pond and Channel (Footprint)



SOURCE: Bexar County Appraisal District (2016)