



Documentation of Public Meeting

Project Location

Bexar County

Principal Arterial (PA) 1502
1502-03-003

Project Limits

Wurzbach Parkway from Lockhill-Selma Road to NW Military Highway

Meeting Location

Barshop Jewish Community Center Auditorium
12500 NW Military Drive, San Antonio, Texas 78231

Meeting Date and Time

Wednesday, August 30, 2017 from 6:00 to 8:00 p.m.

Translation Services

Spanish

Presenters

Not Applicable

Elected Officials in Attendance

Not Applicable

Total Number of Attendees (approx.)

170 (excluding project staff)

Total Number of Commenters

153

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APPENDIX A

COMMENTS MATRIX



PA (Principal Arterial) 1502, from Lockhill-Selma Road to NW Military Highway Open House #2 (8-30-17) Comments

Committer Number	Committer Name	Date Received	Source	Comment Topic	Response
1	Jack and Dottie Nichols	8/30/17	Comment Form	Option #1 - Best long term strategic solution.	Comment noted.
2	Susan Landreth	8/30/17	Comment Form	Option #1 is preferred option. Provides best improvement for traffic flow.	Comment noted.
3	Maria Garcia	8/30/17	Comment Form	I like Option #1.	Comment noted.
4	Heather McNeal	8/30/17	Comment Form	I like the SPUI option, but I see the appeal in the SPUI with frontage roads.	Comment noted.
5	Sharon Saxer	8/30/17	Comment Form	5a. Prefer Option #1 because of the stop light on NW Military at Summerfield.	Comment noted.
				5b. Option #2 is good if we get the light on NW Military for the Summerfield exit. Summerfield must have a safer way to exit our neighborhood.	The Texas Manual of Uniform Traffic Control Devices (TxMUTCD) provides conditions for determining when a traffic signal can be installed at an intersection. The conditions are known as "warrants" and are based on a number of specific factors including travel speeds, frequency of crashes of the type that would be corrected by a signal, and traffic volumes on the major roadway and side street throughout the day and during the peak periods. An engineering study, known as a Signal Warrant Study, must be completed to determine if the intersection needs a traffic signal. A number of warrants must be met before public agencies can approve the installation of a traffic signal. This process conforms to the requirements of the federal and Texas MUTCD and is in place to maintain safety at intersections. While traffic signals provide safety benefits by reducing certain types of crashes, they also increase other types of crashes and increase delays. After implementation of a project at Wurzbach Parkway and NW Military, TxDOT and the city of San Antonio can conduct a follow-up study on the intersection to evaluate how it operates after the proposed project is complete. At that time, if improvements are needed, they can then be addressed as warranted.



PA (Principal Arterial) 1502, from Lockhill-Selma Road to NW Military Highway Open House #2 (8-30-17) Comments

6	Robert Wilkes	8/30/17	Comment Form	Need traffic signals at NW Military and Alon Loop, and at NW Military and Fairfield Boulevard. Option #1 is my preference.	Please see response to comment #5b.
7	Jean Hackett	8/30/17	Comment Form	7a. Please complete the land bridge before starting this project, as that would be less disruptive to wildlife.	The project team will ensure coordination occurs with adjacent projects so that construction activities minimize disruptions to the maximum extent possible. The land bridge project is conducting an analysis of traffic operations to evaluate construction phasing to minimize impacts.
				7b. The traffic signals in Option #1 are needed regardless of which plan is chosen.	The traffic signals were only added in Option #1 because this option does not have continuous frontage roads. Traffic would have to divert to Alon Loop or Fairfield Bend. All other options will keep traffic patterns the way they are today.
8	Sue Starkus	8/30/17	Comment Form	I like Option #1. With the traffic signals added at the neighborhoods, it gives everyone the ability to get out of their neighborhoods. This is by far the best option.	Comment noted.
9	John Starkus	8/30/17	Comment Form	Option #1 SPUI by far works best to me. This gives much better throughput than the other options. Less idling of cars is better for the environment.	Comment noted.
10	James Carney	8/30/17	Comment Form	Option #1 is the best option. The problem is with Alon, Inverness, and Summerfield. They are, for the most part, against this. We moved here when NW Military was two lanes. We knew it would grow out here and we need to have better traffic flow. People moved to Inverness and Alon when it was a traffic nightmare when they built it. They are trying to keep improvements from happening that would benefit people who have lived here for forty years.	Comment noted.
11	Evelyn Soo	8/30/17	Comment Form	I like Option #1.	Comment noted.
12	Eileen Vram	8/30/17	Comment Form	12a. I endorse Option #1.	Comment noted.
				12b. I would also like to have a traffic light on NW Military at the entrance to Woods of Alon and also servicing the entrance to Hardberger Park.	Please see response to comment #5b and 7b.
13	Charlie Naiser	8/30/17	Comment Form	Option #1 would be preferred if there was an option to go eastbound on Wurzbach from Estates of Alon. There is sufficient room to still allow eastbound access and there are natural breaks, due to the light at Lockhill, to merge into eastbound traffic.	The option described here was presented to the public as Option #2, containing an SPUI geometry while providing lanes for eastbound and westbound traffic to go through the signal above the underpass.



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14	I. Santiago	8/30/17	Transcript	14a. I would like to request a pedestrian overpass on Military Highway right by Hardberger Park. I don't think that we need a light there. Everybody's been talking about getting a light there. I think if we get a pedestrian overpass there that's enough.	A pedestrian bridge crossing NW Military at Hardberger Park is outside the limits of the project area. See comment #5b for a discussion on the possibility of a future light.
				14b. And if I have to go by what is the favorite for me, then Option #2, if we're going to get it – we have the feeling that they already have the money allocated for this and that they're going to do it. So if they're going to do it, then give us the opportunity to go straight on Wurzbach Parkway or go right or left from Wurzbach Parkway. So I think Option #2 would be the best one.	A decision has not be made as to which option will move forward for construction. Your comments will be taken into consideration in the selection process.
15	James Watts	8/30/17	Comment Form	15a. Option #1 is the best option long-term.	Comment noted.
				15b. However, I personally like Option #4 and #5.	Comment noted.
				15c. I would like the city or TxDOT to consider a traffic light at Inverness Boulevard regardless of the option selected.	The intersection of Inverness Boulevard at Wurzbach Parkway is a T-intersection with a stop sign for the exiting Inverness traffic. Traffic volumes at this location do not warrant installation of a traffic signal. See response to comment #5b for a discussion on signal warrant studies.
16	Carmen Gonzalez	8/30/17	Comment Form	16a. I like Option #1. It fixes the problem once and for all.	Comment noted.
				16b. My concern is getting out of the subdivision onto NW Military. This is going to be bad because of all the additional traffic created by the project. I propose that the traffic light on NW Military be moved to the Woods of Alon and Hardberger Park entrance.	See response to comment #5b.
17	Brian Smith	8/30/17	Comment Form	Go with Option #1. Rip the band-aid off. Make sure I can access Wurzbach Parkway during construction.	Comment noted. As the project development process progresses and a preferred alternative is selected, more detailed information will be developed regarding construction methods, phasing and scheduling.
18	Chris Corbett	8/30/17	Comment Form	18a. I prefer Option #1 for several reasons. It allows the most traffic through the intersection, allowing for the most growth. Exiting from Summerfield onto Wurzbach or turning left onto NW Military from Fairfield Bend is currently very dangerous.	Comment noted.
				18b. Option #2 with a light at Fairfield Bend would be my second choice.	Comment noted. See response to comment #5b.
19	Bill Hill	8/30/17	Comment Form	I strongly support Option #1.	Comment noted.
20	June Kachtik	8/30/17	Comment Form	Option #1 seems the most efficient for the cost. While it is the most expensive, it seems to best meet the needs for the longest period of time. The last options may be "cheap," but they definitely do not address the problems we are experiencing.	Comment noted.



PA (Principal Arterial) 1502, from Lockhill-Selma Road to NW Military Highway
Open House #2 (8-30-17) Comments

21	Patsy Kuentz	8/30/17	Comment Form	21a. I prefer Option #1. It keeps the traffic moving through the intersection faster.	Comment noted.
				21b. I would like to see another option, however, that incorporates changing some lanes at AM rush hour and the others during PM rush hour.	Changing lanes based on directional traffic patterns is called dynamic lane assignment. It is an effective way of addressing significant changes in traffic volumes associated with each peak hour. Usually the differences in volumes are related to direction of travel. Lane assignments can be changed to add capacity to the peak direction during the peak hour only. This type of improvement is a short-term fix and would not be considered a long-term option for this project. Based on the current conditions at the intersection, it is not likely that a dynamic lane assignment improvement would provide enough additional capacity to make a significant reduction in delay, and if it did, it would not last more than 1 to 3 years. The improvements developed and evaluated for this project were targeting a life cycle up to year 2040, wherever possible.
				21c. Also, I'd like to see mitigation for trees that are removed in the process.	Every attempt will be made to preserve green space in the corridor while meeting the need and purpose of the project.
22	Alan Kuentz	8/30/17	Comment Form	Option #1 is ideal for me. I get through the intersection faster with just two lights. It provides easier access from the Alon Center onto Wurzbach and then onto NW Military.	Comment noted.
23	N/A	8/30/17	Comment Form	Option #1 will be long to do, but best prepares for future growth.	Comment noted.
24	Jerry McKinney	8/30/17	Comment Form	Prefer Option #1.	Comment noted.
25	Valerie Garrison	8/30/17	Comment Form	25a. Why can't the traffic light at Alon Loop and Hardberger Park be an option for Options #2, #3, #4 and #5?	See response to comment #5b.
				25b. Please create open space at the median outside Alon Loop to gain access to the tunnel.	This was evaluated during the schematic development phase. The geometry of the underpass and the location of the lane split will not allow for access from Alon Loop to eastbound Wurzbach Parkway, per TxDOT standard design criteria. See also response to comment #13.
				25c. Is there any way to create a short light at Alon Loop to gain access to the tunnel? This creates a less dangerous exit from neighborhood.	See response to comment #5b. While a formal signal warrant study has not been performed, the traffic volumes exiting Estates of Alon do not fulfill the criteria for installation of a traffic signal.



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Open House #2 (8-30-17) Comments

26	Harry and Victoria Caylor	9/5/17	Email	<p>We are residents of Woods of Alon. Our preference is your recommendation of the SPUI (Option #1). With this configuration, we would also get a traffic light at Alon Loop at NW Military Highway. Some members of the Estates of Alon have their "panties in a wad" because they think they are going to lose their neighborhood access off of Wurzbach, which is not the case, based on your visual presentation. They were probably whipped into a frenzy by some inaccurate information. Go get 'em. Build it right. Let me know if I need to do something else to get my vote recorded.</p>	Comment noted.
				<p>27a. Basis of Project: Purpose of project was to improve traffic flow and congestion at the Wurzbach/NW Military intersection only. The problem consists almost entirely of rush hour traffic both in the morning and the late afternoon. (A) State jurisdiction: Wurzbach Parkway up to Lockhill-Selma intersection (B) City of San Antonio jurisdiction: Lockhill-Selma intersection road and Wurzbach west of the intersection. Previous requested efforts: (A) Requests had been made to the city of San Antonio through our Councilman, now Mayor, to have the city Public Works Department create plans to improve the traffic lights and other work at both the Vance Jackson/Wurzbach and Lockhill/Wurzbach intersections. I understand that some plans were made. (B) Requests were made to TxDOT to improve the NW Military/Wurzbach intersection. State proposals for the NW Military: There are six options presented. All options reflect much effort by TxDOT for the most part, with several suggestions. Option #1 is best in some respects because it helps regulate the traffic flow down to Lockhill-Selma by the use of signal lights at NW Military, which helps avoid westbound congestion problems, by intermittent flow of traffic (especially with the traffic created with the third lane problem due to the horrible third lane proposal) down to Lockhill-Selma Road. NOTE: Signal lights on Wurzbach could also be included in other Options.</p>	Comment noted. See response to comment #5b regarding signal options on NW Military.



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27	William Chenault	9/8/17	Letter	<p>27b. Option #4 (the partial below-grade separation) may be problematic in that there appears to be no drainage studies. I am not saying that there is a problem, but TxDOT has made a big mistake before on 281 just south of Basse, which has flooded and become impassable about every five or so years. There is an even bigger problem on the I-35 below-grade under San Pedro and McCullough about every two or so years. There are others. There should be a drainage study, just in case.</p>	<p>A drainage study will be conducted as part of the project development.</p>
				<p>27c. All options appear to include a third lane headed west from NW Military past Lockhill-Selma to the bridge at Lockhill-Selma.</p>	<p>This is correct. The added lane in each direction increases the amount of traffic that can flow through the project limits.</p>
				<p>27d. The basic complaint concerns the questionable inclusion of the third westbound lane past the Inverness exit, and especially past Lockhill-Selma up to the bridge just before the Elm Creek entrance and Whisper Bow entrance. Background: As Whispering Oaks Neighborhood Association President, I worked with other neighborhood representatives, Councilman Thornton, and others concerning the design and development of the Wurzbach Parkway while it was being designed. We specifically addressed this very issue of traffic congestion for this portion of the Parkway at that time. In addition, I travel this section almost every day at various times of the day and am familiar with this section. As part of those discussions, I requested, and it was agreed, to add a third eastbound lane, which we felt was necessary to help dilute and speed up eastbound traffic. It was also discussed and agreed, at that time, that a third lane on the westbound traffic should not be included, because it would concentrate and further slow the traffic causing congestions. Since those discussions and as part of a separate project, Lockhill-Selma and its intersection with Wurzbach was expanded, including a westbound turn lane from southbound Lockhill-Selma onto westbound Wurzbach, further adding additional traffic onto Wurzbach. It should be noted that Whisper Valley also enters onto Wurzbach at or very near this same section location. The result means that the original two lanes on Wurzbach, plus the one southbound turn lane from Lockhill-Selma onto Wurzbach, plus another turn lane from Whisper Valley onto westbound Wurzbach traffic adds up to an existing four lanes, which turn into only two westbound lanes of traffic - resulting in all the congestion at that point. Now, TxDOT proposes to add a third lane past</p>	<p>The third lane will increase the flow of traffic through the signalized intersection of Lockhill-Selma. We have extended the third lane to the entrance of Elm Creek as a right-only lane. This gives the Elm Creek traffic a way to go around the stacked up traffic to enter their subdivision. The third lane also gives traffic more weave length so they can merge back into two lanes to go westbound. Having three lanes will give Whispering Oaks more gaps during non-peak periods to get out faster. The third lane begins at Whispering Oaks, which will make it easier to go eastbound on Wurzbach.</p>



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				27e. It is requested that (A) there be, at least, a brief drainage report for any section of Wurzbach which is below-grade level.	Please see response to comment #27b.
				27f. It is requested that (B) the proposed third lane be deleted in its entirety after the exit from Inverness.	Please see response to comment #27d.
				27g. It is requested that (C) any third lane and any other related work, or other third-lane extensions west of Lockhill-Selma be completely deleted and removed from the proposal.	Please see response to comment #27d.
				27h. It is requested that (D) traffic signals be included at all intersections in the project area.	Please see response to comment #5b.
				27i. It is requested that (E) the city of San Antonio handle the improvement at the Lockhill-Selma/Wurzbach intersection.	TxDOT is working in coordination with the city of San Antonio for the development and construction of this project.
28	Barry Spigel	8/30/17	Comment Form	Preference: Option #2. Through lane on access road is a great idea (should have proposed that initially). Interchange allows good movement when turning left/right onto NW Military.	Comment noted.
				29a. Options #2 and #3 are the best compromise to moving traffic and keeping adequate access to and from neighborhoods.	Comment noted.



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 Open House #2 (8-30-17) Comments

29	Irwin Fletcher	8/30/17	Comment Form	<p>29b. Option #1 is horrible and does not consider the detrimental impacts to existing neighborhood access.</p>	<p>The intersection currently experiences significant congestion and delays, and is operating at a failing LOS F during the PM peak hour. This is projected to worsen as volumes on Wurzbach Parkway are anticipated to increase by 127% by year 2040. The most effective way of reducing delays at the intersection is through grade separation. Removing the Wurzbach through traffic from the signal greatly improves the performance. Eastbound Wurzbach traffic will travel a long distance, unimpeded on the main lanes, until the signalized intersection with Weidner Road. Westbound Wurzbach traffic will travel under NW Military, bypassing the signal. By removing the through movements at NW Military, the signal requires fewer phases to complete a full cycle. That time saved is distributed to the other movements. These changes, in addition to the added efficiency of concurrent left turns from northbound/southbound NW Military, provide the most efficient configuration. While queues at Lockhill-Selma may be longer, there is significant reduction in overall travel time for the westbound direction. Because Option #1 removes the through movement that Inverness and Summerfield use to enter and exit their neighborhoods from Wurzbach Parkway, Option #1 includes the proposed signal, which is intended to make it easier for that traffic to enter/exit on NW Military at Turnberry Way/Fairfield Bend. No other option proposes to change access; therefore, no other option proposes to add a signal at that location.</p>
				<p>29c. Without more significant improvements on Wurzbach from Lockhill-Selma to I-10, this all may be a waste of time and money.</p>	<p>Our traffic modeling shows considerable savings in delays even without any improvements to Wurzbach Road from Lockhill-Selma to I-10. TxDOT and the city of San Antonio are aware of the need to improve Wurzbach from Lockhill-Selma to I-10. Those improvements and efforts are outside the scope of this project.</p>
30	Richard Senelick	8/30/17	Comment Form	<p>30a. I prefer Option #2, but you need to add traffic signals at Turnberry. Seems if you can do it with Option #1, you could also do it with Option #2.</p>	<p>Please see response to comment #5b. Also, since Option #2 has frontage roads which keeps all existing traffic patterns as they are today, there should be no need to add traffic signals at Turnberry because of increased traffic using that exit.</p>
				<p>30b. My second choice is Option #5.</p>	<p>Comment noted.</p>



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				30c. I do not like Option #1 as it eliminates access to Inverness from the east.	Comment noted.
31	Gupinath Akalkotkar	8/30/17	Comment Form	I prefer Option #2. It is a good compromise for local and through traffic.	Comment noted.
32	Manjiri Akalkotkar	8/30/17	Comment Form	32a. As a resident of Summerfield using the exit on Wurzbach Parkway going to I-10 for my work commute, I prefer Option #2 - modified SPUI with frontage roads.	Comment noted.
				32b. We need signals at the entrance and exit at NW Military from Fairfield Bend.	Please see response to comment #5b.
33	Anthony Hutfles	8/30/17	Comment Form	33a. I live off of Huebner Road. I go through this intersection multiple times during the week. My choice would be Option #2, SPUI with frontage roads. It appears to lower wait times at the lights. Something needs to be done since traffic is not decreasing.	Comment noted.
				33b. Doing Options #3 - #6 appears to accomplish little to help traffic flow in the future.	Comment noted.
34	Yolanda Patti	8/30/17	Comment Form	I am for doing the underpass, whichever option that includes. Option #2 is my preference.	Comment noted; Option #2 includes an underpass.
35	Stacie Schroeder	8/30/17	Comment Form	35a. Thank you for presenting us with options. I like Option #2 the best, the modified SPUI with frontage roads. This allows immediate residents to enter and exit their subdivisions like we do now.	Comment noted.
				35b. I would like to see the eastbound and westbound U-turns start farther away from the lights though.	The proposed U-turn lanes are longer than the existing lanes. With the reduced queuing for other modifications, the length should be adequate as designed.
				35c. I am happy with Option #2 and Option #3, but would prefer Option #2.	Comment noted.
36	Jacques Braha	8/30/17	Comment Form	100% for Option #2, with modified SPUI. Best solution for the long-term.	Comment noted.
37	Michele Haussmann	8/30/17	Comment Form	Please build Option #2, with modified SPUI. The inclusion of eastbound and westbound frontage roads is extremely important. I live in Inverness and represent the Alon Town Centre.	Comment noted.
38	Scott Bishop	8/30/17	Comment Form	38a. I think Option #2 is the best.	Comment noted.
				38b. Option #3 is good because it still allows for at-grade through movements along Wurzbach. This allows for transit service along Wurzbach and provides stops for the neighborhoods and the Alon Shopping Centre.	Comment noted.



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39	Bill Yates	8/30/17	Comment Form	I prefer Option #2, as I like having access across NW Military on side lanes. Also, the widened intersection equals clear turn lanes not crossing. I still like the tunnel under NW Military, but am concerned about the traffic increase at Lockhill-Selma at Wurzbach.	Comment noted.
40	A. Santiago	8/30/17	Transcript	40a. In Option #2, I want to see if they can add a traffic light at the intersection of NW Military and Alon Loop.	Please see response to comment #30a.
				40b. Also, I want to see the possibility of a pedestrian overpass over NW Military, joining one side with the entrance of Phil Hardberger Park.	A pedestrian overpass to the entrance of Phil Hardberger Park is outside the scope of this project.
				40c. And then for Option #5, I'd also have the same petitions that are in Option #2: the same traffic light and also a pedestrian overpass.	Comment noted. Please see response to comment #40b regarding a pedestrian overpass.
41	Nicole Boarnet	8/30/17	Comment Form	41a. In my opinion, Option #1 is terrible. Alon is way too busy to not allow a left turn out. The traffic signals won't help. The parking lot already backs up.	Comment noted.
				41b. Option #3 - Does this option with too many lights really save time?	All options save time compared to the "No Build" option.
				41c. Options #4 and #5 - I am worried that these options are confusing and could cause accidents. This area is also a high pedestrian area.	Safety considerations were analyzed for all options and for all modes of travel.
				41d. Either do nothing (Option #6) or I like Option #2.	Comment noted.
42	Jorge Senekes	8/30/17	Comment Form	I like Option #3.	Comment noted.
43	Jorge Sanchez	8/30/17	Comment Form	As a Summerfield resident, I favor Option #3 the most. Doing nothing to Wurzbach Parkway would be a mistake due to the ever increasing traffic on the street. Options #1 and #2 are respectable options, but I strongly recommend Option #3.	Comment noted.
44	Stephen Lindewood	8/30/17	Comment Form	Option #3.	Comment noted.
45	Harold Dillashaw III	8/30/17	Comment Form	Option #3 is what's needed. It should have been built like this originally. A young engineer at a public showing of the Wurzbach Parkway said, "A bridge for NW Military over Wurzbach Parkway couldn't be done because of Lockhill-Selma." Yet now it can. Sure.	An overpass was analyzed and the design did not fit within the existing right of way.
46	Dave Adams	8/30/17	Comment Form	Option #4 is best.	Comment noted.
47	Gerald Cherico	8/30/17	Comment Form	47a. I strongly urge going with Option #5 - at-grade improvements option. Constructing a tunnel is much too disruptive and expensive. We need access to the turnaround at NW Military so I can safely exit our property and get to my job at USAA.	Comment noted.



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				47b. We also require a traffic light at Alon Crossing at NW Military to enter and exit safely and to access Hardberger Park.	Please see response to comment #5b.
48	Elizabeth Cherico	8/30/17	Comment Form	We strongly prefer Option #5 - at-grade improvements. A tunnel will tremendously degrade the value of our property and cause havoc for our residents.	Comment noted.
49	Lawrence Fagen	8/30/17	Comment Form	Option #5. No tunnel, too much disruption, dirt, etc.	Comment noted.
50	Melissa Dowell	8/30/17	Comment Form	I prefer Option #5 due to cost and environmental impact. It is positive for Alon Estates access.	Comment noted.
51	TJ Dowell	8/30/17	Comment Form	I prefer Option #5 due to construction cost and environmental impacts. It has positive access for Alon Estates.	Comment noted.
52	Chris Amaker	8/30/17	Comment Form	52a. Option #5 is the right one for this project if you must spend the money. I prefer you do nothing. You are only improving one block if changes are made.	Comment noted.
				52b. This is not a Top 100 roads project in Texas.	Comment noted.
				52c. Widening the lanes reduces the natural trees and beauty of the street.	Widening was kept to the minimum needed in order to reduce the number of trees to be removed.
				52d. This will create more traffic at our home gate entrances off of Wurzbach.	Traffic is expect to increase over time with or without this project being built. Building any option should alleviate congestion within the project limits.
				52e. Lastly, the citizens should meet the companies that will profit off of this project. Who are they related to in the city?	We are only in the planning stage. When the project is ready for construction, an open and transparent bid process will be followed, as are all TxDOT projects.
53	Cindy Wilks	8/30/17	Comment Form	I vote for Option #5 - Improvements with tree retention and the least amount of time and money.	Comment noted.
54	Carlos Ortiz	8/30/17	Comment Form	54a. I disapprove of Options #1, #2, #3 and #4. I favor Option #5 because it saves the trees and makes movement of traffic a little easier.	Comment noted.
				54b. Lower the speed limit on Wurzbach to 55 mph.	A speed zone study is required to determine if the posted speed limit should be reduced based on crash history, roadway design, 85th percentile speeds, traffic volumes, sight distance and other factors. Speed reductions and other safety measure will be considered during the plan design stage.
55	Emma Pledger	8/30/17	Comment Form	55a. I oppose the construction, but of the options provided, Option #5 is my family's preference. It is the least obstructive, keeps the median with trees, and widens the lanes to help improve traffic routes.	Comment noted.



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				55b. Consideration should also be made to timing of traffic lights, not currently conducive to current traffic patterns.	The city of San Antonio maintains the traffic signal timings and are working in cooperation with TxDOT on this project.
56	Jeffrey Hoefre	8/30/17	Comment Form	56a. I prefer Option #5.	Comment noted.
				56b. I recommend traffic lights at Alon Loop at NW Military intersection, entrance of Hardberger Park, and Inverness and Summerfield/NW Military intersections.	Please see response to comment #5b.
57	John Trollinger	8/30/17	Comment Form	57a. I prefer Option #5. It is the least intrusive, but still addresses the issue while spending the least amount of money.	Comment noted.
				57b. I would like to see a light at Alon Loop and NW Military and a light at Inverness and Summerfield.	Please see response to comment #5b.
58	Maria Luna and Alejandro Morales	9/14/17	Email	58a. I live in Alon Estates. My neighborhood and my house will be directly impacted by the Wurzbach Parkway proposed changes. I have a strong opposition to all four graded designs. These are the ones whereby a tunnel would be constructed under NW Military. My house backs up directly to Wurzbach so these changes would be directly behind my house.	Comment noted.
				58b. I think the at-grade design will be the best option for my neighborhood and my home. The existing sidewalk and green space buffer will remain as-is. There would be less noise with this design than the others.	Comment noted.
				58c. However, if this one is done there should be an addition of a light at our NW Military gate entrance across from the Hardberger Park entrance.	Please see response to comment #5b.
59	Chris Cummings	8/30/17	Comment Form	Alon Estates residents prefer Option #5 or #6 to allow access through NW Military without waiting. I would like to go through NW Military without waiting.	Comment noted.
60	Matthew Patti	8/30/17	Comment Form	60a. Option #5 is my first choice.	Comment noted.
				60b. Option #2 is my second choice. It is okay with incremental improvements at \$7.5 million, but if other options are \$20 million, then I go almost all the way with Option #2. I like throughway on Wurzbach going east on frontage.	Comment noted.
61	Ban and Sukanya Lean	8/30/17	Comment Form	61a. We strongly support Option #5.	Comment noted.
				61b. Our second choice is Option #1.	Comment noted.
				61c. Thank you for presenting us more options. We need a light on NW Military before the turn at Alon Estates gate.	Please see response to comment #5b.



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62	Timothy Farris	8/30/17	Comment Form	62a. Best Options: #5 and #6. Worst Options: #1, #2, #3 and #4. This project, contrary to poor study designs that showed only mild improvements, would severely impact the surrounding neighborhoods at this time, in very predictable ways.	Comment noted.
				62b. Lower property values would be incurred by several residents.	Comment noted.
				62c. Excessive noise would diminish child health and well-being.	Once a preferred build alternative is selected, environmental studies will determine if there are any noise impacts, consistent with TxDOT/FHWA regulations. If noise impacts are present, then TxDOT will determine if abatement is reasonable and feasible for incorporation into the proposed project; including an assessment of constructability.
				62d. Excessive construction hazard and long time for project completion would not yield expected results.	Every option has it's own construction timetable. The at-grade option has the shortest construction timeframe of 8 to 10 months, which is not very long for a project of this size.
63	Lori Boies	8/30/17	Comment Form	63a. Ranking from best to worse: Options #5, #6, #2, #1, #3 and #4.	Comment noted.
				63b. Adding a tunnel will cost \$20+ million or more and years of construction to save less than a minute. Add a lane and time the lights; it could save our money and construction headache without pushing the issue to Lockhill-Selma.	Comment noted.
				63c. Other Wurzbach projects (Fredericksburg @ I-10) are much worse. 1604 is a nightmare! These projects (Fredericksburg, I-10, all of 1604) would provide better relief for our community. Better yet, send that \$20 million to Houston to recover from Hurricane Harvey.	Comment noted.
				63d. The tunnel can always come later if the most cost effective, Lockhill-Selma/tree-saving (to some extent) option does not work.	Comment noted.
				64a. I'm a homeowner at the Estates of Alon. Options #1, #2 and #3 do not benefit our neighborhood. We don't even have access to the proposed improvements. Why do we want to be bothered by all the construction?	Comment noted.



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64	Roxana De Leon Fuentes	8/30/17	Comment Form	64b. These proposals will negatively affect the value of our homes.	Comment noted.
				64c. Getting rid of the median and trees is not good for our neighborhood.	Comment noted.
				64d. If an option must be built, I prefer Option #5 or no-build Option #6.	Comment noted.
65	Joel Griffith	8/30/17	Comment Form	65a. Option #5 (at-grade improvements) is the clear best choice.	Comment noted.
				65b. Lights at Summerfield and Alon Loop should be added to address safe entry and exit to these neighborhoods and Hardberger Park. These should be considered for all options, not just Option #1.	Please see response to comment #5b.
66	Shelley Baillargeon	8/30/17	Comment Form	The only option that helps traffic and does not increase pedestrian injury and death is Option #5. This is the only option I feel is not a huge overkill by creating more problems than it solves. Option #5 makes sense, and with smart lights, it would likely solve the traffic problem.	Comment noted.
67	Adam Saucedo	8/30/17	Comment Form	Option #5 is the best option.	Comment noted.
68	Laura Saucedo	8/30/17	Comment Form	The best option is Option #5.	Comment noted.
69	Steven Sanders	8/30/17	Comment Form	69a. My property values will decrease with a stupid wall. No wall!	Comment noted.
				69b. Option #5 is the best for our area. Improve the flow of traffic and all will be better.	Comment noted.
				70a. I live in the Estates of Alon and I'm opposed to almost all of the options. I think that Options #5 and #6 are the only two viable options. Because they're just going to move traffic down from NW Military to Lockhill-Selma, it's going to back up right behind my house. I back up to Wurzbach.	Comment noted.



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70	W.H. Hawkins	8/30/17	Transcript	<p>70b. It's going to cause my property value to decrease, we're going to have noise, we're going to have smog/pollution from the cars sitting there at the light. I just feel like it's a gross waste of taxpayer money to do these \$20 million tunnels and things like that. Plus it's going to inconvenience our whole neighborhood for years just during the construction phase, and it's not going to really do that much because they haven't made any plans west of Lockhill-Selma. It's just going to bottleneck. There's no expansion plans for west of Lockhill-Selma, there's no room to expand. I believe it's just a waste of taxpayer money.</p>	Comment noted. Please see response to comment 62c regarding noise.
71	Charles Roger Macias Jr.	8/30/17	Comment Form	<p>71a. The options other than Option #5 and #6 will increase noise and create a tunnel in a residential area.</p>	Comment noted. Please see response to comment 62c regarding noise.
				<p>71b. If you increase a lane each way to Wurzbach Parkway and increase timing of lights by 45 seconds at peak hours, it will eliminate traffic congestion.</p>	Increasing the green time of one movement takes away time from another movement. By allowing time for one left turning vehicle with only one left-turn lane takes away time that was used by 3 vehicles from the through lanes because there are 3 through lanes. The SPUI eliminates entire movements for the intersection. The time from the removed movements can now be given to the other movements which increases capacity. A revision made after the meeting is to have the At-Grade option revised to add an additional dedicated left-turn lane in each direction and allow the left turns to turn concurrently, thus reducing the left time or doubling it in effect, as you suggest.
				<p>71c. Options #1, #2, #3 and #4 will decrease my property value considerably.</p>	Comment noted.
72	David Mansour	8/30/17	Comment Form	Option #5. All other options are horrible and will cause major problems.	Comment noted.
73	Ratafelcher Borra	8/30/17	Comment Form	Save trees. Option #5 is my choice. Don't waste public money.	Comment noted.
74	Raja Kandi Kayala	8/30/17	Comment Form	Option #5 is the best and there is no real advantage for all other options. Don't waste public money. Use for a good reason. Save trees and have healthy life.	Comment noted.
75	Sasikanth Nallagatla / Swapna Ghattamaneni	8/30/17	Comment Form	Option #5 is the best for my community. I strongly recommend it.	Comment noted.



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76	James Lutz	8/31/17	Email	76a. I prefer Option #5. The cost vs. benefit doesn't balance. Until the city is willing to buy the necessary ROW to continue the improvement to I-10, this project gives the east-west commuter little benefit.	Please see response to comment #29c.
				76b. Additionally, what will happen to the traffic when improvements to Loop 1604 are made? Will it relieve the traffic?	Improvements on Loop 1604 could relieve traffic in the short term but the benefits are uncertain considering the number of variables involved. Also, traffic is still expected to increase by 127% by 2040 so taking no direct action is not recommended.
77	C. Von Bertrab	8/30/17	Transcript	I would like Option #5 because I really do not believe that the others will solve the problem completely and this is a much more cost-efficient project. It's not going to take as long and it won't impact the trees and the environment as much. I really think this is the simplest solution to the problem, and I really reject the ones with the underpass because the cars will reach Lockhill-Selma at a very high speed, and I'm very concerned about that.	Comment noted.
78	H. Cerda	8/30/17	Transcript	I live in Alon Estates, and when we heard about this project we were a little excited that we were going to see some improvements on the traffic as we try to exit from our resident area onto Wurzbach Parkway. But then we learned that it was going to stop at Lockhill-Selma, so then this traffic is just going to be pushed from Lockhill-Selma all the way to I-10. What we wanted to say is we don't really want any of these improvements, but we were told by TxDOT we're going to do something, we have to do something. So we want to make it known that we want the at-grade improvement because we'll still keep some natural trees there, they'll be reduced to one kind of lane or one median. We would prefer that instead of a tunnel which would get flooded when we have good rain here in San Antonio, or if there is an accident, traffic is still going to be backed up and they're going to wind up going on the access road, which will still create problems for us. So I just want to state for the record as a resident of Alon Estates that, if there's going to be an improvement, we'd rather have the at-grade improvement, which would be \$7.5 million instead of spending \$28.5 million.	Comment noted.
				79a. I would just like to say to TxDOT, as a resident of the Estates of Alon, which is a neighborhood that is directly next to the proposed construction site, that I am very much against any of these options with the exception of Option #5, which is the at-grade improvement option.	Comment noted.



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79	David Mansour	8/30/17	Transcript	<p>79b. I feel that TxDOT should take into consideration opinions from our neighborhood very much so because we are directly impacted. We are next to the area, we live there, we walk there, we bike there and we have to see and listen and drive in this area every day for the rest of our lives because we live there.</p>	<p>This is the second public meeting in addition to other Focus Group meetings and possible additional public meetings. TxDOT thanks you for your comments and is interested in hearing your opinions and suggestions.</p>
				<p>79c. This proposed project in its infancy was to build a tunnel under NW Military. We feel that any option that incorporates a tunnel is a horrible option because all it does is increase the flow of traffic and causes traffic to back up at Lockhill-Selma. TxDOT has admitted that there are no plans to improve beyond Lockhill-Selma because no further improvements can be made, so why spend this huge amount of money to build a project that all it's going to do is push traffic from one intersection to another?</p>	<p>TxDOT and the city of San Antonio are aware of the need to improve Wurzbach from Lockhill-Selma to I-10. Those improvements and efforts are outside the scope of this project.</p>
				<p>79d. Furthermore and more importantly, we have been told by numerous sources that this will drastically affect property values of the neighborhood we live in; and I think this is evident in the fact that nowhere in San Antonio is there a tunnel like this in a residential neighborhood.</p>	<p>Comment noted.</p>
				<p>79e. There certainly are examples of this and, in all of the examples we can find, none of them are ever in a residential area. These SPUI options are usually done in commercial areas surrounded by businesses and commerce, not neighborhoods.</p>	<p>Typically, SPUI are replacements for tight diamond intersections on highway and parkways such as the current intersection.</p>
				<p>79f. I think the environmental aspect of the project, where you're getting rid of green space for paved roads, again, is going to affect drainage which is going to affect the way we live. Also, I would implore TxDOT to pay attention to city initiatives, which is to place a greater emphasis on the walkability and bike-ability, and the extension of all of these lanes diminishes the ability for people to walk and bike to the various locations in the area; such as the grocery store, church, the park, Starbucks coffee - all of these are within walking distance and people can be witnessed using these walking trails on a regular and daily basis.</p>	<p>Please see response to comments #21c and #27b.</p>



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				79g. Lastly, there are residents who live up against Wurzbach who will have to hear the constant flow of traffic on a regular basis. I'm the president of the Estates of Alon Homeowners Association and, as a representative of my neighborhood, I would just like to formally put down that our neighborhood is strongly opposed to any graded options, any tunnel options. We are for Option #5 which is the at-grade improvements.	Comment noted.
80	B. Sukanya Lean	8/30/17	Transcript	80a. I am a resident of Alon Estates. I've been here about three years now (two, two and a half years). I like Option #5 to build an intersection between Northwest and Wurzbach Parkway because, No. 1, low cost construction, low impact on construction;	Comment noted.
				80b. No. 2, retain some trees and not kill all the trees to widen up the lane; and	Option #5 as you noted provides space to minimize tree impacts.
				80c. No. 3, is low cost on construction, I think the least cost, because it costs less than, you know, making an overpass or tunnel.	Correct.
				80d. No. 4, we need some light or sign or signal before you get into the Northwest Military gate, because it's hard and sometimes I just pass it, you go through it and miss the turn all the time, you know, so we need something warning before you get into the gate on Northwest Military. That's all of my concerns. Thank you very much for the presentation open house here today. That's very helpful. Thank you.	Please see response to comment #5b.
				81a. I live in Alon Estates. I am against this SPUI with signals option because it limits my ability to exit out of my neighborhood and continue straight on Wurzbach to 281. The only option I would have is to go through another neighborhood called Alon Woods and go through four lanes to cross over, and then eventually onto Wurzbach Parkway to 281.	Comment noted.
				81b. Another reason I don't want this construction is because it's going to be at least two years to build, so I'm going to have all of this construction behind my neighborhood, limiting my access again to get out.	Comment noted.



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81	Victoria Vela	8/30/17	Transcript	81c. Also, with this construction, I would see an increase in traffic. That would only help increase traffic up to the Lockhill-Selma intersection, which eventually will be backed up so we'll have more congestion and noise. I live closer to the intersection of Lockhill-Selma and Wurzbach Parkway, so I would hear more noise and interruption in my living.	Comment noted.
				81d. The option I opt for, if there was anything to be done, would be the at-grade improvements, where they would build more lanes and not do the tunnel option. It would be cheaper, there would be less construction, and less tree damage.	Comment noted.
				81e. I think the money that they want to use for this improvement should be used at 1604 or 410 to expand those highways so people would have other ways or other means to get to work besides going through my neighborhood and increasing traffic.	Comment noted.
				81f. I think it definitely benefits other people getting through there, but not me. During those peak hours, I really don't travel, and I just see the traffic of other people in San Antonio trying to get through, so I'm definitely against the SPUI with signals.	Comment noted.
82	J. Grissith	8/30/17	Transcript	82a. I think the options of going with the SPUI interchange is a little bit short-sighted, given that the traffic is just going to move one block away to Lockhill-Selma and this is going to affect traffic both directions. I have grave concerns about the construction going on and the construction that will happen to build the tunnel underneath NW Military.	Comment noted.
				82b. I think the best long-term solution is to improve 1604 and to alleviate some of the traffic that is increasing the demand on Wurzbach Parkway.	All the options presented accomplish the goal of reducing congestion and delays with the project limits. TxDOT is working with others on improving Loop 1604.
				82c. I think the at-grade improvements or changing the timing of the signals is the most responsible way to approach it right now. It's the cheapest solution. They're the ones that can be the most easily corrected or changed in the future if it turns out they don't work. I just don't see the need to go to such a drastic fix right now before we actually have evidence that they're going to fix the problem.	Comment noted.



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83	J. Mansour	8/30/17	Transcript	83a. I am a resident of Alon Estates and recognize that we have a congestion issue at the intersection, and I am excited to see that there are some proposed improvements coming to the area. I am strongly opposed to any graded improvements because of the tree removal, and because there is not a comprehensive plan beyond Lockhill-Selma to address congestion issues in that section and so you're essentially moving the issue from one intersection to another.	Comment noted.
				83b. I feel from a cost benefit perspective that a better use of taxpayer dollars would be to implement the at-grade improvements and to consider some improvements at the Wurzbach and Weidner intersection which will help the I-35 to I-10 commute. It would have, I think, a bigger impact on the overall commute.	Comment noted.
				83c. I also would like to ask for consideration of a light outside of the Alon neighborhood at the NW Military exit with the at-grade improvement because accessing the U-turn will become a little more challenging with the additional two lanes.	Please see response to comment #5b.
				83d. And lastly, I am concerned about the safety of pedestrians and cyclists through the intersection.	All options took into account safety for all modes of travel.
84	S. Sanders	8/30/17	Transcript	84a. Option #5 seems to be the best option. It keeps all of the trees and it allows for the flow of traffic from Wurzbach down to Lockhill-Selma.	Comment noted.
				84b. And, unless you do the improvement all the way to Fredericksburg Road and you either go under or over NW Military, Lockhill-Selma, Vance Jackson and then I-10, and then Datapoint and whatever else is down there towards Fredericksburg, you're not going to improve the time overall. With the population increasing over the next ten to twenty years, there's just no way. Everybody still stops at Wurzbach, and will have to either go right or left, and that's just going to back up traffic all the way to NW Military and even toward Blanco Road. So, again, Option #5 seems to make the most sense.	Please see response to comment #79c.
85	A. Saucedo	8/30/17	Transcript	Option #5 is the best option that provides the least amount of disruption for the neighborhood at Alon Estates and still protects the trees and the environment and what I best like about our neighborhood.	Comment noted.



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86	R. De Leon Fuentes	8/30/17	Transcript	86a. I am a homeowner at the Estates of Alon and I disagree with Options #1, #2 and #3 of the proposed improvements because we do not benefit from these proposals. There is no access to these improvements from our neighborhood.	Comment noted.
				86b. Plus, getting rid of the median and the trees will adversely affect the value of our homes, which is what I'm most concerned about. It will negatively impact the value of the homes. That's really important to me.	Comment noted.
				86c. If anything must be built, Option #5 will be preferred for me living at the Estates of Alon.	Comment noted.
				86d. Or "No Build," because we know for a fact that they are working on 1604 in a few years; they should probably focus more on solving the problem they have on 1604 because traffic going across Wurzbach Parkway is trying to get to 281 and 1604. And, as I said, these first three options do not benefit my neighborhood, so why do we want to be bothered by all of this construction and all of this chaos if we don't even have access to it? We are the ones that live there and we don't benefit from it.	Comment noted.
				87a. So I'm basically against most of these options and, in particular, I'm surprised that Option #1 is still on the table after my community and myself, we got involved in the early stages a few months back when the first meeting was held (Open House) and we learned about the tunneling and that we wouldn't be able to exit our community to continue eastbound toward 281. We would either have to turn left to go southbound on Military or turn right to go southbound on Military. So at that point, we got involved and we made sure our voices were heard. We contacted TxDOT and they, basically, mentioned that the first option probably isn't going to happen because of all of the concerns that we expressed, and so I'm here and surprised to see it's still an option. And, if anything, that's the one that most adversely affects our community, the community of the Estates of Alon. And so, basically, all of these options, except probably #5 and definitely #6 are going to affect our community.	We have retained a SPUI option in consideration of feedback from focus groups and its greater performance. However, Option 1 as shown tonight is slightly different than the prior public meeting; it incorporates signals to mitigate access impacts.



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<p>87b. You know, our community has homes that are in the \$500-, \$600,000, \$700,000s, and additionally, Inverness, the community across from us, and they have lots that just begin at \$250,000, and you know, we're talking about property values increase -- excuse me -- decreasing, we're talking about increased noise from the increased traffic,</p>	<p>Comment noted.</p>
<p>87c. but, I mean, additionally, most of these projects are \$20 million, and they're only going to alleviate the issue for a few years after construction is done, a two- to three-year construction, so not only do we have to endure three years of construction loudness, then we're going to have to endure increased traffic, traffic noise, increased accidents that already are happening at Lockhill-Selma and Wurzbach.</p>	<p>The four grade-separated options are all over \$20 million in estimated cost and would require longer (2 to 3 years) construction periods compared with the at-grade improvements option and the no-build option. However, the four grade-separated options all last significantly longer than the non-grade-separated options (at-grade improvements and no build). The longest effective lifespan would be associated with the traditional SPUI, then the modified SPUI, followed by the diamond and then the partial grade-separation. The reduction in congestion and delays that would be achieved at the intersection of NW Military and Wurzbach Parkway, combined with the improvements proposed at Lockhill-Selma, will improve operations along the corridor between Lockhill-Selma and NW Military which, in turn, should reduce congestion-related crashes.</p>



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87	E. Vela	8/30/17	Transcript	<p>87d. And, basically, it's just pushing down the -- all of this congestion that's happening at Wurzbach and Northwest Military. It's just pushing it down one block to Lockhill-Selma and Wurzbach, and so we really don't see the long-term value in any of these options. But, at least, with Option #5, TxDOT can spend their money, some of their money, and we can have less of that annoyance that we're expecting.</p>	Please see response to comment #29b.
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<p>87e. Additionally, I know the community of Summerfield, one of their main concerns is their inability to get out of their neighborhood. But if they were able to get that traffic signal that they want -- I think it's Fairfield and Military -- that would alleviate most of their concerns. And we're talking about a residential area, this whole area here, and, basically, TxDOT wants to turn it into a highway in the middle of a residential area and it's not fair to all of the communities in this area.</p>	<p>Please see response to comments #5b and #7b.</p>
<p>87f. Additionally, the community, I believe it's called Whispering Oaks, if they realize that, basically, all of that traffic congestion that is now at Wurzbach and Military is going to be pushed down to Lockhill-Selma and Military, and it's going to back up their exits from their communities, I don't think they'll be as happy; and that's one of the points that I plan on getting across to the community in the very near future, to make sure they get involved and have their say in this process, because I don't think they'll be happy.</p>	<p>The additional through lanes at Whisper Valley Street will allow better access in and out of the Whispering Oaks neighborhood. With the beginning of an eastbound lane at Whisper Valley Street, traffic will be able to turn right easier. With the additional lane in the westbound direction during non-peak hours, they should have more gaps in traffic to get out faster.</p>
<p>87g. That's really all I have to say. I just hope that we can go with Option #5 or preferably Option #6. It's not going to -- any of the options aren't going to do anything of long-term value, not in my opinion.</p>	<p>Comment noted.</p>



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				<p>87h. A lot of the traffic that currently -- excuse me, a lot of the traffic that will begin flowing -- well, excuse me, let me restate that. Once the construction is completed, it's just going to bring additional traffic into our area and, in the end, we're going to be back in the same situation we are now, and maybe all we need is the green light at Wurzbach and Military to be on just a little bit longer than the 20 seconds it's set at right now.</p>	<p>Traffic is expected to increase over time with or without this project being built. Building any option should reduce congestion within the project limits, but the greatest reduction is associated with the traditional SPUI option. The other grade-separated options would also significantly reduce congestion. For all options, latent demand can affect the benefit of the improvements. Each improvement helps increase capacity and meet future demand. Increasing the green time for one major movement at the intersection results in increased delays for the opposing movement that will be stopping longer, and usually means a decrease in time for a competing movement. However, this project would reduce signal phases for those options with all left-turns to occur at the same time, which increases efficiency. The city of San Antonio manages the signal timing for the intersection and has regularly improved the phasing and intervals wherever possible.</p>
88	Stuart Cameron	8/30/17	Comment Form	<p>The best option is "No Build." Any work on the Wurzbach and NW Military crossing will make things worse. The traffic we are getting now is from cars escaping 1604. Until 1604 is improved, nothing will help this neighborhood.</p>	<p>Comment noted.</p>
89	Kati Bahrami	8/30/17	Comment Form	<p>89a. I am against building a tunnel, against having a highway in this area. This plan for a highway can increase traffic, which you are proposing it will decrease.</p>	<p>Comment noted.</p>
				<p>89b. I also do not want the trees to be eliminated and our ability to walk on the sidewalks on Wurzbach.</p>	<p>Efforts will be made to reduce the number of trees to be removed in every option. The sidewalks will remain along Wurzbach Parkway and access by pedestrians to business or homes will not be affected.</p>
				<p>90a. I am against building a tunnel and highway on Wurzbach.</p>	<p>Comment noted.</p>
				<p>90b. I don't want the trees to be eliminated and I want to have the peace and quiet that we currently enjoy.</p>	<p>The at-grade option will eliminate the fewest trees of the options presented, other than the no-build option. However, all options will attempt to keep as many trees as possible.</p>



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90	Steve Golub	8/30/17	Comment Form	90c. There will be more accidents.	Our traffic studies indicate all options should have a reduction in crashes mainly because we are reducing the number of conflict points that exist today. Hence, if we have fewer conflict points, we would anticipate fewer crashes.
				90d. And this seems to be more for people living far from here and there is no concern over people who currently live here.	Please see response to comment #87h.
91	Julie McLaughlin	8/30/17	Comment Form	91a. I am against all plans which include an underpass through NW Military.	Comment noted.
				91b. These plans will affect the value of my home negatively.	Comment noted.
				91c. And, I am unconvinced it will create a significant amount of efficiency at the proposed intersection.	Total overall delays will be reduced by all the options. Some options reduce more of the delay than others.
92	David Martin	8/30/17	Comment Form	92a. This intersection is only congested from Monday through Friday, 1-2 hours a day max. All other times (Saturday and Sunday) are not congested. I believe this is a waste of money.	Comment noted.
				92b. And the nearby property values will drop due to this highway.	Comment noted.
				92c. This will only move the traffic down one block to Elm Creek. This will render Inverness and Alon gates unavailable for intended use.	The additional lanes will allow better access in and out of the neighborhoods by reducing the queues and the level of congestion.
93	Tammy Martin	8/30/17	Comment Form	93a. Property values will decline.	Comment noted.
				93b. This will move the heavy traffic to Lockhill-Selma, which has fewer lanes.	All the options are adding an additional lane both eastbound and westbound at Lockhill-Selma.
				93c. This location will be much noisier.	Please see response to comment #62c.
				93d. This intersection is only busy in the afternoon, 5 PM -6 PM (Monday - Friday). Money should be spent on other locations, such as 281 North.	Comment noted. Also see comment #87h.
				94a. It will cheapen and make the area ugly. This will lower our property values considerably because of noise, dirt and unaesthetic appearance of the project.	Comment noted.
				94b. It is also not efficient because traffic will just be worse at Lockhill-Selma.	Please see response to comments #29b and #79c.



PA (Principal Arterial) 1502, from Lockhill-Selma Road to NW Military Highway Open House #2 (8-30-17) Comments

94	Afton Bennia	8/30/17	Comment Form	94c. San Antonio lacks attractive landscaped roads. This is sad to see a nice area go down.	Aesthetic treatments will be developed to ensure the project is compatible with the community, and every attempt will be made to preserve green space in the corridor.
				94d. We can consider a class action lawsuit regarding our property values. No one will want to live in these neighborhoods. Less property taxes for city and county. Please help Houston with this money. What is behind this? Money for whom?	Comment noted.
95	Robert Elizondo	8/30/17	Comment Form	I live on Whisper Dew and my backyard is Wurzbach. I am afraid the increased traffic and noise is going to negatively affect my home's value and will impact my ability to sell my house. I am very sad and upset about your plans.	All of the four grade-separated options will significantly improve traffic flow and reduce congestion, the traditional SPUI the most. Traffic volumes are anticipated to increase substantially by year 2040. The intersection will experience much greater congestion and delays for the no-build option, as compared with the proposed options, and the increased delays will result in worsening air quality.
96	Snivami Ruparel	8/30/17	Comment Form	96a. This intersection at NW Military is only going to cause a bottleneck at Lockhill-Selma.	Comment noted.
				96b. The traffic jam is really one hour or so in the morning and one hour in the evening. Then why do this?	Please see response to comment #29b.
				96c. Our property values will go down by having a highway outside.	Comment noted.
97	Loren Maric	8/30/17	Comment Form	The project is not common sense. You want to spend millions to fix traffic at one major intersection and then have more problems heading west from Lockhill-Selma onward. This is a misappropriation of taxpayers' money. There are several more problematic areas than this intersection. You cannot waste money to change one intersection without planning out the rest of Wurzbach roadways heading west. Who is gaining from this construction project. Who is so motivated to spend this money?	Please see response to comment #29b.
98	Jarvis Witt	8/30/17	Comment Form	Why spend \$20 to \$30 million on a project not completed until 2023 when another 1604 improvement done by 2025 may have a similar, if not better impact on Wurzbach Parkway traffic? I think the drives affected by the huge construction impact 2020-2023 may learn to drive somewhere else anyway.	Comment noted.



PA (Principal Arterial) 1502, from Lockhill-Selma Road to NW Military Highway Open House #2 (8-30-17) Comments

99	Kari Kent Schultz	8/30/17	Comment Form	<p>99a. Spending \$28 to \$29 million dollars at the intersection won't remove the bottleneck at Lockhill-Selma, thus won't move any more traffic. Money is better spent widening NW Military, then widening Huebner to USAA and Medical Center. Also, a better use for the money would be to widen NW Military out to 1604.</p>	<p>Please see response to comment #29b. The other corridors are important also and need improvements, but they are outside the scope of this project.</p>
				<p>99b. Why hasn't a longer-term plan (2030) been presented to the community? You are opening up one intersection, but then where are the cars, traffic and bottlenecks going to end up? Just another problem.</p>	<p>The analysis of the 6 options included results for future year 2030 at both intersections: Lockhill-Selma and NW Military. The exhibits and the PowerPoint presentation were provided at the meeting and online at TxDOT's website shortly following the public meeting.</p>
				<p>99c. Also, we need a lane for Elm Creek residents going westbound. With traffic bottlenecking, we can't get into neighborhoods.</p>	<p>TxDOT will consider extending the right-turn lane to the Elm Creek entrance.</p>
				<p>99d. Also, what are you going to do about air quality for residents?</p>	<p>This project is intended to reduce congestion and delays, which benefits air quality.</p>
				<p>99e. Can't you widen Lockhill-Selma to I-10? Can't you widen Lockhill-Selma to Huebner? Have to widen NW Military - only option for this area? And then be forced to widen Huebner? Spend money to work that out before configuring the intersection at \$28 to \$29 million.</p>	<p>Please see response to comment #99a.</p>
				<p>100a. I have several strong reasons for choosing Option #6 - Make no changes to the intersection of Wurzbach Parkway and NW Military Highway. I have lived near this intersection for 15 years and traverse this intersection many times a day. The traffic buildup occurs only at rush hour - every other time of day, traffic flows freely. \$30 million is too much to invest on a problem that occurs only for a few hours each weekday, particularly as drivers have several other routes they can take to get to I-35.</p>	<p>Comment noted.</p>
				<p>100b. Millennials and Gen X-ers aren't driving as much as the older generation. As the older workforce retires, there will be fewer cars on the road (this could be accelerated by improving our city's public transportation).</p>	<p>Comment noted.</p>



PA (Principal Arterial) 1502, from Lockhill-Selma Road to NW Military Highway
 Open House #2 (8-30-17) Comments

100	Gigi Kendrick	9/2/17	Email	<p>100c. Wurzbach Parkway, between Lockhill-Selma and NW Military Highway, is aesthetically a beautiful road, with trees in its median and walking paths. I often see parents with young children walking or biking there. The proposed design, as featured in the video, is hideous, with no way for pedestrians to safely cross the street. It has destroyed every tree. There is no sense in ruining one of the most attractive roads in San Antonio.</p>	<p>The at-grade option retains most of the trees in the center median. The no-build option removes no trees since it proposes no physical improvement to the intersection. All other options remove the center median, thereby removing significant portions of trees. TxDOT is providing options that offer various designs and address different community concerns in addition to reducing congestion and delays. The public input will assist TxDOT with identifying the preferred option.</p>
				<p>100d. Wurzbach Parkway between NW Military and I-35 is a very curvy road and the speed limit is only 60 mph. Already we deal with people treating Wurzbach as a major highway, which has lead to accidents and fatalities. This new design makes Wurzbach resemble a major highway, which will encourage people to go even faster and lead to more accidents on roads that weren't designed to be treated as interstate highways.</p>	<p>The crash history for the 3-year period from 2012 through 2014 showed a single fatal crash on Wurzbach Parkway between Blanco and NW Military. Crash rates were calculated for segments along the Wurzbach Parkway corridor and compared to statewide average crash rates. The section of Wurzbach Parkway between NW Military and Blanco Road has a crash rate substantially lower than the statewide average. A speed zone study is required to determine if the posted speed limit should be reduced based on crash history, roadway design, 85th percentile speeds, traffic volumes, sight distance and other factors.</p>
				<p>100e. This project will take years. By the time you've finished, there will be some other traffic problem that will require the money we just wasted here. Furthermore, during construction many commuters will elect to use George Road to get from NW Military to Lockhill-Selma. If you think the traffic is bad now, wait until you try to get all those cars down a residential, frequently flooding, small neighborhood road.</p>	<p>The construction phase has not been designed yet. However, during construction we try to maintain the same number of existing through lanes to keep traffic relatively the same as before construction begins.</p>
				<p>100f. I see no way this tunnel will not flood when we have rain. There are several other ways to better the traffic problem at I-35 without ruining one of the few attractive roads in San Antonio.</p>	<p>Please see response to comment #27b.</p>



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 Open House #2 (8-30-17) Comments

101	Alfred White	9/1/17	Email	101a. I am writing to express my opinion regarding the proposed TxDOT project at NW Military and Wurzbach Parkway as a citizen of San Antonio and a resident of the Estates of Alon neighborhood. I have reviewed the current proposed plans and feel that the SPUI, modified SPUI, and diamond interchange plans all possess the same flaw - that they will relocate the bottleneck of traffic from NW Military and Wurzbach Parkway to Lockhill-Selma and Wurzbach Parkway. For westbound traffic, this is an inconsequential improvement over the current design. The huge financial investment, time and disturbance caused by construction are not justified by their meager benefits without tremendous improvements to Wurzbach from Lockhill-Selma to I-10. Therefore I cannot support their implementation.	Please see response to comment #87h.
				101b. The partial-grade separation and the at-grade improvement would have a similar effect as the aforementioned plans but would provide better flow for the eastbound traffic. This appears to me to be a reasonable benefit; however, the cost, time and disturbance are questionable.	Comment noted.
				101c. The bottom line is that without significant improvements to Wurzbach from Lockhill-Selma to I-10 (San Antonio maintained), improvements to NW Military and Wurzbach are pointless for westbound traffic and provide questionable benefit to eastbound traffic. Therefore, I would advocate for the project that has the lightest burdens of cost, time and disturbance (if any project at all).	The traffic analysis shows that the intersection will be greatly improved when completed and in year 2030 for the SPUI or SPUI with frontage roads options. The delays will be 86% less than the no-build option in the worst peak hour (PM). Even the traditional diamond option will significantly reduce delays by 72% in the worst peak hour. The at-grade or no-build options are being considered as well and the cost associated with the at-grade improvements is substantially less, as is the construction duration.
				101d. As an aside, I feel that a light at the intersection of Alon Loop and NW Military would be a significant benefit to the safety of those navigating those turns.	Please see response to comment #5b.
				102a. We do not want the tunnel option for the Wurzbach Parkway project. So many inconveniences, including: noise, lane closures, 2-3 years of this! Our bedroom window is right by the construction!	Comment noted.



PA (Principal Arterial) 1502, from Lockhill-Selma Road to NW Military Highway
 Open House #2 (8-30-17) Comments

102	Cindy Farris	8/30/17	Email	102b. This causes more congestion at Lockhill-Selma.	Yes, the queues will be longer but adding a through lane in each direction will increase the throughput, which will offset some of the congestion\delay overall at the intersection.
				102c. Our walkability and bike-ability will be reduced (safety issues) which is why we moved here in January of 2016.	Sidewalks and bike lanes will remain for the length of the project.
				102d. Tree removal will be ugly and will make the neighborhood feel more like a concrete jungle.	This mainly occurs on the underpass options. The at-grade option keeps most of the trees.
				102e. Inaccessibility to Alon Estates entrance off of Wurzbach will be super annoying. Please do not do the tunnel option.	Comment noted.
103	Hitesh Leva	8/30/17	Transcript	103a. What seems highly inefficient here is that they're not increasing the lane count across the artery. I'm not a transportation engineer, but I am an engineer, and I can tell you that when I try to cross NW Military on Wurzbach some times of the day, the issues that I'm facing are that the lights are highly inefficient. The bulk of these problems can be resolved by just fine-tuning and optimizing the duration of the traffic lights, and when I've asked across the stations to the people presenting have they explored that option, I didn't quite get a clear answer. So I think before we spend any tax dollars, I would really like for that to be explored.	Optimizing the traffic signals and improving progression along Wurzbach Parkway is a short-term improvement that provides some limited benefit. The city of San Antonio continuously monitors and updates the signal timing to improve traffic flow. This does provide benefits but they last only for a short period of time. The objective of this project was to identify and evaluate long-term solutions that will accommodate the anticipated growth.
				103b. By forcing residents who live in the neighborhood to be subjected to this horrible construction period, it's not what some of us signed up for.	Comment noted.
				103c. There is a distinct possibility that our real estate value could get compromised, in which case the city has to really step up to the plate and fix those issues.	Comment noted.



PA (Principal Arterial) 1502, from Lockhill-Selma Road to NW Military Highway Open House #2 (8-30-17) Comments

104	L. Terracas	8/30/17	Transcript	<p>I'm wondering why is it we've had all of this work on the Wurzbach Parkway, which is great, but they have never resurfaced the area from Lockhill-Selma going to Vance Jackson. They have done all of the other sections going down, and then the sections beyond Vance Jackson going south, but that area has been neglected. And I've asked the people with the Texas Department of Public Safety and I just found out that that's because that's the city's section. If the city has taken care of all of the other areas, why have they neglected that area for years? It has not been resurfaced. They just fix the potholes. So I'm wondering if we can get it done anytime within the next 12 months so that would be fixed, and it wouldn't take as much money as some of the other projects they're talking about. And then we've seen an increase in traffic in the last, six to seven years since they started doing the Parkway. I would think that in consideration of all of the additional traffic flow that they've brought that they would have fixed that section or at least repaved it.</p>	<p>You are correct, the maintenance of Lockhill-Selma, a city street, is controlled by the city's Transportation and Capital Improvements (TCI) Department (http://www.sanantonio.gov/TCI). We suggest you address your comments to them. This is a TxDOT project and Lockhill-Selma Road, other than the intersection, is outside the limits and scope of this project.</p>
				<p>105a. My comment goes to some of the things that have not been discussed. The first thing is, the right of way for putting drainage into this new highway - where is the drainage going to be put, and what is the right of way and where is it going to? From Jones-Maltsberger down to Wurzbach, it's like you could take a skateboard at the top at Huebner and skateboard down without stopping. That's a giant hill and it's a giant flood plain. I talked to the gentleman tonight who is doing the environmental study. There are a lot of animals in the area.</p>	<p>Currently it is anticipated that the drainage will be handled with underground pipes within the existing right of way. No additional right of way is planned to handle the drainage.</p>
				<p>105b. The Elm Creek people are not going to be able to get out of their gates, or get back in in the afternoon, because of the traffic flow through there. It's going to be slow as can be.</p>	<p>Elm Creek currently has a traffic signal to allow traffic to get in and out.</p>
				<p>105c. My idea for this thing is to hire a public relations firm, each put in \$2,000 a piece or \$1,000 a piece, have them hire a lawyer, and then start a public relations campaign against this project to alleviate some of the weaknesses in it which will ruin neighborhoods for years to come.</p>	<p>Comment noted.</p>



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 Open House #2 (8-30-17) Comments

105	N. Morgan	8/30/17	Transcript	<p>105d. I am a realtor and I own a real estate company with 250 agents and we know that the value of these homes, because of that Parkway going through, are going to go down. It will affect them. They won't know how badly, but it will be bad.</p>	<p>Comment noted.</p>
				<p>105e. I think that when it goes to vote or whoever does the final thing at AACOG meetings, that the proposal has gone for the vote before whoever votes for it, and then they take it off the table, take it in a back room, come back, and it's changed, and they vote on it, and you don't even know they changed it. Now, this is the way the politics work on this. I think it's a done deal, I think it's a slam dunk, and unless we hire professional people who know how to handle this situation I think you might as well not even be here. This is all for show, this night, tonight.</p>	<p>No decision has been made on the preferred option. We have had two focus group meetings and this is the second public meeting to gather the community's input. There is no vote on the project. At the first public meeting we had one recommended alternative. We listened to the community and returned with 6 options for the community to provide input.</p>
				<p>105f. One other item has got to do with the Alon Shopping Centre. And the owner of that center is very interested, I'm sure, in keeping the traffic slowed down so that they can enter into that huge Alon Shopping Centre, which is one of the biggest in the city. If this goes through and they bypass it, he's not going to be happy, and he's probably working in the background to get the traffic to be able to get off and get into his shopping center at Military and Wurzbach. It's a beautiful shopping center.</p>	<p>Comment noted.</p>



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106	C. Salvato	8/30/17	Transcript	<p>I am absolutely opposed to the new roadway situation here. I'm a resident in Alon Estates and I think that the impact that it's going to have on our property values, just from the long construction period, and, ultimately, the minimal amount of time that the difference between the way the traffic is now and the way that it will be when this is completed is not worth the headache that we're all going to go through while this is underway. And the cost, in my opinion, certainly is not worth it.</p>	Comment noted.
107	Katherine Bowman	8/30/17	Transcript	<p>I'm a homeowner in an adjacent neighborhood. I am vehemently opposed to the major construction options that are being considered, primarily because I feel like it's extremely shortsighted and highly irresponsible of both the State Department of Transportation as well as our city roadworks in terms of planning. They're talking about a \$20 million fix to this intersection of NW Military and Wurzbach. The next situation which will come is the intersection at Lockhill-Selma which is all under city control up to close to I-10. There is not room, accessibility or real estate to accommodate the future traffic flow that this is going to send that way. Extremely shortsighted and highly irresponsible. When I was talking with the State Department of Transportation representative here tonight, he basically said as much. I don't understand how he can come, represent and support this project and feel comfortable about it when there have been a complete lack of studies and understanding of what's going to happen on the next project. People are calling it "kicking the can down the road." I'm really disappointed about that. I think that those steps, those studies, and the understanding of what the traffic is going to look like and how it's going to be handled past the Lockhill-Selma intersection has got to be looked at and understood very clearly before this project is decided upon. The state and the city of San Antonio have a responsibility to their citizens to do that thoroughly and not just spend state money because it's here and it's available.</p>	Please see response to comment #101c.



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108	K. McLaughlin	8/30/17	Transcript	<p>108a. I just wanted to state for the record that I am very much concerned and against the idea of having any underpass going under NW Military at Wurzbach. I believe it's going to affect the value of our homes in a very significant and negative way. My wife and I spent an exhaustive amount of time trying to find a place after I retired from the Air Force and now that we've been here for a couple of years, we don't want to see our investment destroyed.</p>	Comment noted.
				<p>108b. Furthermore, I don't think there's going to be any improvement that's going to be caused by this project. The efficiencies, I am not convinced are there. We move the problem down the street, which makes it a bigger problem for my community, and at the same time Texas, the city of San Antonio, has also stated that there are no more improvements that can be made beyond Lockhill-Selma which then implies, to me, that there is really no efficiency coming out of this at all. So unless they can make this happen all the way through to I-10 or the Medical Center, they are not going to realize the kind of efficiencies they state they will. So otherwise this is an extreme waste of money and a bad idea.</p>	Please see response to comment #29b and #29c.
109	C. Morrison	8/30/17	Transcript	<p>I have yet to see the option of exploring intelligent lights at this intersection to I-10, or throughout NW Military, that would improve the traffic flow rather than shift the bottleneck. Intelligent lights are a much lower cost. It's something that hasn't been explored on a relatively new intersection, and it makes no sense to me to spend millions of dollars on a project without having tried more efficient options. This project is invasive. The homeowners who would be affected by this are horrified by the impact this will have. Commuters will be facing years of congestion to avoid a problem of, basically, two minutes in the afternoon and a minute in the morning according to TxDOT traffic statistics. Because the intelligent lights I've seen haven't been explored, all of the options except for one keep reverting to this underpass option. It seems to me very much a pet project that I cannot find any rationale behind.</p>	Intelligent Transportation System-based traffic signals are being considered for installation at intersections by the city of San Antonio, who maintains and operates the signals on Wurzbach Parkway between NW Military and I-10. However, our traffic analysis demonstrated that more substantial types of improvements would be needed to address the anticipated future volumes and the increases in delays.



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110	K. Peacock	8/30/17	Transcript	110a. I'm a resident of Alon Estates. I'm here tonight to get more information regarding the options that we have. I've been to all of the meetings and I still do not have an understanding of why this project was even brought about. When I first heard of TxDOT's plan, I didn't realize that their jurisdiction stopped at Lockhill-Selma, so I think this is, in fact, a misuse of taxpayer dollars to do any of these improvements. My understanding is something has to be done, so I would say then to do something with minimal invasiveness, which is expanding the lanes on Wurzbach Parkway.	Please see response to comment #52d.
				110b. Even with that, I think the impact to commuters is going to be very minimal given that it will stop at Lockhill-Selma. I understand that there are intersections with better traffic, but, regardless, we're going to have traffic. I commute to the Medical Center every day, sometimes in very high peak times, and I don't mind sitting in the traffic.	Please see response to comment #101c.
				110c. I think if TxDOT were to go with this SPUI option, which I'm very much against (the tunnel), by the time they spend the years that it's going to take to get that done, we're going to have to contend with that construction and I think the commute will be even longer than what we have now. By the time they were to finish that, hopefully 1604's work would be done, and I think some of this traffic is going to be moved over to 1604 anyway, so it would be a moot point to do this.	Please see response to comment #87h.
				110d. I'm very concerned, as a resident living in Alon, about the impact that it will have on my community, the safety of my community, and so I'm very much against doing anything. I would much rather see these TxDOT funds go to other places in San Antonio or, heck, now re-route it to Houston. Lord knows they're going to need more help. So I think there are plenty of places to put these funds rather than having someone's pet project be funded. And most likely the individuals who are making these decisions don't reside in communities in this area.	Your comments are noted. The no-build is an option which will leave the intersection as-is.



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				<p>111a. First of all, I think that TxDOT is increasing the traffic through a neighborhood, and we're moving the traffic congestion from NW Military to Lockhill-Selma and it's going to bottleneck there.</p>	<p>All of the options proposed will add an additional lane both eastbound and westbound at Lockhill-Selma, which will reduce overall intersection delay at that intersection, as compared to the No Build scenario. The improvements at Lockhill-Selma are proposed for construction in tandem with improvements at NW Military to alleviate the bottlenecks that might occur otherwise. The traffic analysis shows that westbound queues at Lockhill-Selma will only increase for the Option 1 (SPUI with signals), and that while queues may increase at that location, the time savings achieved at westbound NW Military will result in an overall time savings for westbound travellers. Additionally, the time savings for NW Military traffic and for eastbound Wurzbach Parkway traffic will be greatly improved with Option #1 and significantly improved for Options #2 and #3.</p>
				<p>111b. We know that Nirenberg lives in Summerfield; we know that he's probably behind pushing this through because his residents that live in that neighborhood have no access out of their neighborhood because of the traffic that was created by Wurzbach Parkway. Common sense would say that they could put a dedicated barricaded exit from their subdivision out onto Wurzbach Parkway, and then they would have the traffic light there at NW Military.</p>	<p>Projects are selected by the Alamo Area MPO based on numerous criteria and as funding becomes available. The MPO identified the study intersection as a priority project.</p>
				<p>111c. Another thing that they need to do is give them a traffic light at their main entrance. They have no traffic light so they can't exit. I understand their frustration, but barricade an exit so they have a dedicated lane for them to exit out. They don't have one now.</p>	<p>Please see response to comment #5b.</p>



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111	E. Mendoza	8/30/17	Transcript	<p>111d. Increase the timing of the traffic light at NW Military. Double it maybe. I don't know how long it is, but it seems to me that only two, three or four cars can pass at a time. We're creating a bottleneck and all we're doing is moving the traffic from NW Military to Lockhill-Selma.</p>	Please see response to comment #71b.
				<p>111e. The other thing that I'm concerned about is that we're creating a traffic jam up Wurzbach to I-10. We have schools there. We have a Catholic school. We have a public school. The public school has a lot of refugee children who go there. I know because I was a teacher there and I retired from there. Refugees do not know how to respond to increased traffic. They don't cross it at corners. They don't wait for lights. They dart across the roads. I've seen it traveling on the other side of I-10. So we're trying to move people from Wurzbach Parkway up to the Medical Center or Fredericksburg Road to go to USAA; you're going to create more and more fatalities. I know these kids get on the bus, but a lot of them cross over the street, over Wurzbach, to go to Valero for the convenience stores. I've seen a fatality there. It's increasing your traffic.</p>	TxDOT and the city of San Antonio are aware of the limited capacity of Wurzbach Parkway between Lockhill-Selma and I-10. Other efforts outside this project scope will be looking to address these concerns.
				<p>111f. Another thing that I disagree with is getting rid of the trees that are in our esplanade. We're supposed to be a green city, we're supposed to promote health, but we're destroying the trees and eliminating the air pollution that they absorb. The photosynthesis these trees absorb, we're destroying that and that's a shame.</p>	Please see response to comment #21c.
				<p>111g. I'm concerned about the increased traffic, the noise, the devaluation of our houses. I already see that there are a lot of houses in our neighborhood that are now going up for sale because of this mess that whoever created. Common sense, to me, is to divert some of this money for our neighborhood that we do not want and put it at 1604. If it's going to be a four-year project then let the four-year project be at 1604 where they need the increased traffic flow.</p>	Comment noted.



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				<p>111h. This is a convenience for people that do not even live in our neighborhood, at a convenience of 30 seconds gained at a cost of \$30 million. That doesn't make any economic sense to me. Those people need to leave a little bit earlier instead of leaving late.</p>	Please see response to comment #101c.
				<p>111i. We have a young community and we have a retired community; we don't want to fight getting out of our subdivision. We don't want to fight this additional traffic. We don't want to exit our beautiful neighborhood that we're looking at these trees and now we're just going to see concrete and steel for the barricades. We don't want to see it.</p>	Comment noted.
				<p>111j. Nowhere have we seen three-dimensional renditions. We only see flat copies, one dimension. Where are the people that are supposed to have these analyses? We want to see them.</p>	The animation shows the rendition of the SPUI in more detail. The at-grade option is similar to what is there today.
				<p>111k. We are very passionate about this, we don't want it in our community. I think common sense has to prevail and, perhaps, diverting some of this money and using it on other cities that need the infrastructure because of the hurricane. I don't know if the money can be appropriated, but common sense has to take place in situations like this.</p>	Please see response to comment #111b.
				<p>111l. Another thing, I looked at the TxDOT 100 most congested streets in the state of Texas; Wurzbach is not even on the map. 1604 is number 43, or something like that - I don't know the specific number. So, again, they're not even on the radar right now; why are we spending this money? So most of the residents do not want it. So that's my strong opinion.</p>	Our intent is to reduce the congestion and delays with one of the options presented. No decision has been made concerning this intersection. The Alamo MPO selects projects for the region and provides the funding.
112	L. Marie	8/30/17	Transcript	<p>112a. This project doesn't make sense. It will improve a block or two, but you still have issues heading west beyond Lockhill-Selma on Wurzbach. The amount of money spent is not worth the time and the labor and to get the end product. I do think there is room for improvement, but I don't see how fixing one intersection is going to change the problem beyond. It's just delaying the problem.</p>	Please see response to comment #87h.
				<p>112b. I wish we could get a light for our neighboring community in Summerfield so they can exit safely.</p>	Please see response to comment #5b.
				<p>112c. And because TxDOT is responsible for Wurzbach heading east and San Antonio is responsible for Wurzbach heading west of Lockhill-Selma, the two need to communicate and work together.</p>	TxDOT and the city of San Antonio have been working in cooperation on this project.



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				112d. And it doesn't seem like the allocation of funds is wisely spent when we need it somewhere else in the community. I would like to know who's motivated to change this and why they're motivated to spend this money and how this became a priority over several other intersections in Bexar County. I think it's corrupt.	Please see response to comment #111k.
113	T. Martin	8/30/17	Transcript	113a. I would like to make my opinion known about this proposed construction at Wurzbach Parkway and NW Military. I am an advocate for Option #6 which is "No Build."	Comment noted.
				113b. I live in the area in one of the communities that backs up to Wurzbach Parkway. The noise level that we experience at this time from traffic during the day and the night is very loud. There are sirens and traffic noise, and I feel like this construction that they're proposing is going to increase the noise that we hear, and, therefore, decrease our quality of life.	Please see response to comment #62c.
				113c. Secondly, the gate that backs up to Wurzbach Parkway is the gate that we use to enter and exit our community, and Options #1 - #4 are going to restrict our use of that gate and restrict us to only be able to travel eastbound, and that's not something that I'm interested in. Options #1 - #4 are not options. The underpass is not something that we're interested in anyway. It's a very costly endeavor and I don't feel like it's necessary.	Comment noted.
				113d. Being a resident of this community, I feel like the traffic is really not that bad at this intersection. During the week there are just a few hours of traffic a day and, outside of that, it is not a congested area by any means. There are areas around town that are significantly more congested than this intersection and spending the amount of money that they're proposing on this project is not prudent in my opinion.	Comment noted.
				113e. I think that Option #6, "No Build," is the best option for the individuals that live in this area. I feel like our property values are going to be devalued with this construction that's being proposed. We pay a lot of money to live in this area, we contribute a lot of tax money, and this proposal is going to decrease the value of our homes, and we've paid a lot of money for these homes.	Comment noted.
				113f. If construction is done, then I feel like Option #5 would be the best, that it would not restrict our ability to travel in any direction at the intersection like Options #1 - #4 would, so I vote for "No Build." And if something's going to be done, it should be Option #5.	Comment noted.



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 Open House #2 (8-30-17) Comments

114	R. Mendoza	8/30/17	Transcript	114a. Options #1 - #4 I'm not even considering. I don't want a tunnel. I'm 100 percent not behind a tunnel.	Comment noted.
				114b. Option #5 is probably the more practical -- if there has to be a change made, if there are imminent plans for change, then Option #5 is probably the best use of money where you're not using the full \$30 million. I think it's \$18.5 million. I would not be opposed to that.	Comment noted.
				114c. Option #6 is the absolute best option. It's where you do zero, leave things as they are. People that live north of town that need to get to the Medical Center or need to get to their USAA, to their place of work, they just need to be better planners, in my opinion, because this does the small number of people that are worried about that crossing, that intersection of NW Military and Wurzbach Parkway, it's probably about 50,000 people going and coming, morning and evening. That's about one-half of one percent of the total population of San Antonio that we're going to spend \$30 million on. That's an absolute waste of money. I've been a taxpayer in this town forever and I will raise holy hell, and I'm going to get to the right people to let them know the Regional Mobility Authority should be here. They should be here. Nelson Wolff, County Commissioner, should be here, all of the people that can move the dial one way or the other. Don't send the little underlings, the down line, to come in to defend this move. Don't do it. Someone come up, step up to the plate and say, I'm the guy that's going to make the change on this deal.	Comment noted.
				114d. What kind of questions do you have? Do you have a Q and A? Get some dialogue going.	Please see response to comment #105e.



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				<p>114e. Because there would be a lot different story if someone got on the podium and felt the passion that people have that live in very, very high-dollar neighborhoods that they've invested a lot of money in, they've invested a lot of money in their homes, only to see the community, the vistas, that whole ambiance of living in that area wiped away and replaced with steel and concrete. (Rewrite for clarify - Well, those 50,000 people that travel through the Wurzbach Parkway and NW Military intersection during morning and evening rush hour aren't invested in the community like us and will not be impacted like I am.) I live in that community, and I paid a lot of money for my home, and I want to be there for a while, and this is going to devalue my property. And no one in this room, from TxDOT or otherwise, can convince me that it's not going to devalue the property.</p>	<p>Comment noted.</p>
				<p>115a. I'm a resident of Alon Estates and I want to get to the bottom line first: that I am strictly against this development. So given a choice, I would pick Option #6, which is a "No Build," and the reason for this is that I do not see any benefit in making this development or this construction at all in terms of traffic freedom, because we -- I and others in my community -- feel that it's going to back up at Lockhill-Selma and create a bottleneck there, and it's just really not going to serve any purpose at all.</p>	<p>Please see response to comment #101c.</p>
115	S. Ruparel	8/30/17	Transcript	<p>115b. In addition to that, we'll have to deal with the construction for a couple of years and that is going to affect our property values, when there is a lot more changes that need to be made in the city to begin with, and the money that's going in, it's a lot of money to just focus on one intersection that is not even on the top most congested areas in the country, in the state. So I really don't understand why we need to do this. And I live there and I commute through those roads all the time and I've never had issues.</p>	<p>Please see response to comment #111a.</p>



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				<p>115c. The other thing, having said that, is that the congestion is really for one hour in the morning and one hour in the evening, and just five days a week. It's clear otherwise, so there's really no issues at all other than those two hours during the day to put in \$30 million. And they say that it's going to decrease time by 30 seconds to 60 seconds, which is not a lot anyway. So I really, at the end of the day, don't see a point of why so much money should be put in an intersection like that.</p>	Please see response to comment #87h.
				<p>115d. But if I had to pick a second option, it would be Option #5 because of its obvious pros that the trees will be maintained, and the construction and the money put in is going to be much less; the time put in is going to be much less, and it is the least amount of the change that is going to be put in. So I pick Option #5 as a second option, but my first option is "No Build."</p>	Comment noted.
116	Maria Luna	8/30/17	Comment Form	I prefer Option #6, followed by Option #5.	Comment noted.
117	Margie Joffe	8/30/17	Transcript	<p>117a. I live in Alon Estates. Wurzbach Parkway seems to be busy only two times a day: morning and afternoon during the week just like any other freeway in San Antonio. At a cost of \$30 million to save 30 seconds per car, I don't think that is a good way to spend our money. I think the stewardship that we need to look at is to put this money where we need it. I know that TxDOT will do something. I don't know what. Doing nothing I understand is not an option, but a full-blown \$30 million project with jackhammers from 8:00 to 3:00, with dust for three years - no. No, thank you.</p>	Comment noted. Also note that Option #6, the no-build option, would leave the intersection as-is.
				<p>117b. I don't want 65 mph traffic going past my neighborhood. I feel like when the traffic comes off of Wurzbach Parkway or goes onto Wurzbach Parkway with the SPUI options, we're going to have excessive speeds. I don't want excessive speed going past my nice, quiet neighborhood. We have a lot of walking people, a lot of bike riders, people walking their dogs, children. I just don't want it. I don't want a freeway going past my window.</p>	The underpass option will not have pedestrians on the lower level. All pedestrian movements will be on the upper level, which will not be at 65 mph and will include signals to assist in crossing the intersection. The at-grade option keeps all movements at the same level as they are today.
				<p>117c. I think there will be an environmental impact by taking out the trees. I think that the noise will bother the animals at Hardberger Park. Please save my trees, consider the people that live in the neighborhood.</p>	Please see response to comment #21c.



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				117d. If this is a ten-year plan, what are the ten-year plans for 1604 and 410? Wurzbach Parkway took ten years to finish. This small stretch, how long is that going to take?	Depending on the option selected, the construction schedules range from 2 years to 10 months.
118	A. Meski	8/30/17	Transcript	118a. As a resident of the Estates of Alon, I am quite against Options #1, #2, #3 and #4, and my best option is #6, no build at all, and if worse comes to worst, Option #5.	Comment noted.
				118b. Is the exit from the Estates of Alon using NW Military Parkway going to be taken into consideration? Is there going to be a light in the future for the residents to move in and out? I understand it's not linked to this particular project as of now. Basically my question is: if Options #1-#5 are being considered, would there be a light at the entrance of NW Military and the Woods of Alon and Estates of Alon subdivisions?	See comment 5b for warranting of a traffic signal. See comment 29b for signals on Option #1 only.
119	Jaya Koneru	8/30/17	Comment Form	Please add a light at NW Military gate entrance at Alon. Very hard to see at night. I have turned into the wrong lane sometimes.	Please see response to comment #5b.
120	Marc Tannenbaun	8/30/17	Comment Form	120a. What about a dedicated bus lane?	VIA's Vision 2040 Long Range Plan does not currently identify Wurzbach Parkway as a rapid-transit corridor, which would require a dedicated lane. All other VIA services will be located in mixed-flow lanes.
				120b. Traffic light at Turnberry for every option.	Please see response to comment #5b.
				120c. Access to Inverness from the east is a must.	Comment noted.
121	James Kurn	8/30/17	Comment Form	If the city is interested in moving traffic, then get together with them and study Huebner. More land available, street is wider, less pedestrian traffic.	Comment noted.
122	Joel Day	8/30/17	Comment Form	Maintain property access.	Comment noted.
123	Rhonda Hoffman	8/30/17	Comment Form	123a. Need a light at Fairfield Bend and Turnberry for all options.	Please see response to comment #5b.
				123b. Do not bring a right-turn lane all the way to Bluffton Oaks as some people requested, as this will make it harder to get out at Bluffton Oaks and much more difficult to get to the left turn lane at NW Military.	We will be analyzing the need for the right-turn lane to be extended. Safety and operations will both be a consideration in the decisionmaking. A decision has not yet been determined.
				123c. More studies are needed concerning length of lights during rush hours. Set lights for weeks or months to see real impacts.	The city of San Antonio maintains the traffic signal timings and are working in cooperation with TxDOT on this project.
				124a. Not sure: Option #5 appears to look like #1, only less expensive and includes a straight away thru Wurzbach Parkway off of Bluffton Oaks.	Option #1 is a grade-separation with an underpass, or tunnel as some call it. Option #5 is at-grade, meaning all movements are at the same ground level, or in other words no underpass/tunnel.



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Open House #2 (8-30-17) Comments

124	Denise Sanders	9/13/17	Comment Form	124b. Could we please add a light at Fairfield Bend for Option #5?	Please see response to comment #5b.
				124c. Probably need to have one more meeting for all neighboring areas for best feedback!	Future meetings are planned. Check the project web site for future meetings. http://www.txdot.gov/inside-txdot/projects/studies/san-antonio/wurzbach-parkway-lockhill-selma-nwmilitary-highway.html
				124d. Consider "noise reducing" windows for our houses. Can we fix noise problem with windows to decrease noise? Add trees along wall at Summerfield or add bushes to fix noise problem?	Please see response to comment 62c.
				124e. The slides are very confusing and need more human explanation.	Comment noted.
				124f. Option #1 Pros: like the light at Fairfield Bend! Cons: Can't go straight on Wurzbach; don't like two right-turn lanes off of Parkway to NW Military; no Trees! Ugh!	Option #2 is like Option #1 with frontage roads to allow going straight on Wurzbach. The at-grade option keeps the most trees. For all options, efforts will be made to preserve as many trees as possible.
				124g. Option #5 Pros: keeps some trees, less money, looks better, seems to make the most sense, fix the timers on lights; Cons: Please add a traffic signal to this option at Fairfield Bend; Don't like two turn lanes off of Parkway onto NW Military (more traffic for Summerfield to get around).	Comment noted.
				124h. Option #6: Pro: prettier; Cons: very loud, horrible traffic, need to reduce the amount of cars using the Parkway. Get them back on 410, I-10 and 1604; do not want a noise wall.	Comment noted.
125	Gretchen Roufs	8/30/17	Comment Form	Thank you for studying this project. Please do something to alleviate the traffic at NW Military and Wurzbach. Ever since Wurzbach opened all the way, the bottleneck and related traffic has been problematic and inspires me to totally avoid this area, which is probably not the intent for this facility. Thank you for asking the difficult questions.	Comment noted.
126	Craig Bell	8/30/17	Email	I am a lifelong resident of San Antonio, and look forward to a solution to the problem at Wurzbach and NW Military. As I look at every other Wurzbach Parkway intersection where traffic was a problem, the solution was an overpass, not a tunnel. To me there is plenty of room for an overpass and it would not close the intersection as long as building a tunnel. I have to believe an overpass would cost millions less as well.	Comment noted.



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127	J. Ritchie	8/30/17	Transcript	<p>What I'm talking about is instead of all of the construction and everything, do minimal construction and take lights and direct traffic. Have four lanes going one way in the afternoon, slow down the traffic going this way, and change the lanes with the lights, whether they would be green lights for changing the lanes to five lanes for everybody to get where they're going; and then in the mornings turn it the other way to where the high traffic comes this way and the little traffic goes that way; the three or four cars that go that way aren't using it. That would be my idea: to have light signals with the big red Xs that this lane is closed like they do on toll lanes. They will put up extra lanes, that's the most cost-effective way to do it; it should work real good that way as I'm thinking. Because I'm budget-minded, I have four kids and four cars and insurance to pay for and budgeting; and I look out for the community because I'm involved in the church and so forth, try to help everybody.</p>	Comment noted. Also please see response to comment #21b.
128	J. Nicholas	8/30/17	Transcript	<p>I would just like any of the options to include better safety measures for bikers on Wurzbach Parkway. None of the examples that I've seen here show anything other than just striped lines, and as it is now, traffic that is cutting across those bike lanes do it without any consideration to the bikers at all, so if there is any way that they could make the bike lanes separate or safer, or put a curb up or something to protect the bikers better, I think, it's going to be safer for all of the bikers and the drivers in the long run.</p>	Comment noted.
129	D. Nicholas	8/30/17	Transcript	<p>All I wanted to say is the same comments that he made (see comment 128) but also relative to pedestrians, we need a safe means to not only cross, but to be able to walk the sidewalks and feel safe.</p>	Comment noted.
130	M Dawson	8/30/17	Transcript	<p>They haven't come up with a plan yet to save the trees. There are over 200 trees in the Parkway.</p>	Comment noted.
131	Mary Gonzalez	8/30/17	Comment Form	<p>Awesome project. Need to complete ASAP.</p>	Comment noted.
132	Ven Koneru	8/30/17	Comment Form	<p>Alon cannot use underpass.</p>	Comment noted.
133	David Rodriguez	8/30/17	Comment Form	<p>Thank you for options.</p>	Comment noted.
				<p>134a. Option #1 - Pro: Light at Turnberry. Cons: Very expensive, no trees and no through lanes westbound to enter Inverness.</p>	Comment noted.
				<p>134b. Option #2 - Pro: one through lane westbound to provide ingress to Inverness Boulevard. Cons: No trees and no light at Turnberry Way.</p>	Comment noted.



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134	Navin Bhatia	8/30/17	Comment Form	134c. Option #3 - Pros: Two through lanes westbound (from Blanco) to provide ingress to Inverness Boulevard. Cons: No trees and light at Turnberry Way.	Comment noted.
				134d. Option #4 - Pros: Some trees remain and excellent access to Inverness. Con: No light at Turnberry Boulevard.	Comment noted.
				134e. Option #5 - Pros: Trees remain (most), continued access, status quo at Inverness and Turnberry, and low cost. Con: No lights at Turnberry Boulevard.	Comment noted.
				134f. Option #6 - Pro: Status quo at Inverness and Turnberry. Con: No improvement to traffic issues.	Comment noted.
135	David Triplet	8/30/17	Comment Form	No Comment	No comment.
136	Jan Kurn	8/30/17	Comment Form	No Comment	No comment.
137	Richard Acosta	8/30/17	Comment Form	No Comment	No comment.
138	Mike Frontz	8/30/17	Comment Form	No Comment	No comment.
				139a. Option #1 has at least one significant drawback: the inability to pass through the intersection going east- and westbound at grade. This will cause numerous other traffic problems in the immediate area. The proposal to add a light at the Military entrance to Inverness only addresses one of several problems and causes problems of its own. A significant amount of traffic that exits Alon Town Centre exits directly onto Wurzbach headed eastbound, which would now need to exit through the light on Military. This intersection on Military already cannot handle the volume of traffic it receives in the afternoon or at any other high-traffic time. This proposal just shifts traffic from one spot to another, causing a greater problem.	Comment noted.
				139b. Option #2 would appear to be the option that addresses the most issues with the traffic needs, while not causing other problems.	Comment noted.



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139	Burt Holland	9/15/17	Email	<p>139c. Options #3 and #4 are just variations on Option #2 that do not provide any additional benefit but create problems, or do not fix some problems solved by Option #2. Given that it does not appear there is any significant cost savings or reduced construction time, I do not see why either Option #3 or #4 would be chosen over Option #2. I do not find the argument that the traditional interchange design of Option #3 is less confusing to be compelling. The exceptionally sharp dual-left turns in the traditional interchange seem to already cause confusion for drivers and create a dangerous situation. Many vehicles cannot make these turns and stay in their lane, or fail to do so because of driver confusion. I have seen many near-misses where an accident almost occurred because of a driver not staying in their lane. The slow speed this turn requires limits the number of cars that can get through this traditional intersection.</p>	Comment noted.
				<p>139d. Option #5, like Option #2, seems to address many of the problems with the current intersection. It may not address them as efficiently as Option #2, but it does so at significantly less cost and with less construction time. Ever since the intersection went in, I have wondered why it did not include features that are incorporated in Option #5.</p>	Comment noted.
				<p>139e. I see advantages to both Options #2 and #5. It is a balancing decision between greater ultimate efficiency but at greater cost and construction time. Please consider Options #2 and #5 as the preferred options.</p>	Comment noted.
				<p>140a. For the operations of our HEB grocery store within the Alon Town Centre, Option #2 would be the best solution for us. Having an efficient traffic signal system using the SPUI signal design system helps improve capacity of the intersection. By including the Wurzbach Parkway through lanes, this option keeps the current customer movements in place and does not force additional customers to use the already busy traffic signal on NW Military Drive.</p>	Comment noted.



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140	Mary Rohrer	9/15/17	Email	<p>140b. As previously mentioned in my earlier comments, please incorporate the following comments as TxDOT designs and constructs the improvements: Please provide locations for installation of permanent TxDOT approved signage for Alon Town Centre on Wurzbach Parkway at the NW Military ramps to direct customers to HEB and Alon Town Centre. With the center's size of over 300,000 sf, the center meets TxDOT's criteria for retail signage on roadways.</p>	<p>The TxMUTCD allows for General Service Signs to inform drivers of needed services. However, on most conventional roadways, commercial services are within sight, making signing unnecessary. Most drivers using Wurzbach Parkway are familiar drivers and will be aware of the locations of local businesses and how to access them. TxDOT will review the need for General Services Signs as detailed design plans are prepared.</p>
				<p>140c. Consider limiting major traffic construction disruptions during major holidays, such as Thanksgiving, Christmas and Easter, when the HEB store and the center is most busy.</p>	<p>Yes, construction schedules take into account local and national holidays to minimize disruptions.</p>
				<p>140d. Temporary direction signage to Alon is needed during construction and shall be visible and inviting for customers at Wurzbach driveways, and include directional signs at the new SPUI intersection.</p>	<p>During the construction plans development stage, temporary direction signage will be analyzed.</p>
141	Michele Haussmann and Jack Braha	9/12/17	Email	<p>This letter serves as the official comment from us on the project as conceptually designed by TxDOT. It is important that the design includes the tunnel under NW Military Highway as well as at-grade westbound and eastbound lanes on Wurzbach Parkway through the NW Military Highway intersection. This MSPUI design continues to provide direct access to the main gate into the Inverness neighborhood. If these lanes are not included in the project, we feel our neighborhood and our place of office will be severely and adversely impacted.</p>	Comment noted.
142	Wurzbach/NW Military Partners, LTD	9/13/17	Email	<p>142a. This letter serves as the official comment from Wurzbach/NW Military Partners Ltd, as owner of Alon Town Centre, on the project as conceptually designed by TxDOT. We understand that there are six options TxDOT has evaluated for this intersection. After reviewing the updated conceptual designs presented by TxDOT on August 30th, we feel that the Option #2, Modified Single Point Urban Interchange with Frontage Roads (MSPUI), provides the best long-term solution for the immediate area and access to the South Texas Medical Center.</p>	Comment noted.
				<p>142b. More specifically, the MSPUI option includes the continued at-grade eastbound lane on Wurzbach Parkway, which will allow vehicles exiting Alon Town Centre that want to go east on Wurzbach Parkway to continue through the intersection.</p>	Comment noted.



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				142c. To restate our concerns about the original SPUI design, with the elimination of the Wurzbach at-grade eastbound lane, vehicles exiting Alon Town Centre that want to go east on Wurzbach will be forced to use the NW Military entrance in the middle of our property, which will cause added congestion and access issues for customers of H.E.B. and surrounding tenants. Without the eastbound lane, traffic congestion will be created on privately owned property and cars will stack in an unsafe manner for pedestrians in our shopping center drives and parking lanes. Please see the attached letter to you dated March 13, 2017, for more detail.	Comment noted.
143	Richard Slife	9/13/17	Email	My wife and I support design Option #1. This option provides for the safest entrance and exit from Summerfield to and from all directions.	Comment noted.
144	Daniela Oliver	9/12/17	Email	We live in the Inverness subdivision. If TxDOT feels a split-grade or tunnel intersection is necessary, then we prefer the modified SPUI with lights at the Summerfield/Inverness drives on NW Military from the split-grade choices. Our overall preference is Option #5.	Comment noted.
145	Saul F. Levenshus	9/12/17	Comment Form	The lengthy construction timeframe and the reality that the problem will only be mitigated for one traffic light leaves me to believe that Option #5 is the best option. All options only move the problem up one light. Solve the problem at I-10 and Wurzbach or these solutions won't stand the test of time. Best option is Option #5. More lanes will leave less cars waiting on Wurzbach as widening the road will give cars turning left or right easier access.	Comment noted.
146	Sylvia Weise Griffith	9/11/17	Comment Form	146a. Option #5, at-grade improvements, is best. Start with this less costly, less destructive solution.	Comment noted.
				146b. Reset the timing of the traffic signals to accommodate east-to-west traffic, which is heaviest. North-to-south traffic on NW Military is not nearly as bad.	Signal adjustment will be implemented with the new construction for any option selected.
				146c. Addition of a signal at Fairfield Bend would help tremendously.	Please see response to comment #5b.
				147a. Thank you for the opportunity to comment on the proposed traffic improvements for this stretch of Wurzbach Parkway. I live in this area of San Antonio and use this roadway frequently. I also represent over 2,000 San Antonio Sierra Club members. I attended the August 30th Open House at the Jewish Community Center, reviewed displays and discussed them with staff. I have reviewed online documents. I summarize my comments as follows: 1. The least bad alternative, which we would favor, is Option #5, at-grade improvements.	Comment noted.



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 Open House #2 (8-30-17) Comments

147	Terry Burns	9/9/17	Email	147b. None of the options presented for the Wurzbach/NW Military intersection is acceptable. By 2030, even the modified SPUI shows total interchange LOS of D in AM, and E in PM. This will be achieved over less than 10 years from construction completion, for \$28.5 million. This is a poor return on investment.	Comment noted. Please also see response to comment #87h.
				147c. SPUI with signals at that interchange achieves total LOS of C in AM and PM, for \$29 million, over the same time period.	Comment noted. Please also see response to comment #87h.
				147d. All other alternatives do far worse. However, the at-grade improvements (Option #5) at that Interchange show a 70% reduction in Total Interchange Delay in AM, and a 68% reduction in PM. This is achieved for a cost of \$7.5M. This is a much better return on investment than others. SPUI, by contrast, achieves 86% and 91% reductions, for \$29M.	Comment noted. Please also see response to comment #87h.
				147e. All of the displayed alternatives add traffic lanes (except Option #6, No Build). NW Military is generally shown with 10 lanes at the interchange, Wurzbach Parkway with 11. This will create a huge interchange right in these neighborhoods. This follows the tradition of building roads THROUGH neighborhoods, destroying them in the process. It follows the tradition of counting SECONDS of delay at an intersection as of paramount importance.	Comment noted. Please also see response to comment #87h. The build options would not displace any homes or businesses.
				147f. The shown provisions for bicycle and pedestrian traffic would be laughable if they weren't so frightening. Tracking the tiny bike lanes through the spaghetti of high speed traffic lanes is daunting. There are no safety studies provided, and there is nothing here at all to suggest that this project intends to do anything EXCEPT speed traffic through this interchange. This is totally at odds with COSA efforts for a Vision Zero pedestrian and bicycle safety record. There is nothing SMART STREETS about this plan.	See response to comment #102c. TxDOT encourages design elements that enhance safety for all modes of transportaion.
				147g. There is also NO provision for any transit options. Presumably buses mixed with the traffic will benefit equally from the improved LOS.	Existing bus stops and other facilities will remain and specific design accommodations will be considered at the final phase of project design. Currently, VIA operates one bus route north/southbound on NW Military, and one route between NW Military and Lockhill-Selma.



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				<p>147h. It is interesting to note, also, that the Wurzbach/Lockhill-Selma interchange shows NO benefits from the Wurzbach/NW Military Interchange improvements. In 2030, regardless of alternative, PM LOS at Lockhill-Selma is graded "F." AM LOS is graded "E" for all but the no-build alternative.</p>	<p>The intersection of Wurzbach with Lockhill-Selma shows significant reduction in delay for the 2030 analysis year as compared to the No Build scenario. While the LOS in 2030 only improves from F to E (AM peak hour), and does not improve in the PM peak hour, there is approximately 63% reduction in congestion delay in the AM peak hour and 33% reduction in the PM peak hour.</p>
				<p>147i. I see nothing detailing traffic speeds or signal functions. I suspect that there is a built-in assumption that there will NEVER be a walk signal activated, or a bicyclist needing safe passage.</p>	<p>Pedestrian demand was measured and modeled (including needed signal phases) in the schematic phase of this project. Final signal timing, including those for pedestrian phases, are developed during the final phase of design, and will look to optimize pedestrian calls as they relate to the high vehicular volume at this intersection.</p>
				<p>147j. The Alamo Sierra Club does not support yet another plan to add more traffic lanes, and maybe a couple more underpasses, to get a few more years of better traffic flow before returning to the same situation again, after millions of dollars spent. It is past time that COSA and TxDOT should abandon this never ending spiral of road construction leading to more traffic, leading to more construction. I am familiar with the headaches involved at this intersection at some times of the day. Instead of enduring years of added headache with construction, to gain less than ten years of mild improvement, TxDOT needs to examine ways to reduce traffic, enhance transit, enhance bike and pedestrian travel, support Vision Zero, build Smart Streets. Real bike lanes that are separate and safe are needed. Intersections that are designed FOR safe pedestrian and bike crossing are needed. Dedicated transit lanes are needed.</p>	<p>Comment noted.</p>
148	Eric Weis	9/8/17	Email	<p>I wanted to make sure my voice is heard. After reviewing all the options, my vote would be for Option #1.</p>	<p>Comment noted.</p>
149	William and Melissa Dewey	9/7/17	Email	<p>149a. We feel that the best option for the intersection as proposed is Option #1, SPUI with signals. The only negative for Option #1 is that there is no ability to leave the Summerfield neighborhood exit off of Wurzbach Parkway, or stores between neighborhood exit off of Wurzbach and NW Military, and go west on Wurzbach over NW Military; there is a forced right or left onto NW Military.</p>	<p>Comment noted.</p>



PA (Principal Arterial) 1502, from Lockhill-Selma Road to NW Military Highway
 Open House #2 (8-30-17) Comments

				149b. Thus, Option #2 (Modified SPUI with Frontage Roads) with signals would be best, but that does not appear to be an option. I do feel that having the proposed lights at Fairfield Bend and Alon several months prior to the construction would be beneficial for traffic flow purposes.	Comment noted.
150	Dixie Doodle	9/7/17	Email	I am in favor of Option #1, but only if it does not have a tunnel. I would like an overpass bridge instead of any tunnel. Tunnels flood and make unsafe passage.	An overpass was analyzed and the design did not fit within the existing right of way.
151	Michael C. Griffith	9/7/17	Comment Form	I drive through this intersection an average of twice daily, seven days a week. I support Option #5, along with a new traffic light at Fairfield at NW Military. I am concerned that "flyover" or "tunnel" options will only move the traffic problem to Lockhill-Selma at Wurzbach, still backing up to NW Military.	Please see response to comment #87h.
152	Alicia and Alberto Rubio	9/6/17	Comment Form	We desperately need help to safely get out of our neighborhood, Summerfield. Please consider adding a traffic light on Fairfield Bend and NW Military.	Please see response to comment #5b.
153	Susan Graham	9/6/17	Email	153a. Option #1 will work best. I have lived in the area 21 years, and the traffic congestion problems are severe. This appears the only option that will give relief.	Comment noted.
				153b. Option #2 is my second choice.	Comment noted.

APPENDIX B

NOTICES



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Open House - Wurzbach Parkway from Lockhill-Selma Road to NW Military Highway

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[Hearings, Meetings and Notices Schedule](#)[Subscribe to Updates](#) [Contact Us](#)

Where: Barshop Jewish Community Center Auditorium
12500 NW Military Drive
San Antonio, TX 78231 ([Map](#))

When: Wednesday, Aug. 30, 2017
6:00 - 8:00 p.m.

Purpose: The purpose of the open house is to gather public input on the proposed transportation improvements along Wurzbach Parkway from Lockhill-Selma Road to NW Military Highway, including alternative intersection improvements at Lockhill-Selma and NW Military intersections, in San Antonio, Bexar County, Texas. The meeting will be an open house format so citizens may come and go at their convenience, and staff will be available to answer questions.

Description: The purpose of the open house is to gather public input on alternative intersection improvements.

Special accommodations: TxDOT makes every reasonable effort to accommodate the needs of the public. The open house will be in English, if you have a special communication accommodation or need for an interpreter, a request can be made. If you have a disability and need assistance, special arrangements can also be made to accommodate most needs. Please call (210) 901-5124 by Aug. 23, 2017, to make a request. Please be aware that advance notice is requested as some accommodations may require time for TxDOT to arrange.

All comments must be received or postmarked by Thursday, September 14, 2017.

The environmental review, consultation and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by TxDOT pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 16, 2014, and executed by FHWA and TxDOT.

Downloads:

- [Notice](#)
- [Notice \(Español\)](#)
- [Presentation](#)
- [Exhibit Boards](#)
- [Option 1 Schematic – SPUI](#)
- [Option 2 Schematic – SPUI Modification \(with frontage roads\)](#)
- [Option 3 Schematic – Diamond Interchange](#)
- [Option 4 Schematic – Partial Grade Separation](#)
- [Option 5 Schematic – At-Grade Improvements](#)
- [Option 6 Schematic – No-Build](#)
- [Comment Form](#)

TxDOT San Antonio District
4615 NW Loop 410
San Antonio, TX

(210) 615-1110
[Email](#)

Posted Aug. 7, 2017



Notice of Open House
Wurzbach Parkway from Lockhill-Selma Road to NW Military Highway

The Texas Department of Transportation (TxDOT), San Antonio District will host an Open House to present possible transportation improvements along Wurzbach Parkway from Lockhill-Selma Road to NW Military Highway, including alternative intersection improvements at Lockhill-Selma and NW Military Intersections, in San Antonio, Bexar County, Texas. The Open House will be held on Wednesday, August 30 from 6:00 p.m. to 8:00 p.m. at the Barshop Jewish Community Center Auditorium located at 12500 NW Military Drive, San Antonio, TX 78231.

The purpose of the open house is to gather public input on alternative intersection improvements. All interested citizens are encouraged to attend the Open House to view the presented information, provide comments, and ask questions. No formal presentation will be made. Written comments can be emailed to Ibette.Cavazos@wsp.com or mailed to Ibette Cavazos at WSP, Bank of America Plaza, 300 Convent St., Suite 1330, San Antonio, TX 78205. All comments must be postmarked or submitted by Thursday, September 14, 2017.

The Open House will be conducted in English. Persons interested in attending the meeting who have special communication or accommodation needs, or need an interpreter, are encouraged to contact Ibette Cavazos at (210) 901-5124. Requests should be made at least five days prior to the Open House. Every reasonable effort will be made to accommodate these needs.

If you have any general questions or concerns regarding the project, please contact Richard De La Cruz, P.E. at (210) 615-6434 or Richard.DeLaCruz@txdot.gov.

The environmental review, consultation, and other actions required by applicable Federal environmental laws for this project are being, or have been, carried out by TxDOT pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 16, 2014, and executed by FHWA and TxDOT.

Exhibición Abierta al Público

Mejoras de Movilidad a Wurzbach Parkway de Lockhill-Selma hasta NW Military Highway

El Departamento de Transporte de Texas (TxDOT), Distrito de San Antonio patrocinara una exhibición abierta al público respecto a posible mejores de transporte por Wurzbach Parkway de Lockhill-Selma hasta NW Military Highway, incluso alternativas mejores en intersecciones en Lockhill-Selma y NW Military en San Antonio, Bexar County, Texas. La exhibición abierta al público se llevara a cabo el miércoles 30 de agosto del 2017 de las 6:00 p.m. a las 8:00 p.m. en la sala del Barshop Jewish Community Center localizada en 12500 NW Military Drive, San Antonio, TX 78231.

La exhibición abierta al público será una oportunidad para que el público comenta respecto a los posible mejores de transporte. Se anima a todos ciudadanos interesados que asista a la exhibición abierta al público para obtener información, expresar sus pensamientos y hacer preguntas. No se llevara a cabo una presentación formal. Comentarios por escrito pueden ser remitidos por correo electrónico a ibette.cavazos@wsp.com o correo a Ibette Cavazos, WSP, Bank of America Plaza, 300 Convent St., Suite 1330, San Antonio, TX 78205. Se requiere que todo comentario se somete o este matasellado antes o el día de jueves 14 de septiembre del 2017.

La exhibición será conducida en Ingles. Personas interesadas en atender la audiencia que tienen necesidades de comunicación o de alojamientos especiales, o necesitan un intérprete, se les solicita que se pongan en contacto con Ibette Cavazos al tel. (210) 901-5124. Las solicitudes deben hacerse al menos cinco días antes de la Junta. Todo esfuerzo razonable se hará para satisfacer sus necesidades.

Si usted tiene preguntas generales o preocupaciones sobre este estudio, por favor comunicarse con Richard De La Cruz al tel. (210) 615-6434 o Richard.DeLaCruz@txdot.gov.

La revisión ambiental, consultas y otras acciones requeridas por las leyes ambientales federales aplicables para este Proyecto están siendo o se han llevado a cabo por TxDOT – “pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 16, 2014, and executed by FHWA and TxDOT.”



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NATION BRIEFS

NEW YORK

Cadet leader a West Point first

A northern Virginian selected to hold the top position in the chain of command for West Point cadets will be the first African-American woman to do so at the U.S. Military Academy.

West Point this week announced that Cadet Simone Askew of Fairfax, Virginia, will serve as first captain of the Corps of Cadets during the upcoming academic year. Askew, an international history major at West Point, will assume her duties this month.

"Simone truly exemplifies our values of Duty, Honor, Country," Brig. Gen. Steven Gilland, commandant of cadets, said in a release.

PENNSYLVANIA

Nurse filmed nude patients

Authorities say a nurse pleaded guilty Friday to secretly filming unclothed female patients as they underwent medical procedures.

Bucks County prosecutors say 45-year-old James Close admitted he videotaped the women, including a 17-year-old girl, during dermatology treatments at Penn Medicine in Yardley.

A lawyer representing some of the victims said police searched Close's phone and found 18 videos of seven women. Victims' ages ranged from 17 to 60.

From wire reports

AG announces moves targeting leaks, press

TRIBUNE NEWS SERVICE

WASHINGTON — Under pressure by President Donald Trump to stanch disclosures of classified information to the media, Attorney General Jeff Sessions on Friday announced efforts to prosecute those responsible for what he called an "unprecedented rise in leaks" and threatened a more aggressive stance toward journalists.

Sessions revealed no new cases but said the Department of Justice has tripled its leak investigations this year. The FBI, he said, has created a counterintelligence squad to manage them.

He also said he was reconsidering Obama administration policies that limited the information prosecutors could demand from reporters.

"This culture of leaking must stop," he said.

The Obama administration was aggressive in pursuing cases against leakers, pursuing more cases than any other administration. No journalists were prosecuted under President Barack Obama, but prosecutors subpoenaed records, secretly obtained phone logs and pressured reporters to reveal their sources.



Alex Wong / Getty Images

Attorney General Jeff Sessions did not reveal any new cases.

In 2015, then-Attorney General Eric Holder announced that some of the Justice Department's efforts had gone too far. He changed policy to make it more difficult for prosecutors to go after journalists' records.

Those policies are now under review, Sessions said.

"We respect the important role that the press plays," but journalists "cannot place lives at risk with impunity," he said.

Media organizations said the administration was trying to use the law to stop reporters from doing their jobs.

"What the attorney general is

suggesting is a dangerous threat to the freedom of the American people to know and understand what their leaders are doing, and why," said David Boardman, chairman of the Reporters Committee for Freedom of the Press.

Since Trump took office, news organizations have revealed a secret foreign intelligence warrant regarding a Trump adviser, Trump's Oval Office conversations with senior Russian officials a day after he fired FBI Director James Comey and other usually closely guarded secrets.

In the latest embarrassment for Trump, the Washington Post published transcripts Thursday of his rocky post-inauguration telephone conversations with the leaders of Mexico and Australia.

Although those disclosures have politically embarrassed Trump, administration officials have not claimed that any "place lives at risk," as Sessions put it.

Trump was a fan of leaks against his opponent during the presidential campaign, even at one point publicly encouraging Russian hackers to try to obtain Hillary Clinton's emails.

But since his inauguration, Trump has frequently raged about unauthorized disclosures.

Mueller wants records on Flynn

NEW YORK TIMES

Investigators working for special counsel Robert Mueller recently asked the White House for documents related to former national security adviser Michael Flynn and have questioned witnesses about whether he was secretly paid by the Turkish government during the final months of the presidential campaign, people close to the investigation said.

Although not a formal subpoena, the document request is the first known instance of Mueller's team asking the White House to hand over records.

In interviews with potential witnesses in recent weeks, prosecutors and FBI agents have spent hours poring over the details of Flynn's business dealings with a Turkish-American businessman who worked last year with Flynn and his consulting business, the Flynn Intel Group.

The company was paid \$530,000 to run a campaign to discredit an opponent of the Turkish government who has been accused of orchestrating last year's failed coup in the country.

Investigators want to know if the Turkish government was behind those payments and if the Flynn Intel Group made kickbacks to the businessman, Ekim Alptekin, for helping conceal the source of the money.

New lawmakers take over in Venezuela

WASHINGTON POST

CARACAS, Venezuela — Amid the blaring sounds of socialist anthems, hundreds of newly elected pro-government lawmakers triumphantly

entered the Federal Legislative Palace on Friday, sending up victory whoops on a day critics called a death blow for democracy in Venezuela.

Their entry into the neoclassical complex

marked the inauguration of an all-powerful law-making body elected Sunday in a vote that has been condemned internationally. Opposition lawmakers elected in 2015, and now shunted aside, decried the new body as a puppet

Congress installed by President Nicolás Maduro.

As members of the new Constituent Assembly entered the building, they held portraits of the late Hugo Chávez side by side with images of Maduro, his anointed successor.

NOTICE OF PUBLIC MEETING TO DISCUSS BUDGET AND PROPOSED TAX RATE

The Northside Independent School District will hold a public meeting at 7:00 PM, August 15, 2017 in Board Room, Central Administration Office Building, 5900 Evers Road, San Antonio, Texas 78238. The purpose of this meeting is to discuss the school district's budget that will determine the tax rate that will be adopted. Public participation in the discussion is invited.

The tax rate that is ultimately adopted at this meeting or at a separate meeting at a later date may not exceed the proposed rate shown below unless the district publishes a revised notice containing the same information and comparisons set out below and holds another public meeting to discuss the revised notice.

Maintenance Tax	\$1.040000/\$100 (proposed rate for maintenance and operations)
School Debt Service Tax	
Approved by Local Voters	\$0.335500/\$100 (proposed rate to pay bonded indebtedness)

Comparison of Proposed Budget with Last Year's Budget

The applicable percentage increase or decrease (or difference) in the amount budgeted in the preceding fiscal year and the amount budgeted for the fiscal year that begins during the current tax year is indicated for each of the following expenditure categories.

Maintenance and operations	4.12% increase
Debt Service	5.74 % increase
Total expenditures	4.37% increase

Total Appraised Value and Total Taxable Value (as calculated under Section 26.04, Tax Code)

	Preceding Tax Year	Current Tax Year
Total appraised value* of all property	\$54,703,939,253	\$59,414,984,594
Total appraised value* of new property**	\$1,748,568,250	\$2,070,930,884
Total taxable value*** of all property	\$46,863,190,583	\$50,817,258,835
Total taxable value*** of new property**	\$1,661,455,983	\$1,993,598,195

*Appraised value is the amount shown on the appraisal roll and defined by Section 1.04(8), Tax Code.

** "New property" is defined by Section 26.012(17), Tax Code.

*** "Taxable value" is defined by Section 1.04(10), Tax Code.

Bonded Indebtedness

Total amount of outstanding and unpaid bonded indebtedness* \$2,211,840,000

*Outstanding principal.

Comparison of Proposed Rates with Last Year's Rates

	Maintenance & Operations	Interest & Sinking Fund*	Total	Local Revenue Per Student	State Revenue Per Student
Last Year's Rate	\$1.040000	\$0.335500*	\$1.375500	\$6,316	\$2,917
Rate to Maintain Same Level of Maintenance & Operations Revenue & Pay Debt Service	\$1.101800	\$0.336070*	\$1.437870	\$6,804	\$2,628
Proposed Rate	\$1.040000	\$0.335500*	\$1.375500	\$7,011	\$2,628

*The Interest & Sinking Fund tax revenue is used to pay for bonded indebtedness on construction, equipment, or both.

The bonds, and the tax rate necessary to pay those bonds, were approved by the voters of this district.

Comparison of Proposed Levy with Last Year's Levy on Average Residence

	Last Year	This Year
Average Market Value of Residences	\$207,071	\$218,540
Average Taxable Value of Residences	\$178,468	\$190,694
Last Year's Rate Versus Proposed Rate per \$100 Value	\$1.375500	\$1.375500
Taxes Due on Average Residence	\$2,454.83	\$2,623.00
Increase (Decrease) in Taxes		\$168.17

Under state law, the dollar amount of school taxes imposed on the residence homestead of a person 65 years of age or older or of the surviving spouse of such a person, if the surviving spouse was 55 years of age or older when the person died, may not be increased above the amount paid in the first year after the person turned 65, regardless of changes in tax rate or property value.

Notice of Rollback Rate: The highest tax rate the district can adopt before requiring voter approval at an election is \$1.376074. This election will be automatically held if the district adopts a rate in excess of the rollback rate of \$1.376074.

Fund Balances

The following estimated balances will remain at the end of the current fiscal year and are not encumbered with or by a corresponding debt obligation, less estimated funds necessary for operating the district before receipt of the first state aid payment.

Maintenance and Operations Fund Balance(s)	\$176,077,417
Interest & Sinking Fund Balance(s)	\$37,385,640



Notice of Open House

Wurzbach Parkway:
From Lockhill Selma Road to NW Military Highway

The Texas Department of Transportation (TxDOT), San Antonio District will host an Open House to present possible transportation improvements along Wurzbach Parkway from Lockhill-Selma Road to NW Military Highway, including alternative intersection improvements at Lockhill-Selma and NW Military Intersections, in San Antonio, Bexar County, Texas.

LEARN MORE AT THE OPEN HOUSE
Date: Wednesday, August 30, 2017
Time: 6:00 p.m. – 8:00 p.m.
Location: Barshop Jewish Community Center
 12500 NW Military Drive
 San Antonio, TX 78205

The Open House will be conducted in English. Persons interested in attending the meeting who have special communication or accommodation needs, or need an interpreter, are encouraged to contact Ibetta Cavazos at (210) 901-5124. Requests should be made at least five days prior to the Open House. Every reasonable effort will be made to accommodate these needs.

Submitting Comments
 All comments must be received or postmarked by **Thursday, September 14, 2017** for inclusion in the public meeting summary using one of the following methods:

- Complete a written comment card at the meeting
- Submit by mail: Ibetta Cavazos at WSP, Bank of America Plaza, 300 Convent St., Suite 1300, San Antonio, TX 78250
- Submit by email: Wurzbach@wsp.com

Project Contact Information
 If you have any general questions or concerns regarding the project, please contact Richard De La Cruz, P.E. at (210) 615-6434 or Richard.DeLaCruz@txdot.gov.

The environmental review, consultation, and other actions required by applicable Federal environmental laws for this project are being, or have been, carried out by TxDOT pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 16, 2014, and executed by FHWA and TxDOT.



Anuncio de Exhibición Abierta al Público

Wurzbach Parkway:
de Lockhill Selma Road hasta NW Military Highway

El Departamento de Transporte de Texas (TxDOT), Distrito de San Antonio patrocinara una exhibición abierta al público respecto a posible mejores de transporte por Wurzbach Parkway de Lockhill-Selma hasta NW Military Highway, incluso alternativas

Detalles de la Exhibición
Fecha: miércoles 30 de agosto del 2017
Hora: 6:00 p.m. – 8:00 p.m.
Lugar: Barshop Jewish Community Center
 12500 NW Military Drive
 San Antonio, TX 78205

La exhibición abierta al público será una oportunidad para que el público comenta respecto a los posible mejores de transporte. Se anima a todos ciudadanos interesados que asista a la exhibición abierta al público para obtener información, expresar sus pensamientos y hacer preguntas.

La exhibición será conducida en Ingles. Personas interesadas en atender la audiencia que tienen necesidades de comunicación o de alojamientos especiales, o necesitan un intérprete, se les solicita que se pongan en contacto con Ibetta Cavazos al tel. (210) 901-5124. Las solicitudes deben hacerse al menos cinco días antes de la Junta. Todo esfuerzo razonable se hará para satisfacer sus necesidades

Compartir Comentarios
 Todos los comentarios deben ser recibidos o matasellados antes o el día de **jueves 14 de septiembre del 2017**. Los comentarios pueden ser entregados utilizando uno de los siguientes métodos:

- Completa una tarjeta de comentario en la exhibición abierta al público
- Envía por correo: Ibetta Cavazos a WSP, Bank of America Plaza, 300 Convent St., Suite 1300, San Antonio, TX 78250
- Envía por correo electrónico: Wurzbach@wsp.com

Información de Contacto
 Si usted tiene preguntas generales o preocupaciones sobre este estudio, por favor comunicarse con Richard De La Cruz al tel. (210) 615-6434 o Richard.DeLaCruz@txdot.gov.

La revisión ambiental, consultas y otras acciones requeridas por las leyes ambientales federales aplicables para este Proyecto están siendo o se han llevado a cabo por TxDOT – "pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 16, 2014, and executed by FHWA and TxDOT."

APPENDIX C
SIGN-IN SHEETS



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
Colt Osburn	Ci			
Jim Rohr	Homeowner	12110 Wild Rose ^{Hill}		neighbor/signs
Eileen Uram	Homeowner	11930 TOKYO		neighbor/sign
Ven Konev	Homeowner	2614 Galit Cv		HOA
Maria L Garcia	Homeowner	13747 Castle Grove Dr		neighbor sign
CHRIS AMAKER	Homeowner	12114 Rossmore Drive		HOA
Jerry Yost	Homeowner	133 Turnberry Way		HOA
ED TAYLOR	"	14310 Fox FIRE		COM HOA
Nim Meishar	"	14119 oakstead		Electronic sign
Steve Choy	homeowner	12706 U.dove Viste		Sis 2



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
Donna Freeman	Homeowner	2919 Hunters Stream		from previous attendance
CJ Freeman	"	" " "		" " "
Joel Griffith	"	11923 Sandbar Hl		HOA
Heart Cameron	"	2727 Zurich		HOA
Saul Levenshuc	JCC representative			JCC
Christi Morrison	Homeowner	118 Regents Park		HOA
Sandy Byler	"	10223 Dreamland		Neighbor
Mary Ann Dana	Homeowner	14035 Oakhill Way		Jcc
ROGER MACIAS	HOMEOWNER	2711 CALEDONIAN CT.		HOA
Tim Scholz	Homeowner	3007 Iron Stone Ct		HOA



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
DAVID BRIGMIEZ	1711	1711 Tarkenton Ln	[REDACTED]	TX Public
Brian Purcell				
MICHAEL BENEK		3030 WHISPER FERO ST		EMAIL
Katayoun Bahrami-Golub				Community
Lori Boies		12039 Indigo Bend 78230		Community
CJ Frerin		2919 Hunter Stream 78230		Email
Gina Meishar	Local Resident	14119 Oakstead 78231		TX DOT / community
Dean Peck	Resident Alon	2806 Wandering Dr. 78230		
Shelley Baillargeon	Resident	23 Inverness Blvd		NOTICE
KARI Kent Schultz	Resident Elm Creek	3007 Iron Stone Ct		mail



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
Jayanti Konebu	Home owner	2614 Calit Cove SA TX 78230		HOA
Loren Marie	Homeowner	12114 Rossmore Dr. SA 78230		HOA
Kevin McLaughlin	Home Owner	11918 Sandbar Hill 78230		HOA
BRAD PEEL	u	15626 DUNE MEADOW 78248		
Marjorie Lucey	Home owner	12835 Castle Bend St 78230		Newspaper
Patti Morehouse	Elm Creek HOA			email
Patrick Wilks	Homeowner	2311 Dunmore Hill 78230		letter
Margaret				
JAMES P. CARNEY	Home Owner	14311 FOX FIRE LANE 78231		LETTER
Jody Forney	Home Owner	2330 Dunmore Hill		Email



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
MELISSA DOWELL		2719 SANTA PARK		
JAN KURZ		2802 Wonder view		
JIM KURZ		" " "		
Mark Johnson				
CARMEN GONZALEZ		11926 TOKYO 98230		my neighbor
Shivani Kuparel		2703 Caledonia Ct 78230		HOA meeting
Jennifer Mansour		11922 Sandbar Hill		
JAKE RITCHIE		14655 Tripe @ NW ^{Ln}		OT
Katsy Morris		33 Villa Verde		Received letter
Cindy + Patrick Willes		2311 Dunmore Hill		Flyer



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
CHRIS				
BARRY SPIGEL		14110 OAKSTEAD ST		MAILING
LAWRENCE FAGEN		11926 SANDBAR HILL		
Thomas Dowell		2719 Sonata Park 78230		HOA
Wendy Mendez	ALON	2335 Danmore Hill		HOA
Linda Rohr	ALON HOMEOWNER	12110 WILDOSE HILL		HOA
Aston Bennia	ALON ESTATES	2611 Galit Co		net HOA
Jeffrey D Hoefle	ALON	2738 Wonderview Dr		HOA
CARL SALVATO	ALON	11911 Sandbar Hill		
David Manser	ALON	11922 Sandbar Hill		



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
Chris Corbett	Summerfield	13123 Winding Ck		email
Alex John Poston	RESIDENT	2906 HUNTERS HORN		walk by
Jennifer Acosta	resident	13919 Cedar Canyon		walk in
M.E. HUBBINGER	RESIDENT	108 Turkey Creek Rd 78231		Signage
Rave Adams	"	414 Cliffside		phon call
J.H. Schumann	Business	34 Tilbury		—
Alex Titerle	Resident	—		—
Tammy Martin	Resident	12126 Wildrose Hill		



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
Mary B Gonzalez	—	SATX 78204 8038 Broadway Apt 110 H		Friend
Scott Bishop		15150 Blanco Rd Apt 14310		e-mail
Peter Geltman		12730 Hunters Ledge 78230		.com "
Leslie Harlan	SAMCO	13526 Gessy Rd #107		Message Board
Garin Gray		13113 Hunters Ledge 78230		email
Jeff Tondre		12946 County Pkwy San Antonio		email
Victo Victoria Vela	Alon Estates	2715 Mossgrove Park SA, TX 78230		message board, Friend, neighbors
Ernesto Vela	Estates of Alon	2715 Mossgrove Park SAT 78230		" - "
ADAM SANCHEZ	Alon Estates	12002 SANDSTAR Hill SATX 78230		com A-0A
Deuse Sanders	Summerfield	14310 Hill Run SA 78231		sign & mail



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
Clay Smith	VIA			TxDOT
Jessica Hart	neighborhood	—		sign
James Casey	Summerfield	—		Wtq.
Kieley Praeger	Whispering Oaks			n
Neil Morgan	Alon			-
Mary K. McKinney	neighbor			mail
Jorge A Sanchez	✓			n ✓
Jorge Sanchez	Summerfield			
Stacie Schroeder	SummerField			com mail
Sherry Gilbert Cruz	Oak Meadow			n media



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
RAY + KAREN REUS		3522 MILLGROVE 78230		MESSAGE BOARD
Mark & Linda Schmeltekopf		18623 Castellani 78258		Inverness HOA
victor + Margie Joffe		2435 Dunmore Hill SAT 78230		Estates of Alon HOA
Louis & Emma Terrazas		11542 Whisper Breeze SAT 78230		Whispering Oaks News Letter
Eunice & Dean Pledge		2806 Wondervior DR SATX 78230		Estates of Alon
W/H Handlin		12114 wildrose Hill 78230		estates of Alon
Joe Day		11930 Sandbar Hill		Estates of Alon
BAN & SUKANYA LEAN		12014 Sandbar Hill		Alon HOA
Alberto Cepeda		2326 Dunmore Hill		Neighbors
PATSY KUENTZ		13031 HUNTERS RIDGE		Neighbors



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
Guillermo Portillo-Ramirez	Homeowner	7211 Norman Ln San Antonio TX 78290		email
Jo Runcie	Homeowner	14338 Hill Prince ⁷⁸²⁴⁸		
Brian Davis	"	3530 Hunters Glade St ⁷⁸²³⁴		from email
ART REINHARDT	CoSA TC			by
Jaime Gutierrez	Homeowner	2734 Wonderview Dr. San Antonio, TX 78230		Neighbor
Harold L Dillashaw	Retired TxDot Employee	2555 NE Loop 410 #2211 San Antonio, TX 78217		Message
Charlie Naiser	Homeowner	12130 Wildrose HZ San Antonio TX 78230		email
Armida Cepeda	Homeowner A. Cepeda	2326 Dunmore Hill San Antonio, TX 78230		e-mail
Alan Kuentz	Resident	13031 Hunters Ridge SA 78230		
Edgar Kraus	Homeowner	11602 Mill Rock Rd SA 78230		email



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
CARLOS ORTIZ		211 Box Oak		Newspaper
Katherine Bowman		12114 Wildrose Hill		com HOA
JAMES PLUMMER		2715 CALEDONIAN CT		HOA
Burt Holland		10003 NW Military #2213 S A TX 78231		word sign
JOHN TROLLINGER		2318 Dunmore Hill S.A. TX. 78230		Neighbor
Jason Rodriguez				
Matthew + Yolanda Patti		2431 DUNMORE HILL S A TX 78230		SIGN
Timothy Farns		2715 Caledonian Ct S A TX 78230		Neighbor
Marshall D. Hrich		10622 Auldine San Antonio TX 78230		Sign / letter in mail
Alfred White		12022 Sandbar Hill San Antonio TX 78230		neighbor



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
Gail & Melvin Topp		13134 Hunters Brook		emails
ROBERT BRAWNOR		11002 Whisper Ridge		"
Cecilia + Hermann van Bergh		2710 Wonderview Dr.		emails
Anna Meski		2513 Dunmore Hill		email
Jack Nichols		2714 Wonderview Dr		"
Dotie Nichols		" "		"
Robert Wilkes		2735 Wonderview Dr.		email
(DAIG + Mary Bell)		6106 Vance JACKSON		Sign
Marsha Penner		11 Tilbury Ln		email
Bill Chew or It		3102 Whisper Trade		News



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
Aector Cerda	Alon Estates	12010 Sandbar Hill SA TX 78230	[REDACTED]	Alon Estates HOA
PAUL GOVONI		13538 SYRACUSE ST. 78249	[REDACTED]	
Jean Dickman	wo neighborhood	11419 Whipper Dew SATY 78230	[REDACTED]	E mail
Judi Rowland			[REDACTED]	
Libby & Jerry Cherico	Alon Estates	12006 Sandbar Hill	[REDACTED]	HOA
Angel Santiago	Alon Estates	2706 Caladonian Ct	[REDACTED]	email
Jerry McKinney	Hunters Creek	3506 Hunters Circle	[REDACTED]	mail
Jean Hackett	Alon Estates	2735 Wanderview	[REDACTED]	HOA
Marc + Edie Tannenbaum	Inverness	110 Turnberry Way	[REDACTED]	signs + email
Gregory Lawler	ALON ESTATES	11919 Sandbar Hill	[REDACTED]	COM E-MAIL



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
Steven R. Wilson	Local Resident	2527 Hollow Village St. SA TX 78231	[REDACTED]	Email via Summerfield HOA
Dannae Henderson	Local Resident	2210 Pebble Crest SA TX 78231		Rec. letter
Lana Sund	" "	2803 Whisper Fawn SAT 78230		Next Door
DELRAX Christensen	Local Resident	13103 Voelcker Ranch 78231		Sign on Road
Deanie Christensen	Local Resident	13103 Voelcker Ranch Dr. 78231		email/summerfield
Susan Landreth	Vickrey & Assoc.	12940 COUNTRY PKWY SA TX 78216		
Sally Molina	local resident	PO BOX 780194 SA TX 78278		sign on rd
Mike Molina	local resident	PO BOX 780194 SA TX 78278		sign on rd
John Stankus	Dan meadow resident	2511 INDIAN RIDGE DR. S.A. 78231		mail
Irma Santiago	local resident	2706 Caledonia Rd SATX 78230		Next door



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
Hi Leuc	Home owner	2610 Galit CV 78230		Email / Sign
ROBERT DRAPER	HOME OWNER	2410 DAWMORSE HILL 78230		NEIGHBOR
BENVADETTE DRAPER	"	"		"
Gene & Mary Lawson	Home owner	29 Tilbury La		.com
Jan Watts	Home owner	119 UMBRIA		y.net
NAVIN BHATIA	Home owner	120 TURNBERRY WAY		ET By Mail
Helene Merren	Homeowner	2 Westelm Garden		Sign on Street
CARYL OLSON	Homeowner	415 Mt Laurel Dr. 78240		Sign
BEN CHURCH	Resident	13734 Cedar Canyon		Mail / Sign
DAVID McBERTA	COGA-TCI	21 INWOOD POINT 78240		Email



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
Maria Luna		2422 Galit Cove San Antonio TX 78230		Alon Estates HOA
Chris Cummings		12162 Wildrose H. II SATX 78230		Alon Estates HOA
Dwight Henderson	Local Resident	2210 Pebble Crest, Cir. S.A., TX 78231		Street signs
James & Judy Allen	Local Resident	13706 Pebble Oak San Antonio, TX 78231		Newspaper
Marcia Childress	" "	13707 Stoney Hill San Antonio, TX 78231		neighbor
ANTHONY HUTFLES	Local Resident	14247 EMERALD HILL DR SAN ANTONIO TX 78231		NOV SIGN
Heather McNeal	Resident	12915 Legendfield SA TX 78230		mailer
Melissa Unsell	resident	11422 Whisper Green 78230		Street sign
V. GARRISON	resident	2726 Sonata Park SATX 78230		neighbor.
Hector Garcia	Resident	13747 Castle Grove		Street Sign



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
Devisi Smith	Home owner	3426 Hunters ⁷⁸²³⁰ Brook	[REDACTED]	Internet
DAVID LABADIE	Home owner	13110 Hunters Brook	[REDACTED]	mail
Deidre LABADIE	Home owner	13110 Hunters Brook	[REDACTED]	mail
CARRI AVALON	y	2412 Brighton Oaks	[REDACTED]	y
Ken Gindy	Homeowner	108 Regents Park	[REDACTED]	Internet HOA
Roxana De Leon Fuentes	Home owner	2422 Dunmore Hill	[REDACTED]	Next door - HOA ALON
DAVID TRIPLET	RETAIL	9100 IH 10 WEST, #102	[REDACTED]	MAIL
Brian Smith	Home owner	11422 Whisperbree	[REDACTED]	Sign
Michele Hausmann	Alon/Homeowner	10003 NW Military	[REDACTED]	letter
Evelyn Soo	Home owner	2438 Enfield Grove	[REDACTED]	email



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
Sharon Saxer	Summerfield neighbor	2426 Mill Creek Dr 78231		NWOD
Stephen Lindeman	none	35 Villa Verde 78230		
Gretchen Koufs	nearby resident	1015 Arizona Ash ^{CITY} 78232		com word of mouth
Manjiri Akalkotkar	Summerfield	2412 Brighton Oaks 78231		TxDOT
Richard C Serzlick	INVERNESS	114 TURNBERRY WAY SAT 78230		Inverness Newsletter
Sue Stankus	Oak meadow	2511 Indian Ridge Dr. San Antonio 78231		letter
David Martin	Alon Estates	12126 Wildrose Hill SAT 78230		letter
Dexter Soo	Summerfield	2438 Enfield Grove Dr		letter
Olga & Richard Sharer	W. Oaks	11234 W. Spring		Nextdoor App
PATRICIA WND	SUMMERFIELD	13323 GALEVILLE		NEXT DOOR



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Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
David Schneider	Truerecess	90 Turnberry Way		Sisac
Jeffrey Van Slyke	Resident	15710 Stone Tower S. A. TX 78248		Internet
Kimberly E. Duncan	Resident	2423 FAIRFIELD Bend Dr S.A. TX 78231		Letter DPS
JAMES LUTZ	RESIDENT	14111 OAKSTEAD ST SA TX 78231		CMS
Rafa Kandil Cayala	Resident	2722 Sonata Park San Antonio, TX 78231		CMS.
Suneetha Challa	Resident	2722 Sonata Park San Antonio, TX 78230		CMS.
TERRY BRUNS	"	13139 VISTA DEL MUNDO		Tx DOT.GOV
Bill Yater	Resident	3040 Oak Sprink s		



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
Cindy Schmitt	Inverness	90 Turnberry Way	[REDACTED]	
SASKANTA NALLAGATLA	Estates of Alan	2718 SONATA Park	[REDACTED]	
SWAPNA GHATTAMANENI	Estates of Alan	2718 SONATA Park	[REDACTED]	
Grethen Olivares	Summerfield	13540 Charter Road	[REDACTED]	
Michael Frantz	Oak Meadow	14103 Blue Smoke	[REDACTED]	
JAVIS Witt	Whispering Oaks	P.O. Box 780326 SATX 78278		mail
Martha Abad	2618	2618 FAIRFIELD Bend Dr		



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
MARTIN + Nancy Hoeng	Resident	14218 Sage Trl 78231	[REDACTED]	signs
MIKE PECEN	"	2914 HUNTERS DEN '30		can letter
Steven Sanders	Resident	14310 Mill Run		m 1+r



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
Robert Elizondo		11501 Whisper ^{Dew}	[REDACTED]	Newsletter
G B Vega		9607 FAIRFIELD BOWS		.



SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy

AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
Laura				
MICHAEL A. OSBORN	CASTLE HILLS FORES RESIDENT	13022 STONEY HILL ⁷⁰²³¹	[REDACTED]	LETTER FROM RON NIRENBURG
Tina M Jeffers	Sommerfield	[REDACTED]		
JP	1	12502 Georgian Oaks St ^{SATX} 78231	[REDACTED]	



ST AFF

ELECTED OFFICIAL SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy

AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
gina downson	TXDOT			
Scott Armstrong	WSP USA			
PATRICIA T Nolasco	TXDOT			
Mike Gorza	TxDOT			
Kamubh Gogavi	WSP USA			
Taylor Hibbens	TxDOT			
Jonathan Serrano	TXDOT			
Carl Hartline	WSP			
Ruben Jander	WSP			
Richard De La Cruz	TxDOT			



STAFF ELECTED OFFICIAL SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy
AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
Laura Lopez	TXDOT			
NARGA DANOU	WSP			
EDDIE GONZALEZ	TXDOT			
Will Martinez	WSP			
John BEYANT	TXDOT			
Rene U. Garza	WSP			
Jorge L FARIAS	WSP		farias@phoenix.com	
CLAYTON RIPP	TXDOT			
Austin Porlier	TXDOT			
Diana Rogerio	TXDOT			



STAFF

ELECTED OFFICIAL SIGN IN SHEET

Wurzbach Parkway at NW Military Hwy

AUGUST 30, 2017, 6:00 PM

Name	Affiliation	Mailing Address	Email	How Did You Hear About the Open House?
CHRIS GEORGES	COSA TCT		CHRIS.GEORGES@SANANTONIO.GOV	I Just know!
Amy Redman	TxDOT			
Colt Osburn	CC District 9		colt.osburn@sanantonio.gov	email
Bill Hill	CITY OF SHAVANO PARK		bhill@SHAVANOPARK.ORG	
Margarita Hernandez	COSA-TCT			
Dale Picha	TXDOT SAT			
Laura Garza	District 8		laura.garza@sanantonio.gov	
Alex Franklin	District 8			

APPENDIX D
COMMENTS RECEIVED



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: JACIE & DOTIE NICHOLS

ADDRESS: [REDACTED]

REPRESENTING: _____

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: OPTION # 1
Best long term
Strategic solution

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
 Email: wurzbach@wsp.com
 Mail: WSP USA
 Attn: Ibetta Cavazos
 Bank of America Plaza
 300 Convent St., Suite 1330
 San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military

The environmental review, consultation, and other actions required by applicable Federal environmental laws for this project are being, or have been, carried-out by TxDOT pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 16, 2014, and executed by FHWA and TxDOT.



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Susan Landreth

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: No, I is preferred option.

Provides best improvement for traffic flow

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibette Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Maria L. Garcia

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: I like Option 1

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibette Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Heather McNeal

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: I like the SPUI option, but
I see the appeal for SPUI w/ Frty Rds.

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibetta Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Sharon Saxer

ADDRESS: 

REPRESENTING: —

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Prefer #1 because of stop light on NW
Military for Summerfield. Option #2 is good IF
we get the light on NW Military for the
Summerfield exit. Summerfield MUST have a
safer way to exit our neighborhood!

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
 Email: wurzbach@wsp.com
 Mail: WSP USA
 Attn: Ibetta Cavazos
 Bank of America Plaza
 300 Convent St., Suite 1330
 San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Robert Wilkes

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Need traffic signs @ NW Military & Alton Loop
and at NW Military & Fairfield Bend.
Option #1 is my preference.

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibette Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military

The environmental review, consultation, and other actions required by applicable Federal environmental laws for this project are being, or have been, carried-out by TxDOT pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 16, 2014, and executed by FHWA and TxDOT.



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Jean Hackett

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: - Please complete the land bridge before starting this project as that would be less disruptive to wild life.
- The traffic signals in Option 1 are needed regardless of which plan is chosen.

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibette Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search Wurzbach Parkway at NW Military

The environmental review, consultation, and other actions required by applicable Federal environmental laws for this project are being, or have been, carried-out by TxDOT pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 16, 2014, and executed by FHWA and TxDOT.



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Sue Stankus

ADDRESS: [REDACTED]

REPRESENTING: myself

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: I like option 1, with the traffic
signals added at the neighborhoods,
it gives everyone the ability to get
out of their neighborhoods. This is
by far the best option.

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibetta Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: John Stanekus

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: OPTION #1 SPUT by fan
LOOKS BEST TO ME, THIS GIVES
MUCH BETTER THROUGHPUT THAN OTHER
OPTIONS
LESS IDLING CARS BETTER FOR
ENVIRONMENT

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibette Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: JAMES P. CARNEY

ADDRESS: _____

REPRESENTING: _____

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: OPTION #1 IS THE BEST OPTION.

THE PROBLEM IS WITH ALON, INVERNESS, & SUMNERFIELD
THEY ARE FOR THE MOST PART AGAINST THIS.

WE MOVED HERE WHEN NW MILITARY WAS 2 LANES. WE
KNEW IT WOULD GROW OUT HERE AND NEED TO HAVE BETTER TRAFFIC FLOW
PEOPLE WHO MOVED TO INVERNESS & ALON WHEN IT WAS A
TRAFFIC NIGHTMARE WHEN THEY BUILT. THEY ARE TRYING TO
KEEP IMPROVEMENTS FROM HAPPENING THAT WOULD BENEFIT PEOPLE
WHO HAVE LIVED HERE 40 YEARS

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Attn: Ibetta Cavazos

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San Antonio, TX 78205



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Wurzbach Parkway at NW Military



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Evelyn Soo

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
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- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: I like #1

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Eileen Uram

ADDRESS: 

REPRESENTING: 

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: I endorse Option # 1.

I would also like to have a traffic light on NW Military at the entrance to Woods of Alon and also servicing the entrance to Hardberger Park.

Deadline for Comments: September 15, 2017

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Charlie Naiser

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
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- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Option #1 would be preferred if there
was option to go EB on Wurzbach from Estates
of Alan. There is sufficient room to still allow
EB access and there are natural breaks due to
light @ Lockhill to merge into EB traffic.

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Wurzbach Parkway at NW Military



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: JAMES R WATTS

ADDRESS: [REDACTED]

REPRESENTING: _____

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: #1 is the best option long term
However, I personally like option 4 or 5.
I would like the city or TX DOT to consider
the traffic light @ Inverness Blvd
Regardless of the option selected.

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Wurzbach Parkway at NW Military



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: CARMEN GONZALEZ

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: LIKE OPTION 1 - IT FIXES PROBLEM
ONCE + FOR ALL! MY CONCERN IS GETTING
OUT OF MY SUBDIVISION ON TO MW MILITARY
IS GOING TO BE BAD BECAUSE OF ALL THE ADDITIONAL
TRAFFIC CREATED BY THE PROJECT, I
PROPOSE THAT THE TRAFFIC LIGHT ON
NW MILITARY BE MOVED TO THE WOODS OF ALON / HADBERGER
PARK ENTRANCE
THANK YOU.

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Wurzbach Parkway at NW Military



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Brian Smith

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Go with option 1,
RIP The Bandaid off,
Make sure I can access Wurzbach
Parkway during construction.

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Chris Corbett

ADDRESS: [Redacted]

REPRESENTING: [Redacted]

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- I am employed by TxDOT
I do business with TxDOT
I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: I prefer option 1 for several reasons.

1. It allows the most traffic through the intersection allowing for the most growth.

2. Exiting from Summerfield onto Wurzbach or turning left on to NW Mil it from Fairfield bend currently is very dangerous. So we need a light at Fairfield Bend.

3. Option 2 with a light at Fairfield Bend would be my second choice.

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Bill Hill

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

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- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Strongly Support Option 1

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: JUNE KACHTIK

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: #1 seems the most effective for the cost. While it is the most expensive, it seems to best meet the needs for the longest period of time. (the last items may be "cheap" - but they definitely do not address the problems we are experiencing.

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: PATSY KUENTZ

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: I PREFER OPTION 1. IT KEEPS THE TRAFFIC MOVING THROUGH THE INTERSECTION FASTER. I WOULD LIKE TO SEE ANOTHER OPTION, HOWEVER, THAT ~~IS~~ INCORPORATES ~~ADDING~~ CHANGING SOME LANES ~~IS~~ AT RUSH HOUR THAT GO ONE WAY IN AM RUSH HOUR AND THE OTHER IN PM RUSH HOUR. ALSO, I'D LIKE TO SEE MITIGATION FOR TREES THAT ARE REMOVED IN THIS PROCESS.

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Alon Kuentz

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

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- I am employed by TxDOT
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- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Option 1 is ideal for me.
I get through the intersection fastest
with just 2 lights.
It provides easiest access from Alon
center onto Wurzbach and then L
on NW Military

Deadline for Comments: September 15, 2017

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: _____

ADDRESS: _____

REPRESENTING: _____

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
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- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Option 1 will be long to do but

best prepares for future growth

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Jersey McKinney

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

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- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Prefer Option #1

Deadline for Comments: September 15, 2017

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Valene Garrison

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

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- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Why can't the traffic light ~~at~~
at Alon Loop/ Phil H. be an option
for ~~B&D~~ 2,3,4,5?

Please create open space
at median outside Alon Loop
to gain access to the tunnel.

Is there anyway to create a short
light at Alon Loop to gain access to

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tunnel?
for create a
less
dangerous
exit
from
neighbor
hood.

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From: Harry E Caylor III
To: [28422C WA#3 - NW MILITARY INTER Project Email](#)
Subject: Comment / Vote
Date: Tuesday, September 05, 2017 4:07:48 PM

We are residents of Woods of Alon. Our preference is your recommendation of the SPUI. With this configuration we would also get a traffic light at Alon Loop at NW Military Hwy.

Some members of the Estates of Alon have their panties in a wad because they think they are going to lose their neighborhood access off of Wurzbach, which is NOT the case, based upon your visual presentation. They were probably whipped into a frenzy by some inaccurate information.

Go get 'em. Build it right.

Let me know if I need to do something else to get my vote recorded.

Thanks.

Harry & Victoria Caylor

[Redacted]
[Redacted]

[Redacted]
[Redacted]
[Redacted]

September 8, 2017

Comments concerning
Wurzbach Parkway/NW Military Improvement

Basis of Project:

Purpose of project was to Improve traffic flow and congestion at the Wurzbach / NW Military Intersection only; The problem consists almost entirely of rush hour traffic both in the morning and the late afternoon.

(A) State Jurisdiction: Wurzbach Parkway up to Lockhill Selma Intersection

(B) City of San Antonio jurisdiction:

Lockhill Selma Intersection Road, and Wurzbach west of the intersection

Previous requested efforts:

(A) Requests had been made to the City of San Antonio through our Councilman, now Mayor, to have the City Public Works Dept create plans to improve the traffic lights and other work at both the Vance Jackson/Wurzbach and Lockhill /Wurzbach intersections. I understand that some plans were made.

(B) Requests were made to TxDot to improve the NW Military /Wurzbach intersection

State proposals for the NW Military:

There are six Options presented.

All options reflect much effort by TxDot for the most part, with several suggestions.

Option #1 is best in some respects because it helps regulate the traffic flow down to Lockhill Selma by the use of signal lights at NW Military, which helps avoid west bound congestion problems, by intermittent flow of traffic (especially with the traffic created with the 3rd lane problem due to the horrible third lane proposal down to Lock hill Selma Road.

NOTE: Signal Lights on Wurzbach could also be included on other Options

Option #4 (the partial below grade separation) may be problematic in that there appear to be no drainage studies. I am not saying that there is a problem, but TxDot has made a big mistake before, to wit: on #281 just South of Basse, which has flooded and become impassable about every 5 or so years. There is an even bigger problem on IH #35 below grade under San Pedro and McCullough about every 2 or so years. There are others. There should be a drainage study, just in case.

All Options appear to include a 3rd lane headed West toward from NW military past Lockhill Selma to the to the bridge Lockhill Selma.

The BASIC COMPLAINT concerns the questionable inclusion of the 3rd West bound lane past the Inverness exit, and very ESPECIALLY past Lockhill Selma up to the bridge just before the Elm Creek entrance and Whisper Bow entrance.

I.

Background:

As Whispering Oaks Neighborhood Association President, I worked with other neighborhood representatives, Councilman Thornton, and others concerning the design and development of the Wurzbach Parkway while it was being designed.

We specifically addressed this very issue of traffic congestion for this portion of the Parkway at that time.

In addition, I travel on this section almost every day at various times of day and am familiar with this section.

*** As part of those discussions, I requested, and it was agreed, to add a third east bound lane, which we felt was necessary to help dilute and speed the east bound traffic.

*** It was also discussed and agreed at that time that a 3rd lane on the west bound traffic should not be included, because it would concentrate and further slow the traffic causing congestions just the Opposite effect.

Since those discussions and as part of a separate project, Lockhill Selma and its intersection with Wurzbach was expanded, including a west bound turn lane from South bound Lockhill Selma onto west bound Wurzbach, further adding additional traffic onto Wurzbach.

It should be noted that Whisper Valley also enters onto Wurzbach at or very near this same section location.

**** The RESULT means that the original TWO lanes on Wurzbach, PLUS the ONE south bound TURN Lane from Lockhill Selma onto Wurzbach, PLUS another ONE LANE TURN lane from Whisper Valley onto West Bound Wurzbach traffic ADDS to an existing FOUR lanes, which turn into only TWO West lanes of traffic (with all the resulting congestion at that point.

NOW, the TxDot proposes to add a 3rd lane past the Inverness exit or even just past Lockhill Selma, which means that we now have FIVE Lanes meeting into the original TWO Lanes.

Admittedly there is some congestion for brief rush hour periods of time, and there

are always some who will complain, but the problem is not that bothersome.

The construction project on at the intersection at NW Military by itself should solve most issues.

In addition, the project deletions will save a significant amount of money and construction time .

II.

Further NOTE:

- This is a "Parkway" which means keeping trees and grass, not clear cutting for an unnecessary 3rd lane,
- The deletion of the 3rd lane will also keep much impervious cover for drainage
- These request will also reduce the cost and time for making this improvement.
- The deletion will also help eliminate any thoughts of extending or expanding Wurzbach beyond Lockhill Selma

REQUESTS:

It is requested that

(A) that there be, at least, a brief drainage report for any section of Wurzbach which is below grade level.

(B) that the proposed 3rd lane be deleted in its entirety after the exit from Inverness.

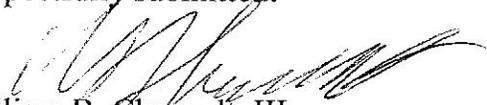
(C) that any 3rd lane and any other related work, or other 3rd lane extensions west of Lockhill Selma be completely deleted and removed from the proposal.

(D) Traffic signals be included at all intersections in the project area;

(E) that the City of San Antonio handle the improvement tot he Lockhill Selma/ Wurzbach intersection.

Respectfully submitted:

Wurzbach@wsp.com


William B. Chenault, III



C3700091417WBCps;2MD: PARKWAY COMMENTS



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: BARRY SPIGEL

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
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- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: PREFERENCE: OPTION 2

- THROUGH-LANE ON ACCESS ROAD IS A GREAT IDEA (SHOULD HAVE PROPOSED THAT INITIALLY!)
- INTERCHANGE ALLOWS GOOD MOVEMENT WHEN TURNING LEFT/RIGHT ONTO MILITARY

Deadline for Comments: September 15, 2017

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: IRWIN M. FLETCHER

ADDRESS: _____

REPRESENTING: _____

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- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: OPTIONS 2 + 3 ARE THE BEST COMPROMISE TO
MOVING TRAFFIC AND KEEPING ADEQUATE ACCESS TO AND FROM
NEIGHBORHOODS. OPTION 1 IS HORRIBLE AND DOES NOT CONSIDER
THE DETRIMENTAL ~~WAA~~ IMPACTS TO EXISTING NEIGHBORHOOD ACCESS.
WITHOUT MORE ~~SEE~~ SIGNIFICANT IMPROVEMENTS ON ~~WEE~~ WURZBACH
FROM LOCKHILL-SELMA TO I-10, THIS ALL MAY BE A WASTE
OF TIME AND MONEY.

Deadline for Comments: September 15, 2017

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Richard C Seneket

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

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- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: PREFER #2 BUT NEED TO ADD
TRAFFIC SIGNAL AT TURNBERRY. SEEMS IF YOU CAN DO
IT AT #4 YOU COULD DO IT AT #2.
MY SECOND CHOICE IS #5. DO NOT
LIKE #1 AS IT ELIMINATES ACCESS TO
INDEMNITY FROM THE EAST.

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibette Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: GOPINATH AKALKOTKAR

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: I PREFER #2.
IT IS A GOOD COMPROMISE FOR LOCAL
+ THROUGH TRAFFIC -

Deadline for Comments: September 15, 2017

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: MANJIRI AKALKOTKAR

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
 I do business with TxDOT *(as a employee of another agency)*
 I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: As a resident of Summerfield
using exit on Wurzbach Parkway going
to I-10 for work commute, I
prefer option 2 - Modified SPUI
with frontage road.
* Need signal at entrance / exit
at NW Military from Fairfield Bend

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: ANTHONY HUTPLES

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: I LIVE OFF OF HUEBUEN RD. I GO THRU THIS INTERSECTION MULTIPLE TIMES DURING THE WEEK. MY CHOICE WOULD BE OPTION 2, SPUI WITH FRONTAGE ROADS. IT APPEARS TO LOWER WAIT TIME AT LIGHTS. SOMETHING NEEDS TO BE DONE SINCE TRAFFIC IS NOT DECREASING. DOING OPTIONS 3 THRU 6 APPEAR TO ACCOMPLISH LITTLE TO HELP TRAFFIC FLOW IN THE FUTURE

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: YOLANDA PATTI

ADDRESS: [REDACTED]

REPRESENTING: _____

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: I AM FOR DOING THE UNDERPASS, WHICH
EVER OPTION THAT INCLUDES. OPTION 2 IS MY
PREFERENCE.

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Stacie Schroeder

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: THANK YOU for presenting us with options !!! I like #2 the best → Modified SUI WITH Frontage Roads. This allows immediate residents to enter + exit their subdivisions like we do now. I would like to see the East + West bound "U" turns start further away from the lights, though. I'm happy with #2 + #3, but would prefer #2.

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: JACQUES BRAKA / ALON TOWN CENTRE

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: 100% FOR No. 2 - MODIFIED SPLIT.
BEST SOLUTION FOR THE LONG TERM.

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Michele Haussmann

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Please build Option #2 Modified
SPUF - the inclusion of eastbound and
westbound frontage roads is extremely
important - I live in Inverness and
represent Alon Town Centre.

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Scott Bishop

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: I think Option #2 is the best.

Option #3 is also good because it still
allows for at grade through movements
along Wurzbach. This allows for transit
service along Wurzbach and be able to
provide stops for neighborhoods and
Alon Shopping Center

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: BILL YATES

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Prefer Opht 2 as I like
fairly access across Military on side lanes
Also, the wider intersection @ clear
turn lanes not crossing.
Still like tunnel under Military
Concerned about traffic increase @
Lochill Selma + Wurzbach

Deadline for Comments: September 15, 2017

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Nicole Boarnet

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
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- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: In my opinion:

① Is terrible. Alon is way too busy to not allow left turn out. The traffic signal won't help. The parking lot already backs up

③ Too many lights. Really save time?

④ & ⑤ worried confusing. Cause accidents
High pedestrian area

Either do nothing #6 or I like #2 best

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Jorge A Sanchez

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

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- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: I like option #3

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Jorge Sanchez Jr.

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: As a Summerfield resident, I ~~would~~ favor
option #3 the most. Doing nothing to Wurzbach
Parkway would be a mistake due to the ever increasing
traffic on the street. #1 and #2 are also respectable
options but I strongly recommend option #3

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Stephen Lindewald

ADDRESS: [REDACTED]

REPRESENTING: _____

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COMMENTS: Adm option #3

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Harold L. Dillashaw III

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: #3 is what's needed. It should have been built like this originally. A young engineer at a public showing of the Wurzbach Parkway said "A bridge of Military over Wurzbach Parkway could not be done because of Lockhill Selma." Yet now it can. Sure.

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: SAVE ADAMS

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: OPTION #4 IS BEST.

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Gerald Cherici

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: I strongly urge going with the #5 At Grade
Improvements option. Constructing a tunnel is much too
disruptive and expensive. We need access to the tunnel
at New Military so I can safely exit our property and get
to my job at USAA. We also require a traffic light
at Alton King at New Military Hwy to enter/exit safely
and access Hardberger Park.

Deadline for Comments: September 15, 2017

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Elizabeth Cherico

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: We strongly prefer #5
At-Grade Improvements.

A tunnel will tremendously
degrade the value of our
property and cause havoc
for our residents

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: LAWRENCE FAGEN

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: # 5 - no tunnel, too
much disruption / dirt, etc

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: MEUSSA DOWELL

ADDRESS: [REDACTED]

REPRESENTING: _____

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: _____

PREFER 5 DUE TO COST /
ENVIRON IMPACT
POSITIVE FOR MUN ESTATES ACCESS

Deadline for Comments: September 15, 2017

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: TJ DOWELL

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: _____

PREFER 5 DUE TO CONSTRUCTION /
COST + ENVIRON IMPACT
POSITIVE ACCESS FOR ALON ESTATES

Deadline for Comments: September 15, 2017

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Chris Amaker

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Option 5 is the right one for this project if you must spend the money. I prefer you do nothing. 1) you are only improving one block if changes are made 2) This is not a top 100 roads project in Texas 3) widening the lanes reduces the natural trees/beauty of the street. 4) this will create more traffic in our home gate entrances off of wurzbach, lastly, we as citizens should meet the companies that will profit off of this project. Who are they related to in the city?

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COMMENT FORM
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August 30, 2017

(PLEASE PRINT)

NAME: Cindy Weeks

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

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- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS:

I vote for #5 - improvements with
free retention in the lease amount
of time & money.

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August 30, 2017

(PLEASE PRINT)

NAME: Carlos R ORTIZ

ADDRESS: [Redacted]

REPRESENTING: [Redacted]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

N/A

- I am employed by TxDOT
I do business with TxDOT
I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Disapprove of 1, 2, 3 + 4 options

Favor #5 because it saves the trees + makes movement of traffic a little easier.

Lower the speed limit on Wurzbach to 55 mph

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COMMENT FORM
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August 30, 2017

(PLEASE PRINT)

NAME: Ernia Pledgers

ADDRESS: [REDACTED]

REPRESENTING: _____

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- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: I oppose the construction, but of the options provided
Option 5 is my families preference.
It is least obtrusive, keeps the median w/ trees
widens the lanes to help improve traffic routes.
Consideration should also be made to timing of
traffic lights, not currently conducive to current
traffic patterns.
Thank you

Deadline for Comments: September 15, 2017

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COMMENT FORM
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August 30, 2017

(PLEASE PRINT)

NAME: Jeffrey D Hoefle, MD

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Prefer Plan #5, recommend
traffic light @ Alon Loop (NW Military)
Hardberger Park and at Inverness /
Summerfield intersection E NW mil.

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibette Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: JOHN TROLLINGER

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
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- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Prefer Option 5 Least Intrusive
but still addresses issue while spending least
amount of money. Would like to see a light at
Alon Loop & NW Military and a light at
Inverness and Summerfield.

Deadline for Comments: September 15, 2017

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San Antonio, TX 78205



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From: Armstrong, Scott D.
To: [28422C WA#3 - NW MILITARY INTER Project Email](#)
Subject: FW: Wurzbach parkway
Date: Friday, September 15, 2017 8:09:44 AM

Scott D. Armstrong, PE
Senior Engineering Manager

-----Original Message-----

[REDACTED]

Scott - Please add to the public summary report.

Thanks

-----Original Message-----

[REDACTED]
Sent: Thursday, September 14, 2017 9:50 PM
To: Richard De La Cruz
Cc: David Mansour
Subject: Wurzbach parkway

Hello Mr. De La Cruz,

I live in Alon Estates. My neighborhood and my house will be directly impacted by the Wurzbach parkway proposed changes. I have a strong opposition to all 4 graded designs. These are the ones whereby a tunnel would be constructed under NW military.

My house backs up directly to Wurzbach so these changes would be directly behind my house.

I think the At Grade design will be the best option for my neighborhood and my home. The existing sidewalk and green space buffer will remain as is.

There would be less noise with this design than the others.

However if this one is done there should be an addition of a light at our northwest military gate entrance across from hardberger park entrance.

Thank you for your time and consideration, Maria Luna and Alejandro Morales

Sent from my iPhone

[Connecting Texans to what matters most. Texas Department of Transportation: 1917-2017 #txdot100]
<<http://www.dot.state.tx.us/txdot100/>>



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Chris Cummings

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

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- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Alon Estates resident prefer
option 5 or 6 to allow access through
NW ~~the~~ Military w/o waiting.

I would like to go through N/W
military w/o waiting.

Deadline for Comments: September 15, 2017

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Wurzbach Parkway at NW Military



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: MATHEW PATTI

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
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- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: OPTION 5 FIRST CHOICE

OPTION 2 SECOND CHOICE

- OR W/ INCREMENTAL IMPROVEMENT @ 7.5 MIL
BUT IF OTHER OPTIONS ARE \$20 MIL + GO ALMOST
ALL THE WAY W/ OPTION 2.

- LIKE THROUGH WAY ON WURZBACH GOING EAST
ON FRONTAGE

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: BAN & SUKANYA LEAN

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

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- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: We Strongly Support OPTION # 5

SECOND CHOICE OPTION # 1

THANK YOU FOR PRESENT US MORE
OPTION !! NEED LIGHT ON N-W MILITARY,
BEFORE @ TURN INTO GATE!

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Timothy Farris (Farris)

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

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- I could benefit monetarily from the project or other item about which I am commenting

BEST: #5 → #6

WORST: #1 - #4 (with the pre-made decision of doing so, I would go with #1)

COMMENTS: This project, contrary to poor study designs that showed only a mild improvement, would severely impact the surrounding neighborhood in - at this time - very predictable ways. Lower property values would be incurred by several residents, excessive noise would diminish child health and well-being, excessive construction hazard, and long-time for project completion would not yield expected results

Deadline for Comments: September 15, 2017

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San Antonio, TX 78205



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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Lori Boies

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

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- I could benefit monetarily from the project or other item about which I am commenting

Rank: BEST → WORSE
5, 6, 2, 1, 3, 4

COMMENTS: Adding a tunnel will cost \$20 million plus more and years of construction to save less than a minute! Add a lane and time the lights- save our money and construction headache without pushing the issue to Lockhill Selma. Other Wurzbach projects (Fred+10) are much worse! 1604 is a nightmare! These projects (Fred, 10, all of 1604) provide a better relief for our community. Better yet, send that \$20 million to Houston to recover from Harvey! The tunnel can always come later

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if the most cost effective, Lockhill Selma helping, tree-saving (to some extent) option does not work!
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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Roxana De Leon Fuentes

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

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- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: I'm a homeowner at the Estates of Alon, options 1, 2 and 3 does not benefit our neighborhood, we don't even have access to the proposed improvements why do we want to be bother by all the construction. These proposals will negatively affect the value of our homes. Getting rid of the median and trees is not good for our neighborhood. If an option must be built I prefer option # 5 or no-build.

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Joel Griffith

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

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- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Option 5 (At-Grade Improvements) is the clear best choice.

Lights at Summerfield and at Alon Loop should be added to address safe entry/exit to these neighborhoods (and to Hardberger Park). These should be considered for all options (not just option 1).

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Shelley Billarson

ADDRESS: [Redacted]

REPRESENTING: _____

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
I do business with TxDOT
I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: The only option that helps traffic & does NOT INCREASE pedestrian injury & death is #5. This is the only option I feel is NOT huge overkill by CREATING more problems than it solves. #5 makes SENSE & with smart lights it would LIKELY SOLVE THE TRAFFIC PROBLEM.

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Adrian Saucedo

ADDRESS: [REDACTED]

REPRESENTING: _____

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Option 5 is the best option

Deadline for Comments: September 15, 2017

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Laura Saucedo

ADDRESS: 

REPRESENTING: _____

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

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- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Best option is #5!

Deadline for Comments: September 15, 2017

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Steven Sanders

ADDRESS: [Redacted]

REPRESENTING: [Redacted]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: My property values will decrease with a stupid wall!
No wall! Option 5 is the best for our area.
Improve the flow of traffic all will be better.

Deadline for Comments: September 15, 2017

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: CHARLES ROGER MACIAS JR.

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: THE OPTIONS OTHER THAN #5, #6
INCREASE NOISE, CREATES A TUNNEL IN
A RESIDENTIAL AREA. IF YOU INCREASE
A LANE EACH WAY ON WURZBACH PKWY AND
INCREASE TIMING OF LIGHTS BY 45 SECONDS
AT PEAK DRIVE WILL ELIMINATE TRAFFIC CONGESTION.
THE OPTIONS 1 - 4 WILL DECREASE MY PROPERTY
VALUE CONSIDERABLY.

Deadline for Comments: September 15, 2017

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Wurzbach Parkway at NW Military



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: David Masouli

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: option 5.
All other options are
harmful and will cause major
problems

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Raja Lelcher Borra

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

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- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Save trees - option 5 - is
my votby. Dont waste public
money

option: 5

Deadline for Comments: September 15, 2017

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Raja Kandil Cayala

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

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- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Option - 5 is the best and
there is no real Advantage of all
other option. Don't wait public
Money. Ule for good reason.
Save trees ~~and~~ and have healthy life

Deadline for Comments: September 15, 2017

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: SASIKANTH NALLAGATLA / SWAPNA CHATTAMAKENI

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: option (5) is the best for my community. I strongly recommend it.

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Attn: Ibette Cavazos
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San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military

From: James Lutz
To: [28422C WA#3 - NW MILITARY INTER Project Email](#)
Subject: Wurzbach Intersection
Date: Thursday, August 31, 2017 7:12:01 AM

I prefer option five.

The cost vs benefit doesn't balance. Until the City is willing to buy the necessary ROW to continue the improvement to IH10 this project gives the east west commuter little benefit.

Additionally, what will happen to the traffic when improvements to loop 1604 are made? Will it relive the traffic?

James A. Lutz



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Stuart Cameron

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

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- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: The best option is "no build". Any work on the Wurzbach/Mil Hwy Crossing will make things worse. The traffic we are getting now is from cars escaping 1604. Until 1604 is improved, no they will help this neighborhood.

Deadline for Comments: September 15, 2017

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Kati Bahrami [REDACTED]

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

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- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: I am against building a tunnel, against having a high way in this area. This can plan for a highway can increase traffic which you are proposing will decrease. It will increase accidents. I also do not want the trees to be eliminated + our ability to walk on the sidewalks of wurzbach

Deadline for Comments: September 15, 2017

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COMMENT FORM
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Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Steven Golub

ADDRESS: [Redacted]

REPRESENTING: [Redacted]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

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- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: I am against building a tunnel/highway on Wurzbach!!
I dont want the trees to be eliminated
& want to have the peace & quiet that we currently enjoy. There will be more accidents & this seems to be more for people living far from here & there's no

concern over people who currently live here!

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Julie McLaughlin

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

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- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: I am against all plan which include an underpass through NW Military Hwy. These will affect the value of my home negatively and I am unconvinced it will create a significant amount of efficiency at the proposed intersection.

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: David Martin

ADDRESS: [Redacted]

REPRESENTING: [Redacted]

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COMMENTS:

- This intersection is only congested for (M-F) 1-2 hours a day max. All other times (Sat. & Sun) are not congested.
I believe this is a waste of money & the nearby property values will drop due to this highway.
This will only move the traffic down 1 block to Elm Creek.
This will render Inverness & Alon gates available for intended use.

Deadline for Comments: September 15, 2017

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San Antonio, TX 78205



Or go to txdot.gov and keyword search Wurzbach Parkway at NW Military

The environmental review, consultation, and other actions required by applicable Federal environmental laws for this project are being, or have been, carried-out by TxDOT pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 16, 2014, and executed by FHWA and TxDOT.



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Tammy Martin

ADDRESS: [REDACTED]

REPRESENTING: _____

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: - Property values will decline
- This will move the heavy traffic
to Lockhill-Selma which has fewer lanes.
- This will be much noisier location
- This intersection is only busy in the
afternoon 5-6 (Monday - Friday) only.
- Money should be spent on other
locations such as 281 North.

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibette Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Altar Benni a

ADDRESS: [Redacted]

REPRESENTING: [Redacted]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
I do business with TxDOT
I could benefit monetarily from the project or other item about which I am commenting

COMMENTS:

It will cheaper + make it ugly - and god know it.
This lowers our prop values considerably, because of noise, dirt, and un-aesthetic appearance of project. It is also not efficient because traffic will just be worse at Lockhill. San Antonio lacks attractive landscaped roads, this is sad to see a "nice" area go down. We can consider a class action re our properties no one will want to live in these

Deadline for Comments: September 15, 2017

Please submit by - Phone: (210) 901-5124

Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibette Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205

For city of Comal County



Or go to txdot.gov and keyword search Wurzbach Parkway at NW Military

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please help Houston w/ this money. What is behind this? (money for whom)



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Robert L. Elizondo

ADDRESS: [REDACTED]

REPRESENTING: _____

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: I live on Whisper Dew my backyard is Wurzbach. I am Afraid the increased traffic and noise is going to negatively affect my home's value and will impact my ability to sell my house. I am very sad and upset about your plans.

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibette Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: SHIVANI RUPAREL

ADDRESS: [REDACTED]

REPRESENTING: _____

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: _____

① This construction on the new military is only going to cause a bottleneck on Lockhill Selma.

② The jam is really for an hr or so in the morning & one hour in evening. Then why do this?

③ Our property values will go down to have a highway outside

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibetta Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Loren Marie

ADDRESS: [Redacted]

REPRESENTING: [Redacted]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
I do business with TxDOT
I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: The project is not common sense. You want to spend millions to fix traffic @ 1 major intersection and then have more problems heading West from LHSelma onward. This is misappropriation of tax payer's money. There are several more problematic areas than this intersection. You cannot waste money to change one intersection w/out planning out the rest of Wurzbach roadways heading West. Who is gaining from this construction project? Who is so motivated to spend this money?

Deadline for Comments: September 15, 2017

Please submit by - Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibette Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search Wurzbach Parkway at NW Military

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: _____

Janis Will

ADDRESS: _____

REPRESENTING: _____

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS:

Why spend \$20-30 million on a project not completed until 2023 when another I604 improvement is done by 2025 may have a similar if not better impact on Wurzbach Parkway traffic.

I think the drivers affected by the huge construction impact 2020-2023 may learn to drive somewhere else

Deadline for Comments: September 15, 2017

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Email: wurzbach@wsp.com
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Bank of America Plaza
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San Antonio, TX 78205



Or go to txdot.gov and keyword search Wurzbach Parkway at NW Military

anyway.

The environmental review, consultation, and other actions required by applicable Federal environmental laws for this project are being, or have been, carried-out by TxDOT pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 16, 2014, and executed by FHWA and TxDOT.



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: KARI Kent Schultz

ADDRESS: [Redacted]

REPRESENTING: [Redacted]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
I do business with TxDOT
I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: spending \$28-29 million dollars at intersection & won't remove bottleneck at Lockhill-Selma, thus won't move any more traffic - Money better spent widening N.W. Military, then widen Huebner to USAA & medical center. * Also better use money, to widen N.W. Military out to 1604 - // Why hasn't longer term plan (2030) been presented to community // you are opening up one intersection, but then where are the cars/traffic/bottlenecks going to end up - Just another problem -

Deadline for Comments: September 15, 2017

Please submit by - Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibette Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search Wurzbach Parkway at NW Military

lockhill selma plan
- Delay in Selma
Texas Department of Transportation

no federal funds → NW military to Huebner → USA 11 - Medina Ave
- not present delay at Lockhill

TARJETA DE COMENTARIO

Wurzbach Parkway y NW Military Exhibición Abierta al Publico

Barshop Jewish Community Center

30 de agosto de 2017

(POR FAVOR IMPRIMA)

NOMBRE: _____

DIRECCION: _____

REPRESENTO: _____

(Código de Transporte de Texas, §201.811(a)(5)): Marque cada una de las siguientes cajas que se aplican a usted:

- Soy empleado de TxDOT
- Hago negocios con TxDOT
- Puedo aprovechar monetariamente del proyecto o cualquier otro elemento sobre que estoy comentando



Also Need a lane for Elm Creek residents going west bound - with traffic bottleneck, we can't get into neighborhood

COMENTARIOS:

- Also, what are going to do about air quality for 30+ residents - //

- Can't widen ~~Wurzbach~~ Lockhill Selma to IH-10; can't widen Lockhill-Selma to Huebner HAVE to widen NW Military - only option for this area - & then then forced to widen Huebner - Spend money to work that out before configure the intersection at \$28-\$29 million

Fecha límite para comentarios: 14 de septiembre de 2017

Por favor, envíe por – Teléfono: (210) 901-5124
Correo electrónico: wurzbach@wsp.com
Correo: WSP USA
Attn: Ibette Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



O ir a txdot.gov y búsqueda con la palabra clave Wurzbach Parkway at NW Military

La revisión ambiental, consultas y otras acciones requeridas por las leyes ambientales federales aplicables para este Proyecto están siendo o se han llevado a cabo por TxDOT - "pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 16, 2014, and executed by FHWA and TxDOT."

From: [REDACTED]
To: [28422C WA#3 - NW MILITARY INTER Project Email](#)
Subject: Comment form Wurzbach Parkway at NW Military
Date: Saturday, September 02, 2017 3:59:19 PM

I had difficulty emailing your preprinted form. Here are my comments:

NAME: Gigi Kendrick
[REDACTED]

(None of the boxes apply to me)

I have several strong reasons for choosing option 6 - Make No Changes to the intersection of Wurzbach Parkway and Northwest Military Highway. I have lived near this intersection for 15 years and traverse this intersection many times a day.

1. The traffic buildup occurs ONLY at rush hour - every other time of day, traffic flows freely. 30 million is too much to invest on a problem that occurs only for a few hours each weekday, particularly as drivers have several other routes they can take to get to I-35.

2. Millennials and Gen X-ers aren't driving as much as the older generation. As the older workforce retires, there will be fewer cars on the road. (This could be accelerated by improving our city public transportation :)

3. Wurzbach Parkway, between Lockhill-Selma and Northwest Military Highway, is aesthetically a beautiful road, with trees in its median and walking paths. I often see parents with young children walking or biking there. The proposed design as featured in the video is hideous with no way for pedestrians to safely cross the street. It has destroyed every tree. There is no sense in ruining one of the most attractive roads in San Antonio

4. Wurzbach Parkway between NW Military and I-35 is a very curvy road and the speed limit is only 60. Already we deal with people treating Wurzbach as a major highway, which has lead to accidents and fatalities. This new design makes Wurzbach resemble a major highway, will encourage people to go even faster and lead to more accidents on roads that weren't designed to be treated as interstate highways.

5. This project will take years. By the time you've finished, there will be some other traffic problem that will require the money we just wasted here. Furthermore, during construction many commuters will elect to use George Rd. to get from NW Military to Lockhill-Selma. If you think the traffic is bad now, wait till you try to get all those cars down a residential, frequently flooding, small neighborhood road.

6. I see no way this tunnel will not flood when we have rain

There are several other ways to better the traffic problem at I-35 without ruining one of the few attractive roads in San Antonio

From: Armstrong, Scott D.
To: [28422C_WA#3 - NW MILITARY INTER Project Email](#)
Subject: FW: NW Military, Wurzbach Parkway improvements
Date: Tuesday, September 05, 2017 9:30:14 AM
Attachments: [image001.png](#)

Scott D. Armstrong, PE
Senior Engineering Manager



[Redacted text block]

Scott – Please add to the comments.

Thanks

From: Alfred White [Redacted]
Sent: Friday, September 01, 2017 7:47 PM
To: Richard De La Cruz
Subject: NW Military, Wurzbach Parkway improvements

Hello,

I am writing to express my opinion regarding the proposed TXDoT project at NW Military and Wurzbach Parkway as a citizen of San Antonio and a resident of the Estates of Alon Neighborhood. I have reviewed the current proposed plans and feel that the SPUI, Modified SPUI, and Diamond interchange plans all possess the same flaw that they will relocate the bottle neck of traffic from NW Military and Wurzbach Parkway to Lockhill-Selma and Wurzbach Parkway. For west-bound traffic this an inconsequential improvement over the current design. The huge financial investment, time, and disturbance cause by those plans' construction are not justified by their meager benefits without tremendous improvements to Wurzbach from Lockhill-Selma to I-10. Therefore I cannot support their implementation. The Partial Grade Separation and the At-grade improvement would have a similar effect as the aforementioned plans but would provide better flow for the east-bound traffic. This appears to me to be a reasonable benefit however the cost, time, and disturbance are questionable. The bottom line is that without significant improvements to Wurzbach from Lockhill-Selma to I-10 (San Antonio maintained) improvements to NW Military and Wurbach are pointless for west-bound traffic and provide questionable benefit to east-bound traffic. Therefore I would advocate for the project that has the lightest burdens of cost, time, and disturbance (if any project at all). As an aside I feel that a light at the intersection of Alon Loop and NW Military would be a significant benefit to the safety of those navigating those turns.

Respectfully,

Alfred White
[Redacted signature]

A horizontal rectangular input field with a thin border. A small blue square icon containing a white question mark is positioned in the center of the field.

From: Armstrong, Scott D.
To: [28422C WA#3 - NW MILITARY INTER Project Email](#)
Subject: FW: We do not want the tunnel option
Date: Thursday, August 31, 2017 9:34:27 AM

Scott D. Armstrong, PE
Senior Engineering Manager

-----Original Message-----

From: Richard De La Cruz [REDACTED]
Sent: Thursday, August 31, 2017 7:48 AM
To: Armstrong, Scott D. [REDACTED]
Cc: Clayton Ripps [REDACTED]
Subject: FW: We do not want the tunnel option

Scott - Please add to the public comments.

Thanks

-----Original Message-----

From: Cindy Farris [REDACTED]
Sent: Wednesday, August 30, 2017 7:41 PM
To: Richard De La Cruz
Subject: We do not want the tunnel option

Hi there,

We do not want the tunnel option for the Wurzbach parkway project. SO many inconveniences:

- 1) noise, lane closures, 2-3 yrs of this!!! Our bedroom window is right by the construction!!!
- 2) more congestion at Lockhill Selma
- 3) our walkability & bike-ability will be reduced (safety issues) which is why we even moved here in January of 2016.
- 4) tree removal will be ugly & will make the neighborhood feel more like a concrete jungle
- 5) inaccessibility to Alon Estates entrance off of Wurzbach will be super annoying

Please do not do the tunnel option.

Cindy Farris
[REDACTED]

[Connecting Texans to what matters most. Texas Department of Transportation: 1917-2017 #txdot100]
<<http://www.dot.state.tx.us/txdot100/>>



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Maria G. Lund

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: I prefer (e), followed by 5
 As the options

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibette Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Jaya Koneru

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS:

Please add light at NW mil Gate
entrance to ALON. Very Hard to
See at night. I have turned
into wrong lane sometimes

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibetta Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: MARC TANNENBAUM

ADDRESS: [REDACTED]

REPRESENTING: _____

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: WHAT ABOUT A DEDICATED BUS
LANE?

TRAFFIC LIGHT @ TURNBERRY FOR
EVERY OPTION.

ACCESS TO BUSINESS FROM EAST
IS A MUST HAVE.

Deadline for Comments: September 15, 2017

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Mail: WSP USA
Attn: Ibetta Cavazos
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San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: James Kuan

ADDRESS: 

REPRESENTING: 

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

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- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: if city is interested in moving traffic then get together with them ~~and~~ and study Huebner — more land available, street is wider, less pedestrian traffic

Deadline for Comments: September 15, 2017

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Attn: Ibetta Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Joel Dav

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

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- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Maintain Property Access!!

Deadline for Comments: September 15, 2017

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Mail: WSP USA
Attn: Ibette Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: RHONDA HOFFMAN

ADDRESS: [REDACTED]

REPRESENTING: _____

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS:

- need light at Fairfield Bend/Turnbury for ALL options

- do not bring Rt turn lane all the way to Bluffton Oaks as some people requested as this will make it harder to get out ~~at~~ at Bluffton Oaks and much more difficult to get to left turn lane at NWMilitary

Deadline for Comments: September 15, 2017

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Attn: Ibette Cavazos
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** more studies needed concerning length of lights during rush hours. Set lights for weeks/months to see ~~impact~~ real impact.



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Denise Sanders

ADDRESS: 

REPRESENTING: 

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: _____

- can we fix the noise problem
with ^{*}windows to decrease
noise

- add trees along wall at Summerfield,
or bushes, to ↓ noise

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
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Attn: Ibetta Cavazos
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San Antonio, TX 78205



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Wurzbach Parkway at NW Military



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: _____

Dretchen Roufs

ADDRESS: _____

REPRESENTING: _____

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS:

Thank you for studying this project — please do something to alleviate the traffic @ NW Military and Wurzbach. Ever since Wurzbach opened all the way, the bottleneck and related traffic has been problematic and inspires me to totally avoid this area... which is probably not the intent for this facility. Thank you for asking

Deadline for Comments: September 15, 2017

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Mail: WSP USA
Attn: Ibette Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search Wurzbach Parkway at NW Military

the different questions.

From: Armstrong, Scott D.
To: [28422C.WA#3 - NW MILITARY INTER Project Email](#)
Subject: FW: Wurzbach Parkway & NW Military
Date: Thursday, August 31, 2017 9:35:41 AM
Attachments: [image001.png](#)

Scott D. Armstrong, PE
Senior Engineering Manager



From: Richard De La Cruz [REDACTED]
Sent: Thursday, August 31, 2017 8:24 AM
To: Armstrong, Scott D. [REDACTED]
Cc: Clayton Ripps [REDACTED]
Subject: FW: Wurzbach Parkway & NW Military

Scott – Please add to the public comments record.

Thanks

From: Craig Bell [REDACTED]
Sent: Wednesday, August 30, 2017 7:13 PM
To: Richard De La Cruz
Subject: Wurzbach Parkway & NW Military

I am a lifelong resident of San Antonio, and look forward to a solution of the problem at Wurzbach and NE Military.

As I look at EVERY other Wurzbach Parkway intersection where traffic was a problem, the solution was an overpass, not a tunnel. To me there is plenty of room for an over pass and it would not close that intersection as long as building a tunnel. I have to believe an overpass would cost millions less as well.

Sincerely,

Craig Bell

[REDACTED]

[REDACTED]





COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: _____

Mary Gonzalez

ADDRESS: _____

[Redacted Address]

REPRESENTING: _____

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS:

*Awesome project
Need to complete ASAP*

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
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Attn: Ibette Cavazos
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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Ven Koneru

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Alon cannot use underpass

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibette Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military

The environmental review, consultation, and other actions required by applicable Federal environmental laws for this project are being, or have been, carried-out by TxDOT pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 16, 2014, and executed by FHWA and TxDOT.



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: _____

DAVID RODRIGUEZ

ADDRESS: _____

REPRESENTING: _____

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: _____

Thank you for

opportunity

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124

Email: wurzbach@wsp.com

Mail: WSP USA

Attn: Ibette Cavazos

Bank of America Plaza

300 Convent St., Suite 1330

San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: NAVIN BHATIA

ADDRESS: 

REPRESENTING: 

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: See Attached

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibetta Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



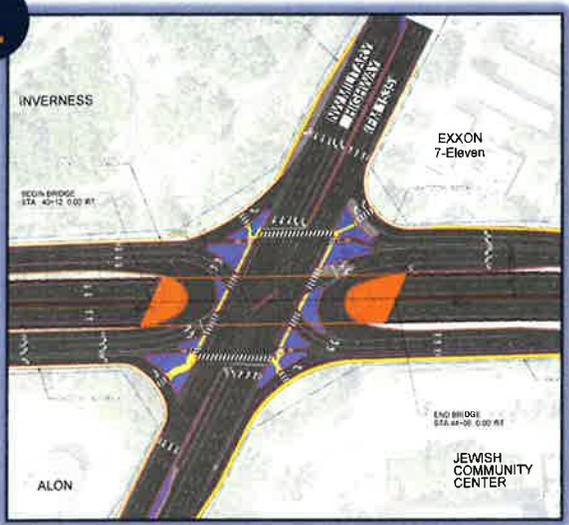
Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military

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NW MILITARY AND WURZBACH PARKWAY OPTIONS

INSTRUCTIONS: Please provide comments on what you like and dislike about each option shown.

1

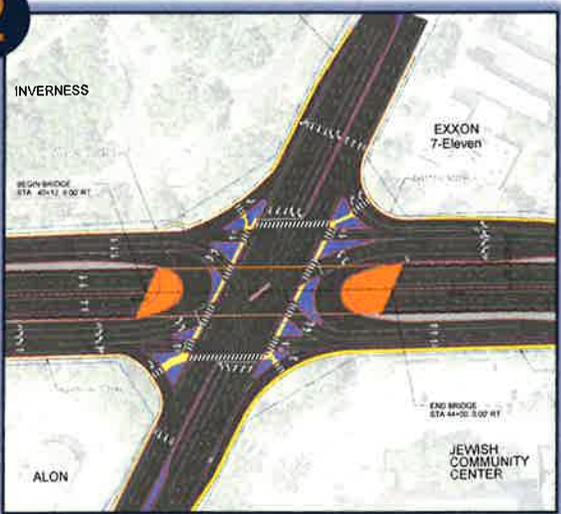


Single Point Urban Intersection (SPUI) with Signals

PROS: LIGHT AT TURNBERRY

CONS: VERY EXPENSIVE
~~NO LIGHT AT~~ NO TREES
 NO THRU LANES
 "WESTBOUND" TO "ENTER INVERNESS"

2



Modified SPUI with Frontage Roads

PROS: ONE THRU LANE
 WESTBOUND TO PROVIDE
 INGRESS TO INVERNESS BLDG

CONS: NO TREES
 NO LIGHT AT
 TURNBERRY WAY

3



Diamond Interchange

PROS: TWO THRU LANES
 WESTBOUND (FROM BLANCO)
 TO PROVIDE INGRESS
 TO INVERNESS BLDG

CONS: NO TREES
 NO LIGHT AT
 TURNBERRY

4

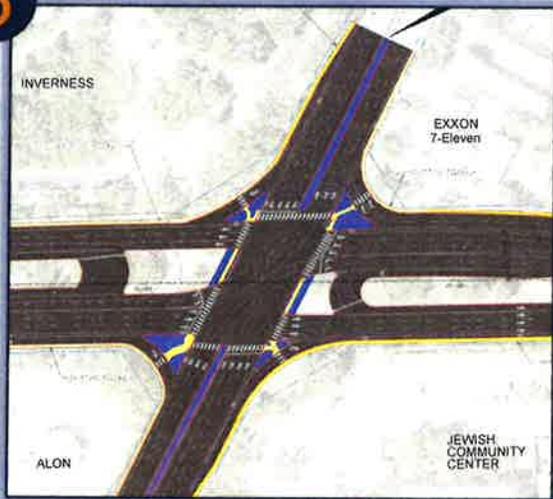


Partial Grade Separation

PROS: SOME TREES REMAIN
 EXCELLENT ACCESS
 TO INVERNESS

CONS: NO LIGHT AT
 TURNBERRY

5



At-grade Improvements

PROS: TREES REMAIN (MOST)
 CONTINUED ACCESS
 @ STATUS QWO AT
 INVERNESS & TURNBERRY
 LOW COST

CONS: NO LIGHT @ TURNBERRY

6



No-Build

PROS: CONTINUED STATUS
 QWO ACCESS TO
 INVERNESS AND ~~PART~~
 TURNBERRY

CONS: NO IMPROVEMENT
 TO TRAFFIC ISSUES



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: DAVID TRIPLETT

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: _____

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibetta Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Jan Kurr

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: _____

Deadline for Comments: September 15, 2017

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Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibette Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Richard Acosta

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

- (Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:
- I am employed by TxDOT
 - I do business with TxDOT
 - I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: _____

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibette Cavazos
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300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military

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COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Mike Frantz

ADDRESS: [REDACTED]

REPRESENTING: _____

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: _____

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibette Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military

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From: Burt Holland
To: [28422C WA#3 - NW MILITARY INTER Project Email](#)
Subject: Comments on Proposed Changes of Wurzbach
Date: Friday, September 15, 2017 5:36:58 PM

I have a business located in the Alon Town Center, as well as having family who live in Elm Creek, Inverness and Castle Hills Forest. I travel through the Wurzbach Parkway/NW Military Highway intersections frequently. Please take into consideration my observations of the proposed Options.

Of the proposals made there are only two that seem reasonable, Options 2 and 5.

Option 1 has at least one significant draw back, the inability to pass through the intersection going east and west bound at grade. This will cause numerous other traffic problems in the immediate area. The proposal to add a light at the Military entrance to Inverness only addresses one of several problems and causes problems of its own. A significant amount of traffic that exits Alon Town Center exits directly onto Wurzbach headed east bound which would now need to exit through the light on Military. This intersection on Military already cannot handle the volume of traffic it receives in the afternoon or at any other high traffic time. This proposal just shifts traffic from one spot to another causing a greater problem.

Option 2 would appear to be the option that addresses the most issues with the traffic needs while not causing other problems.

Options 3 and 4 are just variations on option 2 that do not provide any additional benefit but create problems or do not fix some problems solved by Option 2. Given that it does not appear there is any significant cost saving or reduced construction time I do not see why either Option 3 or 4 would be chosen over Option 2. I do not find the argument that the traditional interchange design of Option 3 is less confusing compelling. The exceptionally sharp dual left turns in the traditional interchange seem to already cause confusion for drivers and create a dangerous situation. Many vehicles cannot make these turns and stay in their lane, or fail to do so because of driver confusion. I have seen many near misses where an accident almost occurred because of a driver not staying in their lane. The slow speed this turn requires limits the number of cars that can get through this traditional intersection.

Option 5 like Option 2 seems to address many of the problems with the current intersection. It may not address them as efficiently as Option 2 but it does so at significantly less cost and with less construction time. Ever since the intersection went in I have wondered why it did not include features that are incorporated in Option 5.

I see advantages to both Options 2 and 5. It is a balancing decision between greater ultimate efficiency but at greater cost and construction time. Please consider Options 2 and 5 as the preferred Options. Thank you.

Burt A. Holland



[REDACTED]

From: Rohrer, Mary L
To: [28422C WA#3 - NW MILITARY INTER Project Email](#)
Cc: [Rohrer, Mary L](#)
Subject: Wurzbach Parkway Comments from Open House, August 30, 2017
Date: Friday, September 15, 2017 4:26:15 PM

Dear Ms. Cavazos,

Thank you for providing information at the Open House to review the proposed options for improvements at NW Military and Wurzbach Parkway held on August 30, 2017.

For the operations of our HEB grocery store within the Alon Town Center, Option 2 would be the best solution for us. Having an efficient traffic signal system using the SPUI signal design system helps improve capacity of the intersection. By including the Wurzbach Parkway through lanes, keeps the current customer movements in place and not force additional customers to use the already busy traffic signal on NW Military Drive.

As previously mentioned in my earlier comments please incorporate the following comments as TxDOT designs and constructs the improvements

- Please provide locations for installation of permanent TxDOT approved signage for Alon Town Center on Wurzbach Parkway at the NW Military Ramps to direct customers to HEB and Alon Town Center. With the center's size over 300,000sf, the center meets TxDOT's criteria for retail signage on roadways.
- Consider limiting major traffic construction disruptions during major holidays, such as Thanksgiving, Christmas, Easter, when the HEB store and the center is most busy.
- Temporary direction signage to Alon is needed during construction and shall be visible and inviting for customers at Wurzbach driveways, and include directional signs at the new SPUI intersection.

Thank you again for the opportunity to provide comments to your plan. We support the Option 2 SPUI design for the Wurzbach Parkway improvements.

Thanks,
Mary Rohrer

[REDACTED]
[REDACTED]
[REDACTED]

Michele Haussmann and Jack Braha



September 12, 2017

Mr. Mario R. Jorge, P.E.

Via Electronic Mail



RE: NW Military Highway and Wurzbach Parkway Intersection Improvements – Partnership between City of San Antonio (“City”) and Texas Department of Transportation (“TxDOT”)

Dear Mr. Jorge,

Thank you for your work on the NW Military Highway and Wurzbach Parkway intersection improvements project (“Project”) to decrease traffic congestion and improve safety in the area, and including the stakeholders in the community input process. We have been residents of the Inverness Neighborhood for four years and office in the Alon Town Centre. We appreciate your leadership in addressing traffic issues in our neighborhood that will improve our quality of life.

This letter serves as the official comment from us on the Project as conceptually designed by TxDOT. We understand that there are six options TxDOT has evaluated for this intersection. After reviewing the updated conceptual designs presented by TxDOT on August 30th, we feel that the Option #2 Modified Single Point Urban Interchange with Frontage Roads (“MSPUI”) option provides the best long term solution for our area.

It is important that the design includes the tunnel under NW Military Highway as well as at grade westbound and eastbound lanes on Wurzbach Parkway through the NW Military Highway intersection. This MSPUI design continues to provide direct access to the main gate into the Inverness neighborhood. If these lanes are not included in the Project, we feel our neighborhood and our place of office will be severely adversely impacted.

Thank you again for the opportunity to give community input into the Project. We applaud your efforts in recognizing the traffic issues at this intersection and look forward to continuing the dialogue with the City of San Antonio and TxDOT on this Project.

Sincerely,

Jack Braha,
Inverness Resident and
Tenant in Alon Town Centre

Michele Haussmann
Inverness Resident and
Tenant in Alon Town Centre

September 13, 2017

Mr. Mario R. Jorge, P.E.
District Engineer, San Antonio District

Via Electronic Mail

RE: NW Military Highway and Wurzbach Parkway Intersection Improvements – Partnership between City of San Antonio (“City”) and Texas Department of Transportation (“TxDOT”)

Mr. Jorge:

Thank you for your work on the NW Military Highway and Wurzbach Parkway intersection improvements project (“Project”) which will decrease traffic congestion and improve safety for customers and tenants of the Alon Town Centre. Our development is frequented by thousands of people each day who shop at H.E.B. and the other retail stores and restaurants. We appreciate your interest in addressing traffic issues at this intersection.

This letter serves as the official comment from Wurzbach / NW Military Partners, Ltd., as owner of Alon Town Centre on the Project as conceptually designed by TxDOT. We understand that there are six options TxDOT has evaluated for this intersection. After reviewing the updated conceptual designs presented by TxDOT at the Open House community meeting on August 30th, we feel that the Option #2 Modified Single Point Urban Interchange with Frontage Roads (“MSPUI”) option provides the best long term solution for the immediate area and access to the South Texas Medical Center.

More specifically, The MSPUI option includes the continued “at grade” eastbound lane on Wurzbach Parkway, which will allow vehicles exiting Alon Town Centre that want to go east on Wurzbach Parkway to continue through the intersection. Thank you for including this option to address our concerns raised with the original conceptual design of the SPUI.

To restate our concerns about the original SPUI design, with the elimination of the Wurzbach “at-grade” eastbound lane, vehicles exiting Alon Town Centre that want to go east on Wurzbach, will be forced to use the NW Military entrance in the middle of our property, which will cause added congestion and access issues for customers of H.E.B. and surrounding tenants. Without the eastbound lane, traffic congestion will be created on privately owned property and cars will stack in an unsafe manner for pedestrians in our shopping center drives and parking lanes. Please see the attached letter to you dated March 13, 2017 for more detail.

There is an immediate need to improve the transportation infrastructure at this highly-congested intersection on the growing northwest side of San Antonio. Thank you again for the opportunity to give input into the Project. We look forward to continuing the dialogue with the City of San Antonio and TxDOT on this Project.

Sincerely,

WURZBACH / NW MILITARY PARTNERS, LTD.



Jacques R. Braha
Development Partner

Attachment – March 13, 2017 Letter to Mario Jorge

CC: Jonathan Bean, TxDOT, *via electronic mail*
Honorable Commissioner Kevin Wolff, Chair Alamo Area MPO, *via electronic mail*
Honorable Mayor Ron Nirenberg, *via electronic mail*
Honorable Councilman Manny Pelaez, *via electronic mail*
Honorable Councilman John Courage, *via electronic mail*
Michele Haussmann, Land Use Solutions, LLC, *via electronic mail*

March 13, 2017



Mr. Mario R. Jorge, P.E.

Via Regular and Electronic Mail

RE: NW Military Highway and Wurzbach Parkway Intersection Improvements – Partnership between City of San Antonio (“City”) and Texas Department of Transportation (“TxDOT”)

Mr. Jorge:

Thank you for your work on the NW Military Highway and Wurzbach Parkway intersection improvements project (“Project”) which will decrease traffic congestion and improve safety for customers and tenants of the Alon Town Centre. Our development is frequented by thousands of people each day who shop at H.E.B. and the other retail stores and restaurants. We appreciate your interest in addressing traffic issues at this intersection.

This letter serves as the official comment from Wurzbach / NW Military Partners, Ltd., as owner of Alon Town Centre on the Project as conceptually designed by TxDOT as a Single Point Urban Interchange (“SPUI”). We understand that TxDOT feels the SPUI option provides the best long term citywide versus local neighborhood solution for this highly-congested intersection on the growing northwest side of San Antonio.

After reviewing the conceptual design presented by TxDOT on February 28th, we respectfully request a modification to the final design to include an “at-grade” eastbound lane on Wurzbach Parkway through the NW Military Highway intersection. Please see the attached exhibit. If this eastbound lane is not included in the Project, Alon Town Centre and its tenants will be adversely impacted and this letter serves as our official protest.

With the elimination of the Wurzbach “at-grade” eastbound lane, vehicles exiting Alon Town Centre that want to go east on Wurzbach, will be forced to use the NW Military entrance in the middle of our property, which will cause added congestion and access issues for customers of H.E.B. and surrounding tenants. Without the eastbound lane, traffic congestion will be created on privately owned property and cars will stack in an unsafe manner for pedestrians in our shopping center drives and parking lanes.

Wurzbach / NW Military Partners, Ltd. also supports the modification to the final design requested by the Inverness neighborhood to include an “at-grade” westbound lane on Wurzbach Parkway through the NW Military Highway intersection to continue to provide direct access to the main gate into the Inverness neighborhood. Please see the attached exhibit.

Thank you again for the opportunity to give input into the Project. We look forward to continuing the dialogue with the City of San Antonio and TxDOT on this Project.

Sincerely,

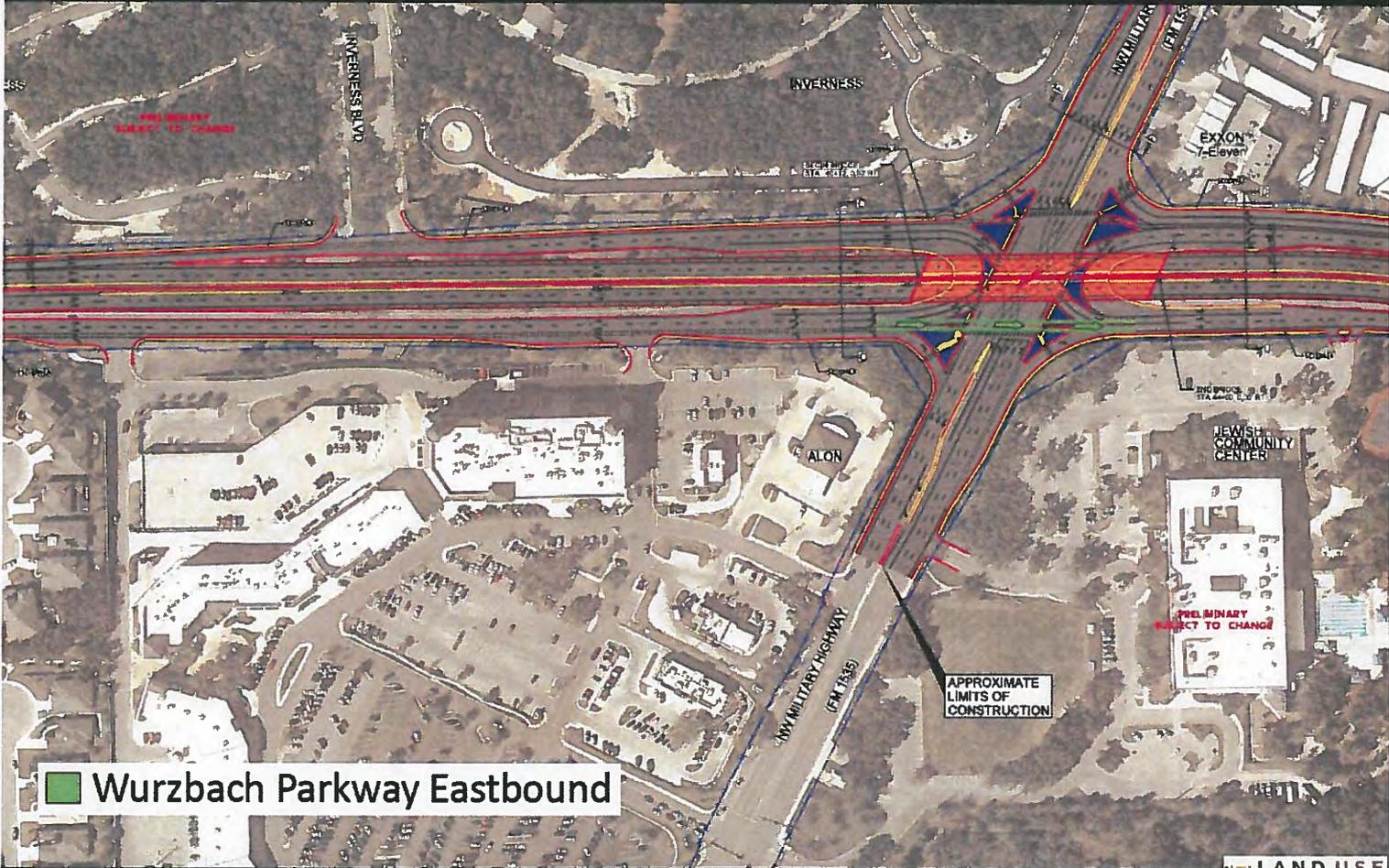
WURZBACH / NW MILITARY PARTNERS, LTD.

A handwritten signature in black ink, appearing to read 'JB', with a long horizontal flourish extending to the right.

Jacques R. Braha
Development Partner

CC: Jonathan Bean, TxDOT, *via electronic mail*
Honorable Councilman Ron Nirenberg, City of San Antonio, *via electronic mail*
Michele Haussmann, Land Use Solutions, LLC, *via electronic mail*

Alon Town Centre - Wurzbach Parkway Modification



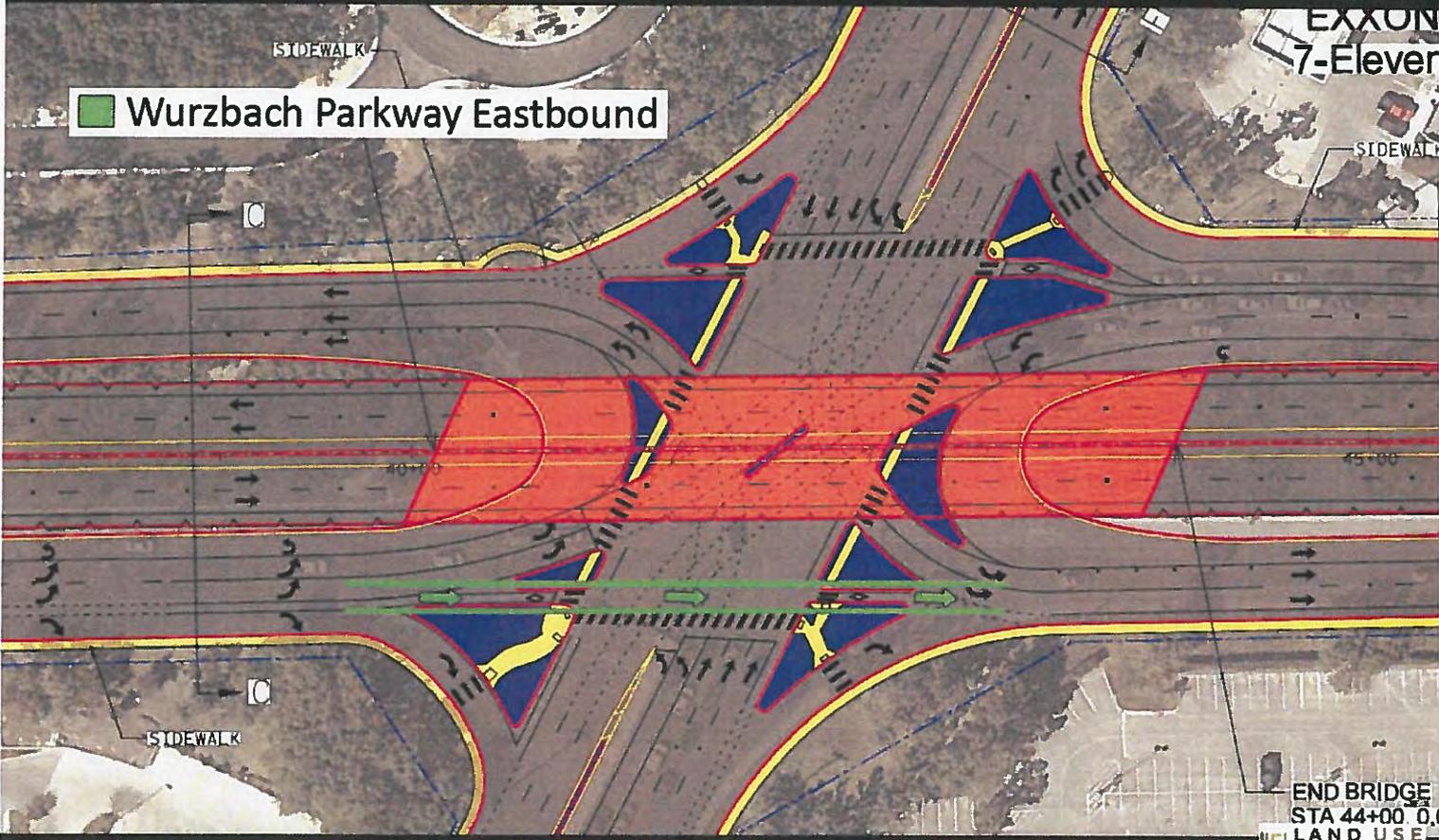
10003 NW Military Hwy., Ste. 2215

San Antonio, TX 78231

210-812-2222

LAND USE SOLUTIONS LLC

Alon Town Centre - Wurzbach Parkway Modification



10003 NW Military Hwy., Ste. 2215

San Antonio, TX 78231

210-812-2222

Alon Town Centre - Intersection Improvements



-  Alon Town Centre Requested Modification to Add Eastbound Lane on Wurzbach Parkway
-  Alon Town Centre and it's Tenants Adversely Impacted if Eastbound Lane Not Added to Wurzbach Parkway in TxDOT's final design
-  Alon Town Centre - 320,000 sf

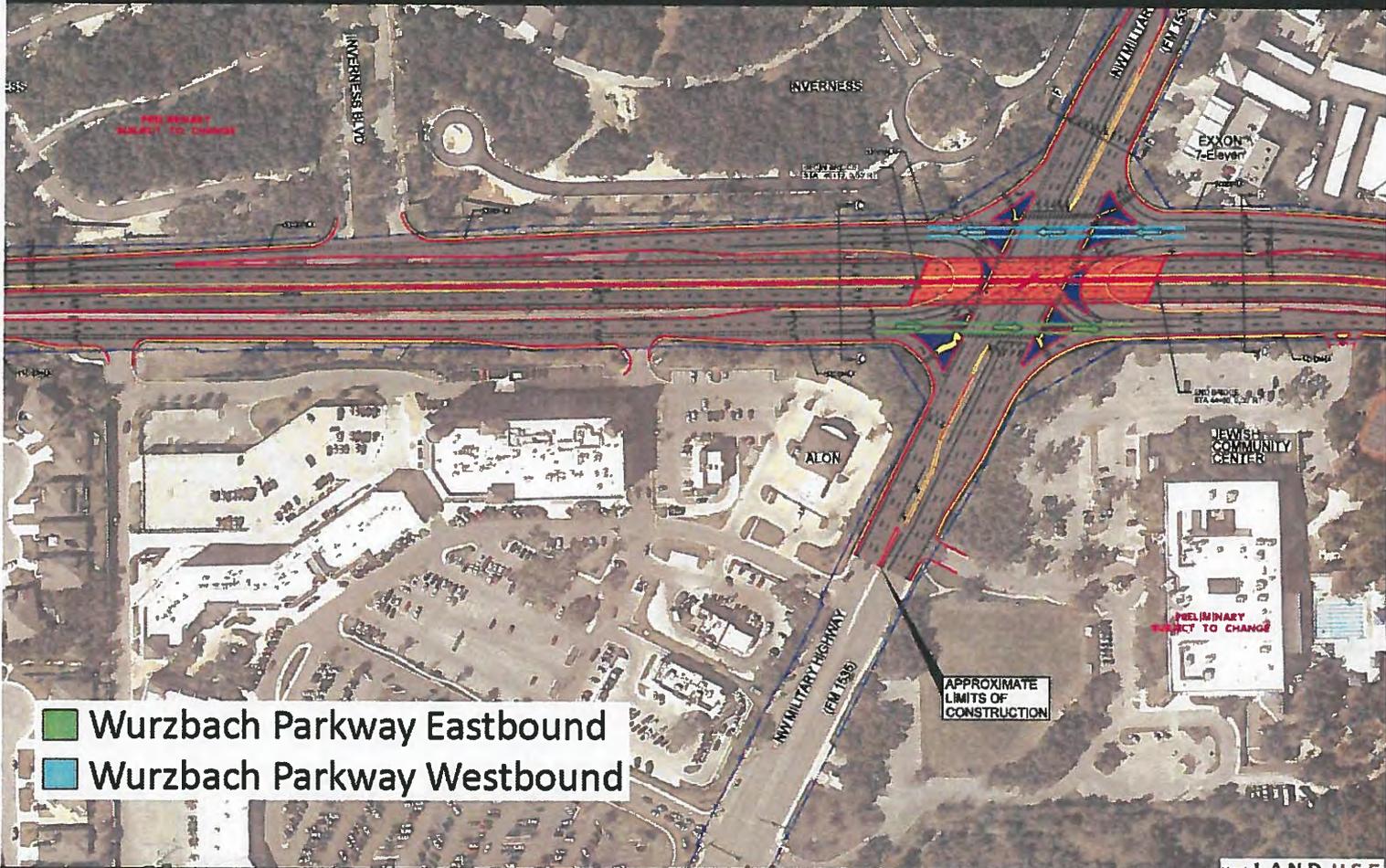
10003 NW Military Hwy., Ste. 2215

San Antonio, TX 78231

210-812-2222



Alon Town Centre - Wurzbach Parkway Modification



10003 NW Military Hwy., Ste. 2215

San Antonio, TX 78231

210 812 2222

LAND USE
SOLUTIONS LLC

From: Richard Slife
To: [28422C WA#3 - NW MILITARY INTER Project Email](#)
Cc: [REDACTED]
Subject: Design choices
Date: Wednesday, September 13, 2017 9:36:59 AM

My wife and I support design option number 1. This option provides for the safest entrance and exit from Summerfield to and from all directions.

Richard Slife
[REDACTED]

From: Guillermo Portillo-Ramila M.D.
To: [28422C WA#3 - NW MILITARY INTER Project Email](#)
Subject: Attn- Ibette Cavazos- NW Military and Wurzbach improvements
Date: Tuesday, September 12, 2017 10:22:09 AM

Greetings,

We live in the inverness subdivision. If TxDOT feels a split grade or tunnel intersection is necessary then we prefer the modified SPUI with lights at the Summerfield Inverness drives on NW Military from the split grade choices.

Our overall preference is number 5

Best,
Daniela Oliver

Sent from my iPhone



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Saul F. Levenshus

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: The lengthy construction timeframe and the reality that the problem will only be mitigated for one traffic light leaves me to believe that option 5 is the best option.

All options only move the problem up one light. Solve the problem at I-10 and Wurzbach or these solutions won't stand the test of time.

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibetta Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



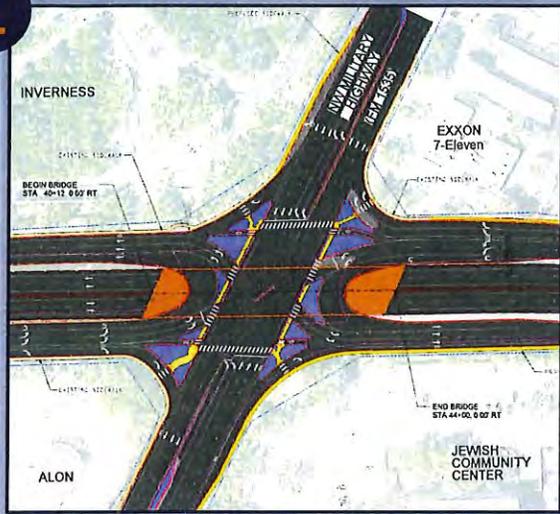
Or go to txdot.gov and keyword search Wurzbach Parkway at NW Military

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NW MILITARY AND WURZBACH PARKWAY OPTIONS

INSTRUCTIONS: Please provide comments on what you like and dislike about each option shown.

1

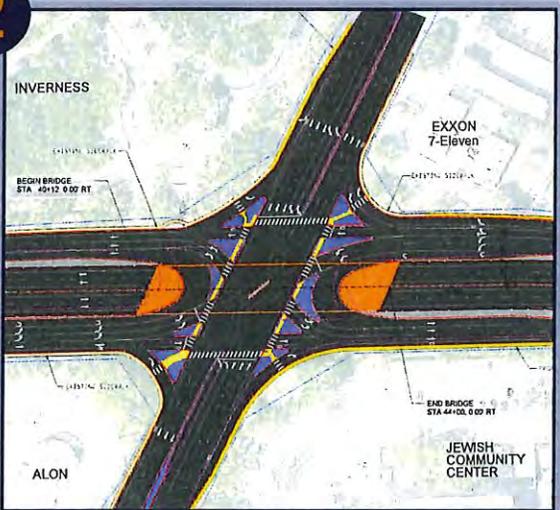


Single Point Urban Intersection (SPUI) with Signals

PROS: _____

CONS: *length of construction and this solution doesn't solve the problem*

2

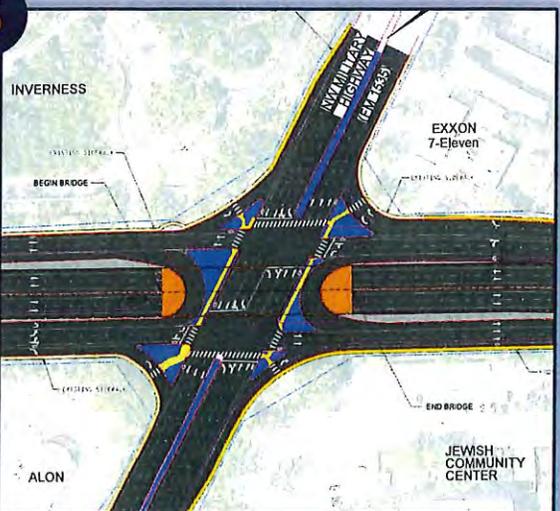


Modified SPUI with Frontage Roads

PROS: _____

CONS: *length of construction and this solution doesn't solve the problem*

3

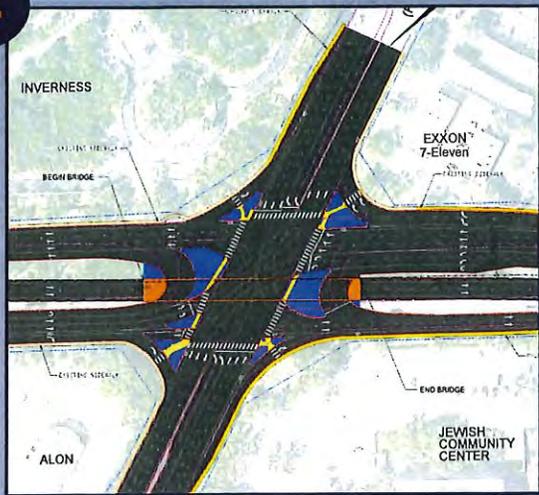


Diamond Interchange

PROS: _____

CONS: *length of construction and this solution doesn't solve the problem.*

4

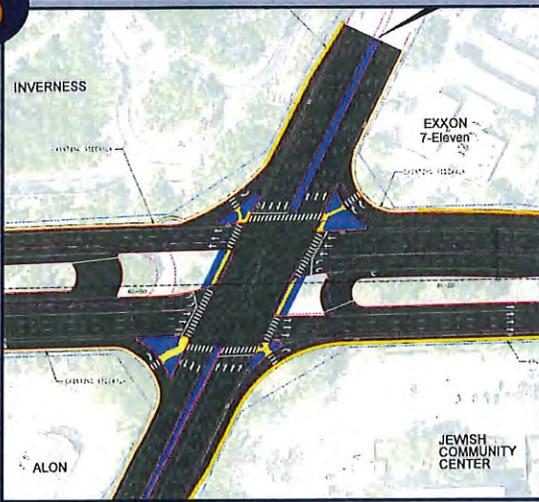


Partial Grade Separation

PROS: _____

CONS: length of construction
& doesn't solve the problem

5

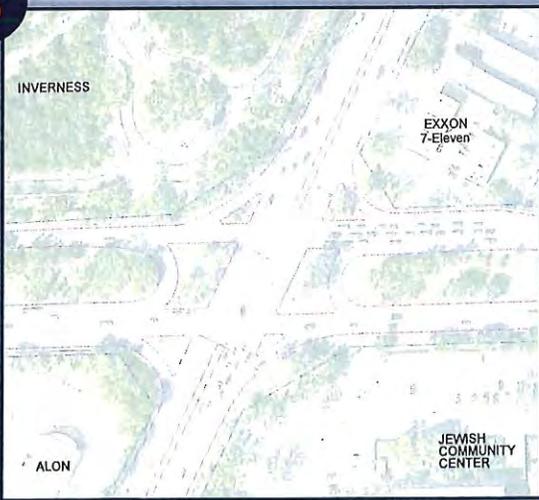


At-grade Improvements

PROS: Best option. More lanes
will leave less cars waiting
on wurzbach as widening the
road will give cars turning left
or right easier access

CONS: _____

6



No-Build

PROS: _____

CONS: No improvement



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Sylvia Weise Griffith

ADDRESS: [REDACTED]

REPRESENTING [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: Option 5, At-grade improvements, is best. Start with this less costly, less destructive solution. Reset the timing of the traffic signals to accommodate east-west traffic, which is heaviest. North-south traffic on NW military hwy, is not nearly as bad. Addition of a signal at Fairfield Bend would help tremendously

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibetta Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search Wurzbach Parkway at NW Military

From: TB
To: [28422C WA#3 - NW MILITARY INTER Project Email](#)
Subject: Wurzbach Parkway at NW Military Hwy Open House Comment Form
Date: Saturday, September 09, 2017 6:33:37 AM
Attachments: [page1image440.png](#)



COMMENT FORM

Wurzbach Parkway at NW Military Hwy Open House

Barshop Jewish Community Center

Phone: (210) 901-5124 Email: wurzbach@wsp.com

Mail: WSP USA
Attn: Ibetta Cavazos
Bank of America Plaza
300 Convent St., Suite 1330 San Antonio, TX 78205

Dear Ms. Cavazos:

Thank you for the opportunity to comment on the proposed traffic improvements for this stretch of Wurzbach Parkway. I live in this area of San Antonio and use this roadway frequently. I also represent over 2000 San Antonio Sierra Club members. I attended the August 30th Open House at the Jewish Community Center, reviewed displays and discussed them with staff. I have reviewed on line documents.

I summarize my comments as follows:

1. The least bad alternative, which we would favor is Alternative 5, At Grade Improvements.
2. None of the options presented for the Wurzbach/NW Military intersection is acceptable. By 2030 even the Modified SPUI shows total interchange LOS of D in am, and E in pm. This will be achieved over less than 10 years from construction completion, for \$28.5 million. This is a poor return on investment.
3. SPUI with signals at that interchange achieves total LOS of C in am, and pm, for \$29 million, over the same time period.
4. All other alternatives do far worse. However, At Grade Improvements (Alternative 5) at that Interchange show a 70% reduction in Total Interchange Delay in am, and a 68% reduction in pm. This is achieved for a cost of \$7.5M. This is a much better return on investment than others. SPUI by contrast achieves 86%, and 91% reductions, for \$29M.
5. All of the displayed alternatives add traffic lanes (except alternative 6 No Build). NW Military is generally shown with ten lanes at the Interchange, Wurzbach Pkwy with eleven. This will create a huge interchange right in these neighborhoods. This follows the tradition of building roads THROUGH neighborhoods, destroying them in the process. It follows the tradition of counting SECONDS of delay at an intersection as of paramount importance.
6. The shown provisions for bicycle and pedestrian traffic would be laughable if they weren't so frightening. Tracking the tiny bike lanes through the spaghetti of high speed traffic lanes is daunting. There are no safety studies provided, and there is nothing here at all to suggest that this project intends to do anything EXCEPT speed traffic through this Interchange. This is totally at odds with COSA efforts for a Vision Zero pedestrian and bicycle safety record. There is nothing SMART STREETS about this plan.
7. There is also NO provision for any transit options. Presumably buses mixed with the traffic will benefit equally from the improved LOS.
8. It is interesting to note also that the Wurzbach/Lockhill Selma Interchange shows NO benefits from the Wurzbach/NW Military Interchange improvements. In 2030, regardless of alternative pm LOS at Lockhill Selma is graded "F". Am LOS is graded "E" for all but the No Build Alternative.
9. I see nothing detailing traffic speeds or signal functions. I suspect that there is a built in assumption that there will NEVER be a walk signal activated, or a bicyclist needing safe passage.

The Alamo Sierra Club does not support yet another plan to add more traffic lanes, and maybe a couple more underpasses, to get a few more years of better traffic flow before returning to the same situation again, after millions of dollars spent. It is past time that COSA and TXDOT should abandon this never ending spiral of road construction leading to more traffic leading to more construction. I am familiar

with the headaches involved at this intersection at some times of the day. Instead of enduring years of added headache with construction, to gain less than ten years of mild improvement, TXDOT needs to examine ways to reduce traffic, enhance transit, enhance bike and pedestrian travel, support Vision Zero, build Smart Streets. Real Bike lanes that are separate and safe are needed. Intersections that design FOR safe pedestrian and bike crossing are needed. Dedicated transit lanes are needed.

Sincerely,

Terry Burns, M.D.
Chair, Alamo Group, Lone Star Chapter, Sierra Club

From: Eric Weis
To: [28422C WA#3 - NW MILITARY INTER Project Email](#)
Subject: Wurzbach Parkway traffic relief for NW Military & Wurzbach
Date: Friday, September 08, 2017 7:44:26 PM
Attachments: [083017-comment-form.pdf](#)

To whom it may concern:

I wanted to make sure my voice is heard. After reviewing all the options. My vote would be for the Option 1.

I have included the comment form in this email.

Best Regards,

Eric Weis

From: Scott & Missy Dewey
To: [28422C WA#3 - NW MILITARY INTER Project Email](#)
Subject: NW Military and Wurzbach Pkwy Comment
Date: Thursday, September 07, 2017 8:04:03 PM

Name: William and Melissa Dewey



We feel that the best option for the intersection as proposed is Option #1 SPUI with signals. The only negative for option 1 is that there is no ability to leave the Summerfield neighborhood exit off of Wurzbach Pkwy or stores between neighborhood exit off of Wurzbach and NW Military and go west on Wurzbach over NW Military, there is a forced right or left onto Military. Thus, Option #2 (Modified SPUI with frontage rds) with signals would be best, but that does not appear to be an option. I do feel that having the proposed lights at Fairfield Bend and Alon several months prior to the construction would be beneficial for traffic flow purposes.

My least favorite option is no-build.

Summary:

1st choice: Option 1

2nd choice: Option 2

Best choice: Option 2 with addition of lights as proposed on Option 1

Worst choice: No build

Thank you.

From: Dixie Doodle
To: [28422C WA#3 - NW MILITARY INTER Project Email](#)
Subject: No Tunnel
Date: Thursday, September 07, 2017 6:28:16 PM

I am in favor of plan option # 1, but only if it DOES NOT HAVE A TUNNEL. I would like an overpass bridge instead of any tunnel. TUNNELS FLOOD and make unsafe passage.



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: MICHAEL C. GRIFFITH

ADDRESS: [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: I drive through the intersection
an average of twice daily, seven days a week.
I support Option 5, along with a new traffic light
at Fairfield @ NW Military. I am concerned that
"flyover" or "tunnel" options will only move the
traffic problem to Lockhill Selma @ Wurzbach, still
backing up to NW Military. Thank you.

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibetta Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military

The environmental review, consultation, and other actions required by applicable Federal environmental laws for this project are being, or have been, carried-out by TxDOT pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 16, 2014, and executed by FHWA and TxDOT.



COMMENT FORM
Wurzbach Parkway at NW Military Hwy Open House
Barshop Jewish Community Center

August 30, 2017

(PLEASE PRINT)

NAME: Alicia Rubio

ADDRESS: [REDACTED] [REDACTED]

REPRESENTING: [REDACTED]

(Texas Transportation Code, §201.811(a)(5)): check each of the following boxes that apply to you:

- I am employed by TxDOT
- I do business with TxDOT
- I could benefit monetarily from the project or other item about which I am commenting

COMMENTS: We desperately need help to safely
get out of our neighborhood,
Summerfield.

Please consider option # 2 and also
consider adding a traffic light on
fairfield bend and nw military

Deadline for Comments: September 15, 2017

Please submit by – Phone: (210) 901-5124
Email: wurzbach@wsp.com
Mail: WSP USA
Attn: Ibetta Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



Or go to txdot.gov and keyword search
Wurzbach Parkway at NW Military

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TARJETA DE COMENTARIO
Wurzbach Parkway y NW Military Exhibición Abierta al Publico
Barshop Jewish Community Center

30 de agosto de 2017

(POR FAVOR IMPRIMA)

NOMBRE: _____

DIRECCION: _____

REPRESENTO: _____

(Código de Transporte de Texas, §201.811(a)(5)): Marque cada una de las siguientes cajas que se aplican a usted:

- Soy empleado de TxDOT
- Hago negocios con TxDOT
- Puedo aprovechar monetariamente del proyecto o cualquier otro elemento sobre que estoy comentando

COMENTARIOS: _____

Fecha límite para comentarios: 14 de septiembre de 2017

Por favor, envíe por – Teléfono: (210) 901-5124
Correo electrónico: wurzbach@wsp.com
Correo: WSP USA
Attn: Ibette Cavazos
Bank of America Plaza
300 Convent St., Suite 1330
San Antonio, TX 78205



O ir a txdot.gov y búsqueda con la palabra clave Wurzbach Parkway at NW Military

NW MILITARY AND WURZBACH PARKWAY OPTIONS

INSTRUCTIONS: Please provide comments on what you like and dislike about each option shown.

1



Single Point Urban Intersection (SPUI) with Signals

PROS: Eases congestion

CONS: No option for Summerfield residents
To continue on Wurzbach west

2



Modified SPUI with Frontage Roads

PROS: Summerfield residents can continue
On Wurzbach west

CONS: Nonr

3



Diamond Interchange

PROS:

CONS: Congestion would be the same

(Continued on reverse side)

4



Partial Grade Separation

PROS: _____

CONS: It doesn't solve any problems.

5

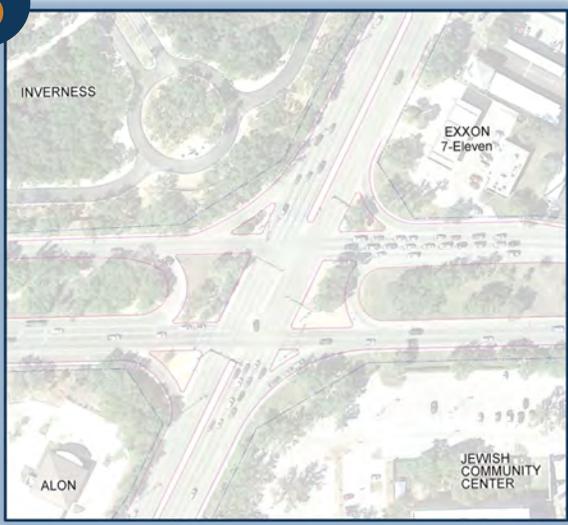


At-grade Improvements

PROS: _____

CONS: Minimal help

6



No-Build

PROS: _____

CONS: It is unsafe to get out of Summerfield during peak hours. Not doing anything puts lives in danger.

From: Armstrong, Scott D.
To: [28422C WA#3 - NW MILITARY INTER Project Email](#)
Subject: FW: TxDOT Internet E-Mail
Date: Wednesday, September 06, 2017 4:32:42 PM

Scott D. Armstrong, PE
Senior Engineering Manager

-----Original Message-----

From: Clayton Ripps [REDACTED]
Sent: Wednesday, September 06, 2017 4:06 PM
To: Armstrong, Scott D. [REDACTED]
Cc: Richard De La Cruz [REDACTED] >
Subject: FW: TxDOT Internet E-Mail

Scott, please add to comments for the meeting.

-----Original Message-----

From: Jefferson Grimes
Sent: Wednesday, September 06, 2017 3:44 PM
To: Clayton Ripps
Cc: Amy Redmond; Jonathan Bean
Subject: FW: TxDOT Internet E-Mail

Hello Clayton and trust you are well. The San Antonio District may have also received the below comment but I am forwarding it to you for your handling as you deem appropriate. For our records, if you would plz let me know how this is handled I would appreciate it. Take care.

Jefferson Grimes
Director of Public Involvement

[REDACTED]

-----Original Message-----

[REDACTED]
Sent: Wednesday, September 06, 2017 3:21 PM
To: Jefferson Grimes
Subject: TxDOT Internet E-Mail

Name: Ms. Susan Graham <[REDACTED]>
Address:
Wurzbach Parkway-I believe Option 1 is best San Antonio, TX 78231

Phone:
[REDACTED]

Requested Contact Method: Email

Reason for Contact: Customer Service

Complaint: No

Comment: Option 1 will work best. I have lived in the area 21 years and the traffic congestion problems are severe. This appears the only option that will give relief. Option 2 is second choice.

[Connecting Texans to what matters most. Texas Department of Transportation: 1917-2017 #txdot100]

<<http://www.dot.state.tx.us/txdot100/>>

Transcript of the Testimony of
TxDOT OPEN HOUSE

Date:

August 30, 2017

Case:

WURZBACH PARKWAY: LOCKHILL SELMA TO NW MILITARY

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TEXAS DEPARTMENT OF TRANSPORTATION
OPEN HOUSE
WURZBACH PARKWAY: FROM LOCKHILL SELMA ROAD TO NW
MILITARY HIGHWAY
BEXAR COUNTY, TEXAS

BARSHOP JEWISH COMMUNITY CENTER
12500 NW MILITARY DRIVE
SAN ANTONIO, TEXAS 78231

AUGUST 30, 2017
6:00 P.M. - 8:00 P.M.

Page 2

1 MS. C. VON BERTRAB: So I would like
 2 Option 5 because I really do not believe that the others
 3 will solve the problem completely and this is a much
 4 more cost-efficient project. It's not going to take as
 5 long and it won't impact the trees and -- and the
 6 environment as much. I really think this is the
 7 simplest solution -- solution to the problem, and I
 8 really reject the ones with the underpass because the
 9 cars will reach Lockhill-Selma at a very high speed and
 10 I'm very concerned about that.
 11 That's it. My number is (210)492-0407.
 12 My e-mail is CEBertrab3@me.com.
 13 *****
 14 MR. H. LEVA: So my name is Hitesh Leva.
 15 So I -- it -- What seems highly inefficient here is that
 16 they're not increasing the lane count across the artery.
 17 I'm not an -- I'm not a transportation engineer, but I
 18 am an engineer, and I can tell you that when I try to
 19 cross Northwest Military on Wurzbach some times of the
 20 day the issues that I'm facing are that the lights are
 21 highly inefficient.
 22 The bulk of these problems can be resolved
 23 by just fine tuning and optimizing the duration of the
 24 traffic lights, and when I've asked across the stations
 25 to the people presenting have they explored that option,

Page 3

1 I didn't quite get a clear answer. So I think before we
 2 spend any tax dollars, No. 1, I would really like for
 3 that to be explored.
 4 No. 2, by adding all -- all -- by forcing
 5 some of us residents who live in the neighborhood to be
 6 subjected to this horrible construction period, it's not
 7 some -- what some of us owned up to, signed up to,
 8 No. 1. No. 2, there is a succinct possibility that our
 9 real estate value could get compromised in which case
 10 the City has to really step up to the plate and -- and
 11 fix those issues.
 12 *****
 13 MR. L. TERRACAS: Well, I -- I have a
 14 question and I'm -- and I'm wondering why is it we've
 15 had all of this work on the Wurzbach Parkway, which is
 16 great, but they have never resurfaced the area from
 17 Lockhill-Selma going to Vance Jackson. They have done
 18 all of the other sections going down, and then the
 19 sections beyond Vance Jackson going -- going south, but
 20 that area has been neglected. And I've asked the people
 21 with the Texas Department of Public Safety and I just
 22 found out that that's because that's the City's section,
 23 and I'm -- my -- my comment and, I guess, my question,
 24 if the City has taken care of all of the other areas why
 25 do they neglect that area for years.

Page 4

1 It never -- it has not been resurfaced.
 2 They just fix the potholes. So I'm wondering if we can
 3 get it done any time within the next 12 months so that
 4 would be fixed, and it wouldn't take as much money as
 5 some of the other projects they're talking about. Thank
 6 you.
 7 And then we've seen an increase in traffic
 8 in the last, oh, six to seven years since they started
 9 doing the Parkway. So I would think that in
 10 consideration of all of the additional traffic flow that
 11 they've brought that they would have fixed that section
 12 or the -- at least, repave it. Thank you.
 13 *****
 14 MS. M. DAWSON: They haven't come up with
 15 a plan yet to save the trees. There are over 200 trees
 16 in the Parkway.
 17 *****
 18 MR. J. NICHOLS: Okay. I would just like
 19 any of the options to include better safety measures for
 20 bikers on Wurzbach Parkway. None of the examples that
 21 I've seen here show anything other than just striped
 22 lines, and, as it is now, traffic that is cutting across
 23 those bike lanes do it without any consideration to the
 24 bikers at all, so if there is any way that they could
 25 make the bike lanes separate or safer or put a curb up

Page 5

1 or something to protect the bikers better, I think, it's
 2 going to be safer for all of the bikers and the drivers
 3 in the long run.
 4 *****
 5 MS. D. NICHOLS: All I wanted to say is
 6 the same comments that he made but also relative to
 7 pedestrians. We need a safe means to not only cross,
 8 but to be able to walk the sidewalks and -- and feel
 9 safe.
 10 *****
 11 MR. J. RITCHIE: Okay. What we're -- what
 12 I'm talking about is, maybe, instead of all of the
 13 construction and everything, do a minimal construction
 14 and take lights and direct traffic.
 15 Have four -- four lanes in the afternoon
 16 going east -- no -- going one way in the afternoon, slow
 17 down the traffic going this way, and change the lanes
 18 with the lights, whether they would be green lights for
 19 changing the lanes to five lanes for everybody to get
 20 where they're going; and then in the mornings turn it
 21 the other way to where the -- the high traffic comes
 22 this way and the little traffic goes that way; the three
 23 or four cars that go that way aren't use -- using it.
 24 That would be my idea is to have light
 25 signals with the big red X's that this lane is closed

Page 6

1 until -- like they do on toll lanes or something like
 2 that. They will put up extra lanes rather than --
 3 that's the most cost-effective way to do it; it should
 4 work real good that way as I'm thinking.
 5 Because I'm budget minded, I have four
 6 kids and four cars and insurance to pay for and
 7 budgeting; and I look out for the community because I'm
 8 involved in the church and so forth, try to help
 9 everybody. So that's my input. Thank you.
 10 *****
 11 MR. N. MORGAN: Okay. My comment goes to
 12 some of the things that have not been discussed.
 13 The first thing is, the right-of-way for
 14 putting drainage in to this new highway, where is the
 15 drainage going to be put, and what is the right-of-way
 16 and where is it going to? From Jones-Maltsberger down
 17 to Wurzbach, it's like you could take a skate ball --
 18 board at the top at Huebner and skateboard down
 19 whatever -- without stopping. That's a giant hill and
 20 it's a giant floodplain.
 21 I talked to the gentleman tonight who is
 22 doing the environmental study. There is a lot of
 23 animals in the area. The deer in -- in Elm Creek, the
 24 Elm Creek people are not going to be able to get out of
 25 their gates or get back in, in the afternoon because of

Page 7

1 the traffic flow through there. It's going to be slow
 2 as can be.
 3 My idea for this thing is -- and I've
 4 worked with AACOG, A-A-C-O-G. My idea is for the
 5 neighborhood and -- to hire a public relations firm,
 6 each put in \$2,000 a piece or \$1,000 a piece, have them
 7 hire a lawyer, and then start a public relations
 8 campaign against this project to alleviate some of the
 9 weaknesses in it which will ruin neighborhoods for years
 10 to come.
 11 I am a realtor and I own a real estate
 12 company with 250 agents and we know that the value of
 13 these homes, because of that Parkway going through, are
 14 going to go down. It will affect them. They won't know
 15 how badly, but it will be bad.
 16 So I think that when it goes to vote or
 17 whoever does the final thing -- I know at -- at AACOG
 18 meetings that the proposal has gone for the vote before
 19 whoever votes for it, and then they take it off the
 20 table, take it in a back room, come back, and it's
 21 changed, and they vote on it, and you don't even know
 22 they changed it.
 23 Now, this is the way the politics work on
 24 this. I think it's a done deal, I think it's a slam
 25 dunk, and unless we hire professional people who know

Page 8

1 how to handle this situation I think you might as well
 2 not even be here. This is all for show, this night,
 3 tonight.
 4 Okay. One other item has got to do with
 5 the Alon Shopping Center. And the owner of that center
 6 is very interested, I'm sure, in keeping the traffic
 7 slowed down so that they can enter into that huge Alon
 8 Shopping Center, which is one of the biggest in the
 9 city. If this goes through and they bypass it, he's not
 10 going to be happy, and he's, I think, in -- I probably
 11 think he's working in the background to get the traffic
 12 to be able to get off and get in to his shopping center
 13 at Military and Wurzbach. It's a beautiful shopping
 14 center.
 15 So that's going to be an interesting
 16 thing. And I think he's a big player in it that nobody
 17 knows about except for the state, and I think that
 18 probably the people that want to -- to change the
 19 patterns of this could probably talk to Mr. -- I can't
 20 remember his name right now -- it's something in -- I'm
 21 drawing a blank. But anyway, he -- he's the owner of --
 22 of the Alon Shopping Center. That should about take it.
 23 Thank you.
 24 *****
 25 MR. H. CERDA: Okay. I'm just here to

Page 9

1 just -- I live in Alon Estates, and when we heard about
 2 this project we were a little excited that we're going
 3 to see some improvements on the traffic as we try to
 4 egress from our resident area in to Wurzbach Parkway.
 5 But then we've heard, the residents heard or we learned
 6 that it was going to stop, this improvement was only
 7 going to go to Lockhill-Selma, so then it was this
 8 traffic is just going to be pushed from Lockhill-Selma
 9 all the way to I-10.
 10 So what we wanted to do is, say, okay, out
 11 of all of these, we don't want really no improvements,
 12 but we were told by TxDOT we're going to do something,
 13 we have to do something, so we just want to make it
 14 known that we want the "At Grade" improvement. Because
 15 we'll still keep some natural trees there, they'll be
 16 reduced to a one -- one kind of lane or one median, and
 17 we would prefer that instead of a tunnel which would --
 18 then if it gets flooded when we have good rain here in
 19 San Antonio, or if there is an accident, there's
 20 still -- traffic is going to be backed up and they're
 21 going to wind up, you know, going on the access road,
 22 which is then -- will still create problems for us.
 23 So I just want to state for the record as
 24 a resident of Alon Estates that, you know, if there's
 25 going to be improvement, we'd rather have the "At Grade"

Page 10

1 improvement, which would be \$7.5 million instead of
 2 spending \$28.5 million.
 3 *****
 4 MR. C. SALVATO: I am absolutely opposed
 5 to the new roadway situation here. I'm a resident in
 6 Alon Estates and I think that the impact that it's going
 7 to have on our property values, just from the long
 8 construction period, and, ultimately, the minimal amount
 9 of time that the difference between the way the traffic
 10 is now and the way that it will be when this is
 11 completed is not worth the headache that we're all going
 12 to go through while this is underway; and the cost, in
 13 my opinion, certainly does not -- is -- is not worth it.
 14 That's it.
 15 *****
 16 MS. K. BOWMAN: So my name is Katherine
 17 Bowman and I'm a homeowner in an adjacent neighborhood,
 18 so right here at this intersection.
 19 I am vehemently opposed to the major
 20 construction options that are being considered,
 21 primarily because I feel like it's extremely
 22 shortsighted and highly irresponsible of both the State
 23 Department of Transportation as well as our City road --
 24 roadworks in terms of planning. They're talking about a
 25 \$20 million fix to this intersection of Northwest

Page 11

1 Military and Wurzbach.
 2 The next situation which will come is the
 3 intersection at Lockhill-Selma after that, which is all
 4 under City control up to close to I-10, there is not
 5 room or accessibility or -- or -- or real estate to
 6 accommodate the future traffic flows that this is going
 7 to send that way. Extremely shortsighted and highly
 8 irresponsible.
 9 When I was talking with the State
 10 Department of Transportation representative here
 11 tonight, he, basically, said as much. I don't
 12 understand how he can come and represent and -- and
 13 support this project and feel comfortable about it when
 14 there have been a complete lack of studies and
 15 understanding of what's going to happen on the next
 16 project. People are calling it kicking -- kicking the
 17 can down the road.
 18 I'm really dis- -- I'm really disappointed
 19 about that, and I think that those steps, those studies,
 20 and that understanding of what the traffic is going to
 21 look like and how it's going to be handled past the
 22 Lockhill-Selma intersection has got to be looked at and
 23 understood very clearly before this project is decided
 24 upon.
 25 The State and the City of San Antonio have

Page 12

1 a responsibility to their citizens to do that thoroughly
 2 and not just spend state money because it's here and
 3 it's available. Thank you.
 4 *****
 5 MS. M. JOFFE: Okay. My name is Margie
 6 Joffe, I live in Alon Estates. Wurzbach Parkway seems
 7 to be busy only two times a day: Morning and afternoon
 8 during the week just like any other freeway in San
 9 Antonio. I don't want 65-mile-an-hour traffic going
 10 past my neighborhood. I feel like that when the traffic
 11 comes off of Wurzbach Parkway or goes on to Wurzbach
 12 Parkway with the SPUI options we're going to have
 13 excessive speeds. I don't want excessive speed going
 14 past my nice quiet neighborhood. We have a lot of
 15 walking people, a lot of bike riders, people walking
 16 their dogs, children. I just don't want it.
 17 I think there will be an environmental
 18 impact by taking out the trees. I think that the noise
 19 will bother the animals at Hardberger Park. At a cost
 20 of \$30 million to save 30 seconds per car, I don't think
 21 that that is a good way to spend our money. I think the
 22 stewardship that we need to look at is to put this money
 23 where we need it.
 24 If this is a ten-year plan, what are the
 25 ten-year plans for 1604 and 410? Wurzbach Parkway took

Page 13

1 ten years to finish. This small stretch, how long is
 2 that going to take? I -- I know that TxDOT will do
 3 something. I don't know what. Doing nothing I
 4 understand is not an option, but a full blown
 5 \$30 million project with jackhammers from 8:00 to 3:00
 6 with dust for three years, no. No, thank you. Please
 7 save my trees, consider the people that live in the
 8 neighborhood. I don't want a freeway going past my
 9 window. No. I just don't. Thank you.
 10 *****
 11 MR. J. GRISSITH: Okay. So I think the
 12 options of going with the -- the SPUI interchange is a
 13 little bit shortsighted given that the traffic is just
 14 going to move one block away to Lockhill-Selma and this
 15 is going to affect both directions. I have grave
 16 concerns about the -- the construction going on and the
 17 construction that will happen to build the tunnel
 18 underneath Northwest Military.
 19 I -- I think the best long-term solution
 20 is to improve 1604 and to alleviate some of the traffic
 21 that will be -- that is increasing the demand on
 22 Wurzbach Parkway. I think the "At Grade" improvements
 23 or changing the timing of the signals is the most
 24 responsible way to approach it right now. It's the
 25 cheapest solution. They're the ones that can be the

Page 14

1 most easily corrected or changed in the future if it
 2 turns out they don't work. I just -- I don't see the
 3 need to go to such a drastic fix right now before we
 4 actually have evidence that they're -- they're going to
 5 fix the problem. Thank you.
 6 *****
 7 MR. K. McLAUGHLIN: Okay. So my name is
 8 Kevin McLaughlin and I just wanted to state for the
 9 record that I'm -- I am very much concerned and -- and
 10 against the idea of having any underpass going under
 11 Military Highway via Wurzbach interchange. I believe
 12 it's going to affect the value of our homes with -- in a
 13 very significant and negative way.
 14 My wife and I spent an exhaustive amount
 15 of time trying to find a place after I retired from the
 16 Air Force and now that we've been here for a couple of
 17 years and we -- we don't want to see it -- our
 18 investment destroyed here.
 19 Furthermore, I don't think there's going
 20 to be any improvement that's going to be caused by this
 21 project. The efficiencies I am not convinced are there.
 22 We move the problem down the street, which makes it a
 23 bigger problem for my community, and at the same time
 24 Texas -- the City of San Antonio has also stated that
 25 there are no more improvements that can be made beyond

Page 15

1 Lockhill-Selma which then implies, to me, that there is
 2 really no efficiency coming out of this at all.
 3 So unless they can make this happen all
 4 the way through I-10 or the Medical Center, they are not
 5 going to realize the kind of efficiencies they state
 6 they will. So otherwise this is an extreme waste of
 7 money and a bad idea. That's it.
 8 *****
 9 MR. A. SANTIAGO: In Option No. 2 I want
 10 to see if they can add a traffic light at the
 11 intersection of Northwest and Alon Loop. Also you --
 12 can you stop typing?
 13 Also I want to see the possibility of
 14 one -- of a pedestrian overpass over Northwest Military
 15 Drive joining one side with the entrance of Phil
 16 Hardberger Park.
 17 Okay. And then Option 5 -- Option 5 I'd
 18 also have the same petitions that are in Option 2; the
 19 same traffic light and also a pedestrian overpass.
 20 We'll see what happens. Okay. Thank you very much.
 21 *****
 22 MR. W.H. HAWKINS: Okay. Okay. W.H.
 23 Hawkins. I live at 12114 Wildrose Hill in the Estates
 24 of Alon and I'm opposed to almost all of the options.
 25 I -- I only think that Option 5 and 6 are the -- the

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1 only two viable options. Because they're just going to
 2 move traffic down from Northwest Military to
 3 Lockhill-Selma, it's going to back up right behind my
 4 house. I back up to Wurzbach. It's going to cause my
 5 property value to decrease, we're going to have noise,
 6 we're going to have smog/pollution from the cars sitting
 7 there at the light. I just feel like it's a -- it's a
 8 gross waste of taxpayer money to do these -- these
 9 \$20 million tunnels and things like that.
 10 Plus it's going to inconvenience our whole
 11 neighborhood for years just during the construction
 12 phase, and it's not going to really do that much because
 13 they haven't made any plans west of Lockhill-Selma.
 14 It's just going to bottleneck. There's no expansion
 15 plans for -- for west of Lockhill-Selma, there's no room
 16 to expand. It's -- it's -- I -- I believe it's just a
 17 waste of taxpayer money. Thank you.
 18 *****
 19 MS. S. BAILLARGEON: I think of all of the
 20 plans, number -- No. 5 is the only one that makes any
 21 sense in terms of helping with traffic and not creating
 22 an even greater danger for the pedestrian traffic by
 23 encouraging people to use the Parkway and then Wurzbach
 24 itself to access the Medical Center, that by ending the
 25 Parkway at Military and encouraging people to use

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1 Military as an access to Huebner they would have a much
 2 safer route in to the Medical Center and we would no
 3 longer be hitting one pedestrian every other month and
 4 killing one pedestrian every year. Just last week
 5 driving to the Medical Center on Wurzbach, I had to
 6 drive around a lady that had been hit in the
 7 intersection that was laying and waiting for an
 8 ambulance.
 9 We have a huge pedestrian population and a
 10 huge population of children on the very street that
 11 we're encouraging people to use by making the kinds of
 12 improvements that an underpass would create. So
 13 although it may move traffic a little bit, it's killing
 14 people and that's not what the City ought to be doing.
 15 *****
 16 MS. C. MORRISON: Okay. I have yet to see
 17 the option of exploring intelligent lights on this
 18 intersection to I-10, or throughout Northwest Military,
 19 that would improve the traffic flow rather than shift
 20 the bottleneck. Intelligent lights are a much lower
 21 cost, it's something that hasn't been explored on a
 22 relatively new intersection, and it makes no sense to me
 23 to spend millions of dollars on a project without having
 24 tried more efficient options.
 25 This project is invasive. The homeowners

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1 who would be affected by this are horrified by the
 2 impact this will have. Commuters will be facing years
 3 of congestion to avoid a problem of, basically, two
 4 minutes in the afternoon and a minute in the morning
 5 according to TxDOT traffic statistics.
 6 Because these options have not been
 7 explored -- Pardon me. Because the intelligent lights
 8 I've seen haven't been explored, all of the options
 9 except for one keep reverting to this underpass option,
 10 it seems to me very much a pet project that I cannot
 11 find any rationale behind.
 12 *****
 13 MS. K. PEACOCK: So I'm a resident of Alon
 14 Estates, I'm here tonight to actually, you know, get
 15 more information regarding the options that we have.
 16 I've been to all of the meetings, I still do not have an
 17 understanding of why this project was even brought
 18 about.
 19 When I first heard of TxDOT's plan, I
 20 didn't realize that their jurisdiction stopped at
 21 Lockhill-Selma, so I think this is, in fact, a very -- a
 22 misuse of taxpayer dollars to do any of these
 23 improvements. My understanding is something has to be
 24 done, so I would say then, you know, to do where they --
 25 kind of the minimal invasiveness, which is expanding the

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1 lanes on Wurzbach Parkway.
 2 Even with that, I think the -- the impact
 3 to commuters is going to be very minimal given that it
 4 will stop at Lockhill-Selma. I understand that there
 5 are better intersections to have traffic at, but,
 6 regardless, we're going to have traffic.
 7 I commute to the Medical Center every day,
 8 sometimes in very high peak times, sometimes I come out
 9 of my gate and I'm at -- sitting at my desk within 12
 10 minutes. So I live there and I don't mind sitting in
 11 the traffic.
 12 So I think by the time that TxDOT were
 13 to -- if they go with this -- this SPUI option, which
 14 I'm very much against (the tunnel), by the time they
 15 spend the years that it's going to take to get that
 16 done, we're going to have to contend with that
 17 construction that -- and I think the commute will be
 18 even longer than what we have now. By the time they
 19 were to finish that, hopefully 1604's work would be
 20 done, and I think some of this traffic is going to be
 21 moved over to 1604 anyway, so it would be a moot point
 22 to do this.
 23 I'm very concerned, as a resident living
 24 in Alon, the impact that it will have on my community,
 25 the safety for my community, so I'm -- I'm very much

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1 against doing anything. I would much rather see these
 2 TxDOT funds go to other places in San Antonio or, heck,
 3 now re-route it to Houston. Lord knows they're going to
 4 need more help. So I think there is plenty of places to
 5 put these funds than having someone's, you know, pet
 6 project be funded, so -- and most likely the individuals
 7 who are making these decisions don't reside in our
 8 community -- communities in this area anyway. So that's
 9 all I have.
 10 *****
 11 MS. S. RUPAREL: Okay. So I'm a resident
 12 of Alon Estates and I want to get to the bottom line
 13 first, that I am strictly against -- I am strictly
 14 against this development. So given a choice I would
 15 pick Option C, which is a "No Build," and the reason for
 16 the -- the reason for this is that I do not see any
 17 benefit in making this development or this construction
 18 at all in terms of traffic freedom, because we -- I and
 19 others in my community feel that it's -- it's going to
 20 back up at Lockhill-Selma and create -- and create a
 21 bottleneck there and it's just really not going to serve
 22 any purpose at all.
 23 In addition to that, it's going to --
 24 we'll have to deal with the construction for a couple
 25 years and that is going to affect our property values,

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1 when there is a lot more changes that need to be made in
 2 the City to begin with, and the money that's going on
 3 is -- it's a lot of money to just focus on one
 4 intersection that is not even on the top most congested
 5 areas in -- in the country, in the state. So really I
 6 really don't understand why we need to do this. And I
 7 live there and I commute through that -- those roads all
 8 the time and I've never had issues.
 9 The other thing, having said that, also
 10 that the -- the congestion is really for one hour in the
 11 morning and one hour in the evening, and just five days
 12 a week, it's clear otherwise, so there's really no
 13 issues at -- at all other than those two hours during
 14 the day to put in \$30 million. And they say that it's
 15 going to decrease time by 30 seconds to 60 seconds,
 16 which is not a lot anyways, so I really, at the end of
 17 the day, don't see a point of why so much money should
 18 be put in, in a congestion -- in an intersection like
 19 that.
 20 But if I had to pick up a second option,
 21 it would be Option 5 because of its obvious pros and
 22 pros that the trees will be maintained, and the
 23 construction and the money put in is going to be much
 24 less, the time put in is going to be much less, and it
 25 is the least amount of the change that is going to be

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1 put in. So -- so I pick Option 5 as a second option,
 2 but my first option a "No Build."
 3 *****
 4 MS. L. MARIE: This project doesn't make
 5 sense. It will improve for a block or two, but you
 6 still have issues heading west beyond Lockhill-Selma on
 7 Wurzbach. The amount of money spent is not worth the
 8 time and the labor and -- to get the end product.
 9 I wish we could get a light for our
 10 neighboring community in Summerfield so they can exit
 11 safely, and I do think there is room for improvement,
 12 but I don't see how fixing one intersection is going to
 13 change the problem beyond. It's just delaying the
 14 problem.
 15 And because TxDOT is responsible for
 16 Wurzbach heading east and San Antonio is responsible for
 17 Wurzbach heading west of Lockhill-Selma the two need to
 18 communicate and work together. And it doesn't seem like
 19 the allocation of funds is wisely spent when we need it
 20 somewhere else in the community. I would like to know
 21 who's motivated to change this and why they're motivated
 22 to spend this money and how this became a priority over
 23 other -- several other intersections in Bexar County. I
 24 think it's corrupt. That's it.
 25 *****

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1 MS. R. DE LEON FUENTES: Well, I am a
 2 homeowner at the Estates of Alon and I disagree with
 3 Options 1, 2 and 3 of their proposed improvements
 4 because we do not benefit from -- from these proposals.
 5 We -- there is no access to these improvements from our
 6 neighborhood. Plus getting rid of the median and the
 7 trees, that will adversely affect on the value of our
 8 homes which is -- that I'm most concerned about. It
 9 will impact negatively on the value of the homes, so
 10 that -- that's real important to me.
 11 If anything must be built, Option 5 will
 12 be preferred for -- at least, for -- for living at the
 13 Estates of Alon that would be my preference, Option
 14 No. 5, or "No Build," because I think that they should
 15 look in to -- we -- we know for a fact that they are
 16 working on 1604 in a few years, they should probably
 17 focus more on solving the problem they have on 1604,
 18 because they -- this -- Wurzbach Parkway is going
 19 across, traffic is going across Wurzbach Parkway trying
 20 to get to 281 and 1604.
 21 And, as I said, these first three options
 22 do -- do not benefit my neighborhood, so why do we want
 23 to be bothered by all of this construction and all of
 24 this chaos if we don't even have access to it. We are
 25 the ones that live there and we don't benefit from it.

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1 Thank you. That's all.
 2 *****
 3 MS. I. SANTIAGO: Okay. I would like to
 4 request a pedestrian overpass on Military Highway right
 5 by the Hardberger Park. That's -- I don't think that
 6 we -- so far that we need a light over there. You know,
 7 everybody's been talking about getting a light there. I
 8 think if we get a pedestrian overpass there that's
 9 enough.
 10 And if I have to go by what is the
 11 favorite for me I -- maybe No. 1 would be more lanes,
 12 which is No. 5, or then No. 2, you know, if that's --
 13 we're going to get it because we have the feeling that
 14 they already have the money allocated for this and that
 15 they're going to do it. So if they're going to do it
 16 then give us the opportunity to go straight on Wurzbach
 17 Parkway or go right or left from Wurzbach Parkway, so I
 18 think No. 2 would be the best one.
 19 *****
 20 MS. A. MESKI: Okay. As a resident of the
 21 Estates of Alon I am quite, you know, against Option
 22 Nos. 1, 2, 3, 4, and my best option is No. 6, "No Build"
 23 at all, and if worse comes to worse No. 5.
 24 My question is, is it going to be, you
 25 know, taken into consideration the exit from the Estates

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1 of Alon using Military Parkway, is there going to be a
 2 light, you know, in the future for the residents to move
 3 in and out, and if I -- I understand it's not linked to
 4 this particular project as of now. This is my question,
 5 basically, if Option No. 1, 2, 3, 4 will be considered,
 6 and 5, would there be a light, you know, at the entrance
 7 of the Military Parkway and the Woods of Alon and the
 8 Estates of Alon subdivisions. That's it.
 9 *****
 10 MS. V. VELA: Okay. My name is Victoria
 11 Vela and I live in Alon Estates. I am against this SPUI
 12 with signals option because it limits my access to exit
 13 out of my neighborhood and continue straight to Wurzbach
 14 to 281. The only option I would have was -- is to go
 15 through another neighborhood called Alon Woods and go
 16 through four lanes to cross over, and then eventually go
 17 on to Wurzbach Parkway to 281.
 18 Another reason I don't want this
 19 construction is because it's going to be at least two
 20 years to build, so I'm going to have all of this
 21 construction, you know, behind my neighborhood limiting
 22 my access again to get out. Also, with this
 23 construction, I would see an increase in traffic. That
 24 would only help increase traffic up to the
 25 Lockhill-Selma intersection, which eventually will be

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1 backed up so we'll have more congestion and -- and
 2 noises. I live closer to the -- to the intersection of
 3 Lockhill-Selma and Wurzbach Parkway, so I would hear
 4 more noise and interruption in -- in my living.
 5 The option I opt for, if there was
 6 anything to be done, would be the "At Grade"
 7 improvements where they built more lanes and not do the
 8 tunnel option. It would be cheaper, there would be less
 9 construction and less tree damage. I think the money
 10 that should be used -- that they're use -- that they
 11 want to use for this improvement should be used at 1604
 12 or 410 to expand those highways so people would have
 13 other ways or other means to get to work besides going
 14 through my neighborhood and increasing traffic.
 15 I think it definitely benefits other
 16 people getting through there, but not me. I -- You
 17 know, during those peak hours, I really don't travel,
 18 and I just see the traffic of other people in San
 19 Antonio trying to get through, so I -- I'm definitely
 20 against this, the SPUI with signals. That's it.
 21 *****
 22 MR. B. SUKANYA LEAN: Okay. I am a
 23 resident of Alon Estates. I've been here about three
 24 years now -- two, two and a half years. I like Option 5
 25 to build an intersection between Northwest and Wurzbach

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1 Parkway because, No. 1, low cost construction, low
 2 cost -- low impact on -- on construction; No. 2, retain
 3 some trees and not kill all the trees to -- to widen up
 4 the lane; and No. 3 is low cost on construction, I think
 5 the least cost, because it costs less than, you know,
 6 making overpass or tunnel. No. 4, we need some light or
 7 sign or signal before you get in to Northwest Military
 8 gate, because it's hard and sometimes I just pass it,
 9 you go through it and miss the turn all the time, you
 10 know, so we need something warning before you get in to
 11 the gate on Northwest Military. That's all my concern.
 12 Thank you very much for -- for the presentation open
 13 house here today. That's very helpful. Thank you.
 14 *****
 15 MR. D. MANSOUR: So I would just like to
 16 say to TxDOT, as a resident of the Estates of Alon,
 17 which is a neighborhood that is directly next to the
 18 proposed said construction site, that I am very much
 19 against any of these options with the exception of
 20 No. 5, which is the most -- which is the "At Grade"
 21 improvement option.
 22 I feel that TxDOT should take into
 23 consideration opinions from our neighborhood very --
 24 very much so because we are directly impacted. We are
 25 next to the area, we live there, we walk there, we bike

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1 there, and we have to see and listen and drive in this
 2 area every day for the rest of our lives because we live
 3 there.
 4 This proposed project in its infancy was
 5 to build a tunnel under Northwest Military. We feel
 6 that any option that incorporates a tunnel is a horrible
 7 option because all it does is increase the flow of
 8 traffic and causes traffic to back up at Lockhill-Selma.
 9 TxDOT has admitted that there are no plans
 10 to improve beyond Lockhill-Selma because there's -- just
 11 no further improvements can be made, so why spend this
 12 huge amount of money to build a project that all it's
 13 going to do is push traffic from one intersection to
 14 another.
 15 Furthermore and more importantly, we have
 16 been told by numerous sources that this will drastically
 17 affect our property values of the neighborhood we live
 18 in; and I think this is evident in the fact that nowhere
 19 in San Antonio is there a tunnel like this in a
 20 residential neighborhood.
 21 There certainly are examples of this and,
 22 in all of the examples we can find, none of them are
 23 ever in a residential area. These are usually SPUI
 24 options -- S-P-U-I -- are usually done in commercial
 25 areas surrounded by businesses and commerce, not

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1 neighborhoods.
 2 I think the environmental aspect of the
 3 project where you're causing -- you're getting rid of
 4 green space for paved roads, again, is going to affect
 5 drainage which is going to affect our -- the way we
 6 live. Also I would implore TxDOT to pay attention to
 7 City initiatives, which is to place a greater emphasis
 8 on the walkability and bike-ability, and the extension
 9 of all of these lanes diminishes the ability for people
 10 to walk and bike to the various locations in the area;
 11 such as the grocery store, church, the park, Starbucks
 12 coffee, all of these are within walking distance and
 13 people can be witnessed using these walking trails on a
 14 regular and daily basis.
 15 Lastly, there are residents who live up
 16 against Wurzbach who will have to hear the constant flow
 17 of traffic on a regular basis. My name is David
 18 Mansour, I'm the president of the HOA Estates of Alon,
 19 the Estates of Alon Homeowners Association, and as a
 20 representative of my neighborhood I would just like to
 21 formally put down that our neighborhood is strongly
 22 opposed to any graded options, any tunnel options. We
 23 are for Option 5 which is the "At Grade" improvements.
 24 Thank you very much.
 25 *****

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1 MS. E. MENDOZA: So, first of all, I think
 2 that they -- you're -- TxDOT is increasing the traffic
 3 through a neighborhood, and we're moving the traffic
 4 congestion from Northwest Military to Lockhill-Selma and
 5 it's going to bottleneck there.
 6 We know that Nirenberg lives in
 7 Summerfield, we know that he's probably behind pushing
 8 this through because his residents that live in that
 9 neighborhood they have no access out of their
 10 neighborhood because of the traffic that was created by
 11 Wurzbach Parkway. Common sense would say that they
 12 could put a dedicated barricaded exit from their
 13 subdivision out on to Wurzbach Parkway, and then they
 14 would have the traffic light there at Military.
 15 Another thing that they need to do is give
 16 them an -- a traffic light in their main entrance. They
 17 have no traffic light so they can't exit. I understand
 18 their frustration, but barricade an exit so they -- a
 19 dedicated lane for them to exit out. They don't have
 20 one now.
 21 Increase the traffic light at Northwest
 22 Military. Double it maybe. I -- I don't know how long
 23 it is, but it seems to me that only two, three or four
 24 cars can pass at a time. We're creating a bottleneck
 25 and all we're doing is moving the traffic from Northwest

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1 Military to Lockhill-Selma at a cost of \$30 million
 2 which is a gain of these people that are traveling that
 3 don't even live in our neighborhood, it's a convenience
 4 for them, but they gain 30 seconds -- that's what we've
 5 been told -- at a cost of \$30 million.
 6 The other thing that I'm concerned about
 7 is that we're creating a traffic jam up Wurzbach to
 8 I-10. We have schools there. We have a Catholic
 9 school. We have a public school. The public school is
 10 refugees, a lot of refugee children go there. I know
 11 because I was a teacher there and I retired from there.
 12 Refugees do not know how to respond to increased
 13 traffic. They don't cross it at corners. They don't
 14 wait for lights. They dart across the roads. I've seen
 15 it traveling on the other side of I-10.
 16 So we're trying to move people from
 17 Wurzbach Parkway up to the Medical Center or
 18 Fredericksburg Road to go to USAA, you're going to
 19 create more and more fatalities. I know these kids get
 20 on the bus, but they -- a lot of them cross over the
 21 street, over Wurzbach, to go to Valero for the
 22 convenience stores. I've seen a fatality there. It's
 23 increasing your traffic.
 24 The thing I -- I say is that -- another
 25 thing that I -- I disagree with is getting rid of the

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1 trees that are -- that are in our esplanade. We're
 2 supposed to be a green city, we're supposed to promote
 3 health, we're destroying the trees and -- and
 4 eliminating the -- the air pollution that they absorb.
 5 The photosynthesis these trees absorb we're destroying
 6 that and that's a shame.
 7 I -- I'm in -- I'm concerned about the
 8 increased traffic, the noise, the devaluation of our
 9 houses. I already see that there is a -- there is a lot
 10 of houses in our neighborhood that are now going up for
 11 sale because of this mess that -- whoever created it.
 12 Common sense, to me, is to divert some of this money
 13 from our neighborhood that we do not want and put it at
 14 1604. If it's going to be a four-year project then let
 15 the four-year project be at 1604 when there is -- they
 16 need the increased traffic flow. This is a convenience
 17 for people that do not even live in our neighborhood at
 18 a convenience of 30 seconds gained -- gain time at a
 19 cost of \$30 million. That doesn't make any economic
 20 sense to me. Those people need to leave a little bit
 21 earlier instead of leaving late.
 22 This is -- most of the people that live in
 23 this -- we have a young community and we have a retired
 24 community, we don't want to fight getting out of our
 25 subdivision. We don't want to fight this additional

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1 traffic. We don't want to exit our beautiful
 2 neighborhood that we're looking at these trees and now
 3 we're just going to see concrete and steel for the
 4 barricades. We don't want to see it.
 5 Nowhere have we seen three-dimensional
 6 renditions. We only see flat copies, one dimension.
 7 Where are the people that -- you know, they're supposed
 8 to have these analyses? We want to see them.
 9 But we are very passionate about this, we
 10 don't want it in our community. I think it -- common
 11 sense has to prevail and, perhaps, diverting some of
 12 this money and using it to other cities that need it --
 13 the infrastructure because of the hurricane. I don't
 14 know if the money can be appropriated, but common sense
 15 has to take place in situations like this.
 16 Another thing, I -- we looked on TxDOT,
 17 the 100 -- the 100 most congested streets in the State
 18 of Texas, Wurzbach is not even on the map. 1604 is
 19 No. 43 or something like that. I don't know the
 20 specific number. So, again, they're not even on the
 21 radar right now, why are we spending this money? So
 22 most of the residents do not want it. So that's my
 23 strong opinion.
 24 *****
 25 MR. E. VELA: Okay. So I'm, basically,

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1 against most of these options, and, in particular, I'm
 2 surprised that Option 1 is still on the table after I --
 3 my -- well, my community and myself, we -- we got
 4 involved in the early stages a few months back when the
 5 first meeting was held -- open house -- and we learned
 6 about the -- the tunneling and that we wouldn't have --
 7 be able to exit our community to continue eastbound
 8 towards 281. We would either have to turn left to go
 9 northbound Military or turn right to go southbound
 10 Military. So at that point we got involved and we made
 11 sure our -- our voices were heard.
 12 We contacted the consummate, we contacted
 13 TxDOT and they, basically, mentioned that the first one,
 14 the first option probably isn't going to happen because
 15 of all of the concerns that we expressed, and so I'm
 16 here and surprised to see it's still an option. And, if
 17 anything, that's the one that most adversely affects our
 18 community, the community of the Estates of Alon. And
 19 so, basically, all of these options, except probably 5
 20 and definitely 6 are going to affect our community.
 21 You know, our -- our community has homes
 22 that are in the \$500-, \$600,000, \$700,000, and -- and,
 23 additionally, Inverness, the community across from us,
 24 and they have lots that just begin at \$250,000, and
 25 you -- you know, we're talking about property values

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1 increase -- excuse me -- decreasing, we're talking about
 2 increased noise from the increased traffic, but, I mean,
 3 additionally, most of these projects are 20 -- over
 4 \$20 million, and they're only going to alleviate the
 5 issue for a few years after construction is done, a two-
 6 to three-year construction, so not only do we have to
 7 endure three years of construction loudness, then we're
 8 going to have to endure increased traffic, traffic
 9 noise, increased accidents that already are happening at
 10 Lockhill-Selma and Wurzbach.
 11 And, basically, it's just pushing down
 12 the -- all of this congestion that's happening at
 13 Wurzbach and Northwest Military it's just pushing it
 14 down one block to Lockhill-Selma and Wurzbach, and so we
 15 really don't see the long-term value in -- in any of
 16 these options. But, at least, with the Option 5 it --
 17 TxDOT can spend their money, some -- some of their
 18 money, and we can have less of that annoyance that we're
 19 expecting.
 20 Additionally, I know Summerfield, the
 21 community of Summerfield, one of their main concerns is
 22 their inability to get out of their neighborhood. But
 23 if they were able to get that traffic signal that
 24 they -- at -- I think it's Fairfield and Military, I
 25 mean, that would alleviate most of their concerns. And

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1 we're talking about a residential area, this whole area
 2 here, and, basically, TxDOT wants to turn it in to a
 3 highway in the middle of a residential area and it's not
 4 fair to all of the communities in this area.
 5 Additionally, the community, I believe
 6 it's called Whispering Oaks, you know, if -- if they
 7 realize that, basically, all of that traffic congestion
 8 that is now at Wurzbach and Military it's going to be
 9 pushed down to Lockhill-Selma and Military, and it's
 10 going to back up their exits from their communities, I
 11 don't think they'll be as happy; and that's one of the
 12 points that I plan on getting across to the community in
 13 the -- in the very near future, to make sure they get
 14 involved and have their say in this process, because I
 15 don't think they'll be happy.
 16 I mean, that's really all I have to say.
 17 You know, I -- I just hope that we can go with Option 5
 18 or -- or preferably Option 6. It's not going to -- any
 19 of the options aren't going to do any -- anything of
 20 long-term value, not in my opinion. A lot of the
 21 traffic that currently -- Excuse me. A lot of the
 22 traffic that will begin flowing -- Well, excuse me, let
 23 me restate that.
 24 Once the construction is completed, it's
 25 just going to bring additional traffic in to our area

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1 and, in the end, we're going to be back in the same
 2 situation we are now, and, maybe, all we need is the
 3 green light at Wurzbach and Military to be on just a
 4 little bit longer than the 20 seconds it's set at right
 5 now. Thank you.
 6 *****
 7 MR. R. MENDOZA: So Option 1 through 4 I'm
 8 not even considering. I don't want a tunnel. I'm
 9 100 percent not behind a tunnel. Option No. 5 is
 10 probably the more practical -- if there has to be a
 11 change made, if there are imminently plans for change
 12 then Option No. 5 is probably the best use of money
 13 where you're not using the full \$30 million. I think
 14 it's \$18.5 million. I'm -- I would not be opposed to
 15 that.
 16 Option No. 6 is the absolute best option.
 17 It's where you do zero, leave things as they are.
 18 People that live north and -- north of town that need to
 19 get to the Medical Center or need to get to their USAA,
 20 to their place of work, they just need to be better
 21 planners, in my opinion, because this does -- the --
 22 the -- the small number of people that are worried about
 23 that crossing, that intersection of Northwest Military
 24 Highway and Wurzbach Parkway, it's probably about 50,000
 25 people going and coming, morning and evening.

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1 That's about one-half of one percent of
 2 the total population of San Antonio that we're going to
 3 spend \$30 million on. That's an absolute waste of
 4 money. I've been a taxpayer in this -- in the town
 5 forever and I will raise holy hell, and I'm going to get
 6 to the right people to let them know the Regional
 7 Mobility Authority should be here. They should be here.
 8 Nelson Wolff, County Commissioner, should be here, all
 9 of the people that are -- can move the dial one way or
 10 the other. Don't send the little underlings, the down
 11 line, to come in to defend this -- this move. Don't do
 12 it. Someone come up, step up to the plate and say, I'm
 13 the guy that's going to make the change on this deal.
 14 What -- what kind of questions do you have and have a Q
 15 and A, get some dialogue going.
 16 Because there would be a lot different
 17 story if someone got on the podium and felt the passion
 18 that people have that live in very, very high-dollar
 19 neighborhoods that they've invested a lot of money in,
 20 they've invested a lot of money in their homes, only to
 21 see the community, the Vistas, that whole ambiance of
 22 living in that area wiped away and -- and replaced with
 23 steel and concrete.
 24 And they can say that it's going to help
 25 you 50,000 people. Well, those 50,000 people aren't

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1 affected by that like I am. I live in that community,
 2 and I paid a lot of money for my home, and I want to be
 3 there for a while, and this is just going to help --
 4 this is going to devalue my property. And no one in
 5 this room, from TxDOT or otherwise, can convince me that
 6 it's not going to devalue the property. That's all I
 7 got to say. I've said enough. And you've been very
 8 kind.
 9 *****
 10 MS. T. MARTIN: Okay. I -- I would like
 11 to make my opinion known about this proposed
 12 construction at Wurzbach Parkway and Northwest Military.
 13 I am an advocate for Option 6 which is "No Build."
 14 I live in the area in one of the
 15 communities that backs up to Wurzbach Parkway, directly
 16 behind the -- the street is my home. The noise level
 17 that we experience at this time from traffic during the
 18 day and the night is very loud. There is sirens and
 19 traffic noise and I feel like this construction that
 20 they're proposing is going to increase the noise that we
 21 hear, and, therefore, decrease our quality of life.
 22 Secondly, the gate that backs up to
 23 Wurzbach Parkway is the gate that we use to enter and
 24 exit our community, and 1 through 4 options is going to
 25 restrict our use of that gate and restrict us to only be

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1 able to travel eastbound, and that's not something that
 2 I'm interested in. 1 through 4 are not options. The
 3 underpass is not something that we're interested in, in
 4 any way. It's a very costly endeavor and I don't feel
 5 like it's necessary.
 6 Being a resident of this community, I feel
 7 like the traffic is really not that bad at this
 8 intersection. During the week there are just a few
 9 hours of -- of traffic a day and, outside of that, it
 10 is -- it is not a congested area by any means. There
 11 are areas around town that are significantly more
 12 congested than this intersection and spending the amount
 13 of money that they're proposing on this project is not
 14 prudent in my opinion.
 15 I think that the No. 6 option "No Build"
 16 is the best option for the individuals that live in this
 17 area. I feel like our property values are going to be
 18 devalued with this construction that's being proposed.
 19 We pay a lot of money to live in this area, we
 20 contribute a lot of tax money, and this proposal is
 21 going to decrease the value of our homes, you know, and
 22 we've paid a lot of money for these homes.
 23 If construction is done then I feel like
 24 Option 5 would be the best, that it would not restrict
 25 our ability to travel in any direction at the

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1 intersection like Options 1 through 4 would, so I vote
 2 for "No Build." And if something's going to be done, it
 3 should be Option No. 5.
 4 *****
 5 MR. A. SAUCEDO: Okay. Option No. 5 is
 6 the best option that provides the least amount of
 7 disruption for the neighborhood at Alon Estates and
 8 still protects the trees and the environment and what we
 9 best like about our neighborhood. That is it.
 10 *****
 11 MR. S. SANDERS: Upgrade No. 5 seems to be
 12 the best option. It, No. 1, keeps all of the trees;
 13 No. 2, it allows for the flow of traffic from Wurzbach
 14 down to Lockhill-Selma. And unless you do the
 15 improvement all the way to Fredericksburg Road and you
 16 either go under or over Northwest Military,
 17 Lockhill-Selma, Vance Jackson, and then I-10, and then
 18 Datapoint and whatever else is down there towards
 19 Fredericksburg, you're not going to improve the time
 20 overall, because with the population increasing over the
 21 next 10 to 20 years there's, you know, just no way,
 22 unless you increase the -- again, all the way to
 23 Fredericksburg, you're not going to have any
 24 improvement. Everybody still stops at Wurzbach, and
 25 will have to, to either go right or left, and that's

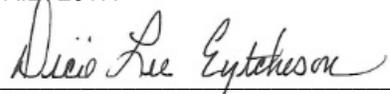
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1 just going to back up traffic all the way to Northwest
 2 Military and even -- even towards Blanco Road.
 3 So, again, Option No. 5 seems to be the
 4 most -- makes the most sense. Thank you.
 5 *****
 6 MS. J. MANSOUR: All right. So I am a
 7 resident of Alon Estates and recognize that we have a
 8 congestion issue at the intersection and am excited to
 9 see that there are some proposed improvements coming to
 10 the area. I am strongly opposed to any graded
 11 improvements because of the tree removal, because there
 12 is not a comprehensive plan beyond Lockhill-Selma to
 13 address congestion issues in that section and so you're,
 14 essentially, moving the issue from one intersection to
 15 another.
 16 I feel from a cost benefit that a better
 17 use of taxpayer dollars would be to implement the "At
 18 Grade" improvements, and to consider some improvements
 19 at the Wurzbach and Weidner intersection which will help
 20 from the 35 to I-10 commute, it would have, I think,
 21 likely, a bigger impact on the overall commute.
 22 I also would like to ask for consideration
 23 of a light outside of the Alon neighborhood off of -- at
 24 the Northwest Military exit with the "At Grade"
 25 improvement because accessing the U-turn will become a

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1 little more challenging with the additional two lanes.
 2 And, lastly, I am concerned about the
 3 safety of pedestrians and cyclists through the
 4 intersection.
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1 COURT REPORTER CERTIFICATION
 2
 3 COUNTY OF BEXAR)
 4 STATE OF TEXAS)
 5
 6 I, Dicie Lee Eytcheson, a Certified
 7 Shorthand Reporter in and for the State of Texas, do
 8 hereby certify that this transcript is as true and
 9 accurate as possible, transcribed by me through
 10 computer-aided transcription.
 11 And further certify that I am not a
 12 relative or employee of any of the parties hereto, nor
 13 interested directly or indirectly in the outcome of this
 14 session.
 15 WITNESS MY HAND, this the 14th day of
 16 September, A.D. 2017.
 17
 18 
 19
 20 Dicie Lee Eytcheson, Texas CSR 5392
 21 Expiration Date: 12/31/18
 22 Firm Registration No. 631
 23 Kim Tindall & Associates, LLC
 24 16414 San Pedro Avenue, Suite 900
 25 San Antonio, Texas 78232
 Phone: (210) 697-3400
 Fax: (210) 697-3408

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APPENDIX B

FIGURES



WURZBACH PARKWAY

NW Military Hwy/Lockhill-Selma

Open House

Barshop Jewish Community Center

August 30, 2017 – 6:00 to 8:00 PM



The environmental review, consultation, and other actions required by applicable Federal environmental laws for this project are being, or have been, carried out by TxDOT pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 16, 2014, and executed by FHWA and TxDOT.

A Partnership between:



and



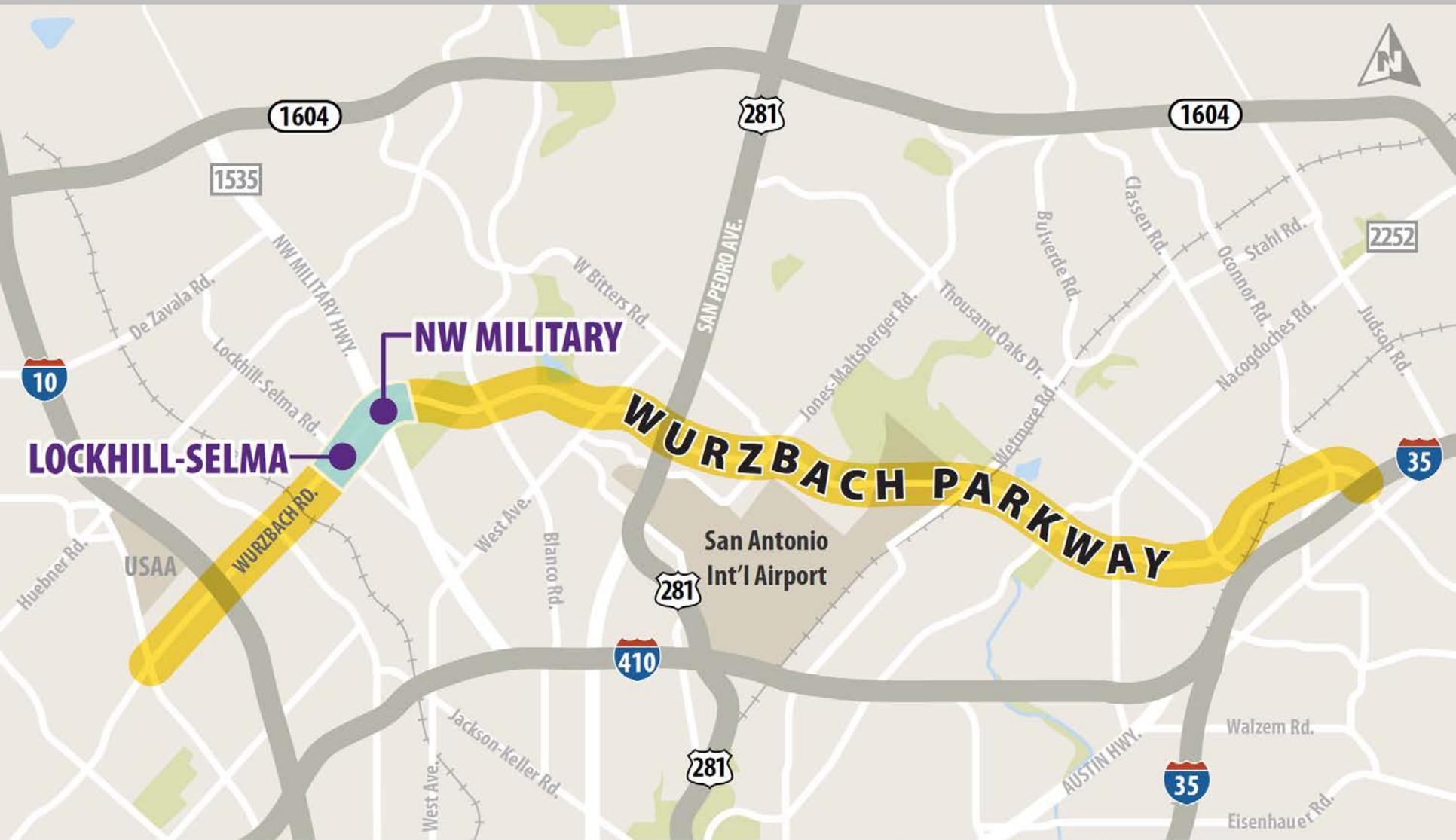
City of
San Antonio

Thank you for participating

Please plan to:

- Watch the presentation
- View the project information and schematics drawings of improvement options
- Visit with team members and ask questions
- Share your comments

Project Location



Public Involvement To Date

❑ Focus Group Meeting #1

July 14, 2016

Hardberger Park Urban Ecology Center

❑ Open House #1

February 28, 2017

Oak Meadow Elementary School

❑ Focus Group Meeting #2

June 13, 2017

Hardberger Park Urban Ecology Center

❑ Open House #2

August 30, 2017

Barshop Jewish Community Center

Focus Group Meetings attended by HOA presidents, elected officials, business reps, and agency reps

Open Houses attended by general public



■ Focus Group Meeting #1

- July 14, 2016 - Hardberger Park Urban Ecology Center
- 16 non-staff attendees

Reviewed several innovative and conventional intersection improvement options

- *Single Point Urban Interchange (SPUI)* with Wurzbach passing under NW Military was option most supported by the focus group

■ Open House #1

- February 28, 2017 - Oak Meadow Elementary School
- 249 public attendees

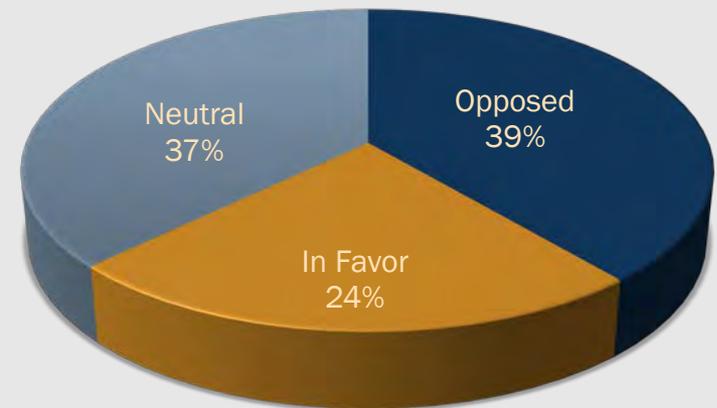
Presented schematic layouts, 3D animation, and traffic analysis depicting the *Single Point Urban Interchange (SPUI)* as the preferred technical option

Public Involvement To Date

- **Most common comments at Open House #1:**
 - Keep frontage roads
 - Analysis may not reflect actual neighborhood trips
 - Do not want increased congestion/delay at Lockhill-Selma
 - Need signals at neighborhood driveways
 - Keep trees
 - Construction impacts

249 Public Attendees
127 Comments Received

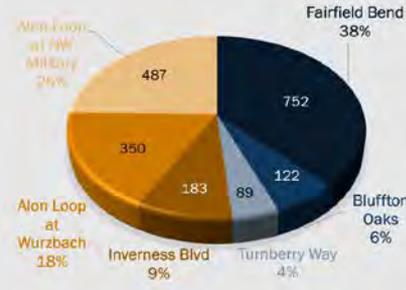
Position on Project



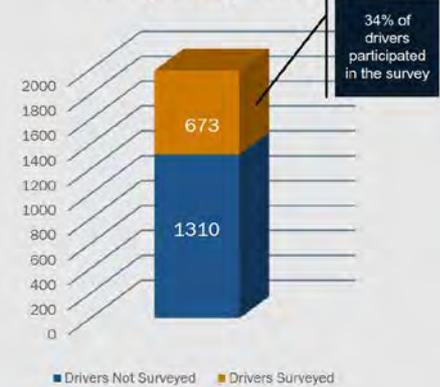
Travel Survey Results



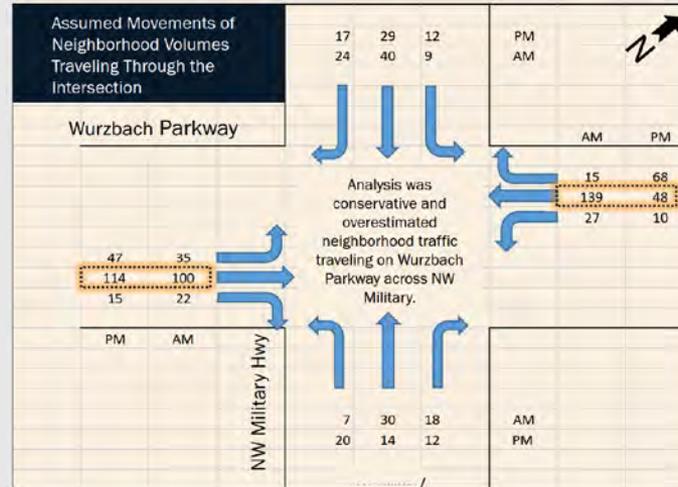
Total Vehicles Counted by Location



Survey Participation



Total Vehicles Counted by Location



Survey Results – Initial Assumptions were Conservative

- ① Single Point Urban Interchange (SPUI) with Signals
- ② Modified SPUI with Frontage Roads
- ③ Diamond Interchange
- ④ Partial Grade Separation
- ⑤ At-grade Improvements
- ⑥ No-Build

*Review Updated
Intersection
Options at the
Layout Tables*

■ Focus Group Meeting #2

- June 13, 2017 - Hardberger Park Urban Ecology Center
- 16 non-staff attendees

Discussed updated intersection options

Focus Group gave feedback on preferred options:

Options	Preferred Options
1 – SPUI With Signals	7 (44%)
2 – Modified SPUI (w/ Frontage Roads)	5 (31%)
3 – Diamond	0 (0%)
4 – Partial Grade Separation	1 (6%)
5 – At-Grade Improvements	3 (19%)
6 – No Build	0 (0%)

NEXT STEPS

- Gather Public Feedback
- Select Preferred Option
- Final Public Meeting/Hearing

Anticipated Construction Funding

- ❑ The Wurzbach Parkway project was identified as a priority by the Alamo Area Metropolitan Organization (AAMPO) for additional funding
- ❑ \$30 million of State, Federal, and Local funds have been identified

Provide Your Comments

- ❑ Leave a comment card
- ❑ Talk to the court reporter
- ❑ Email your comments to: wurzbach@wsp.com
- ❑ Mail your comments to:

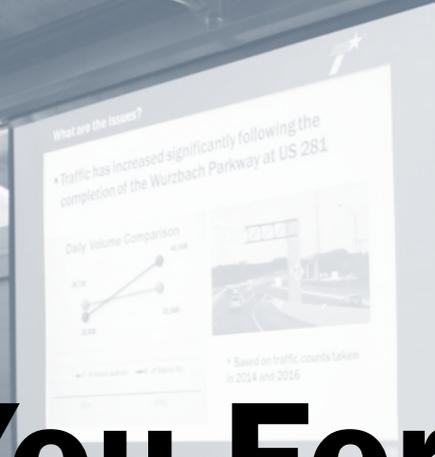
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San Antonio, TX 78205



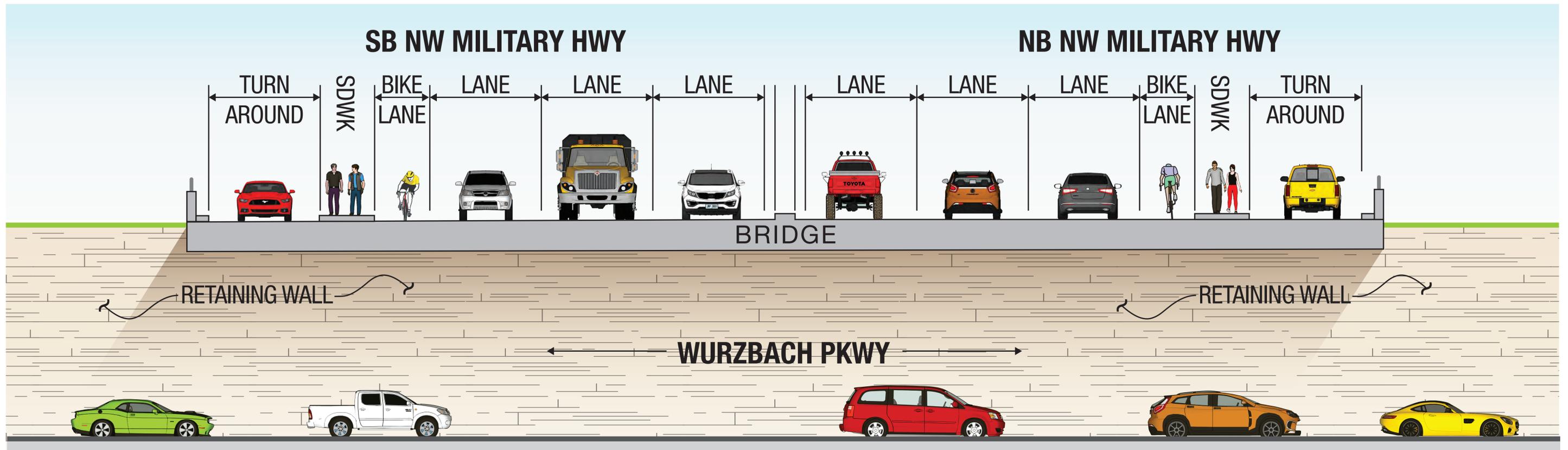
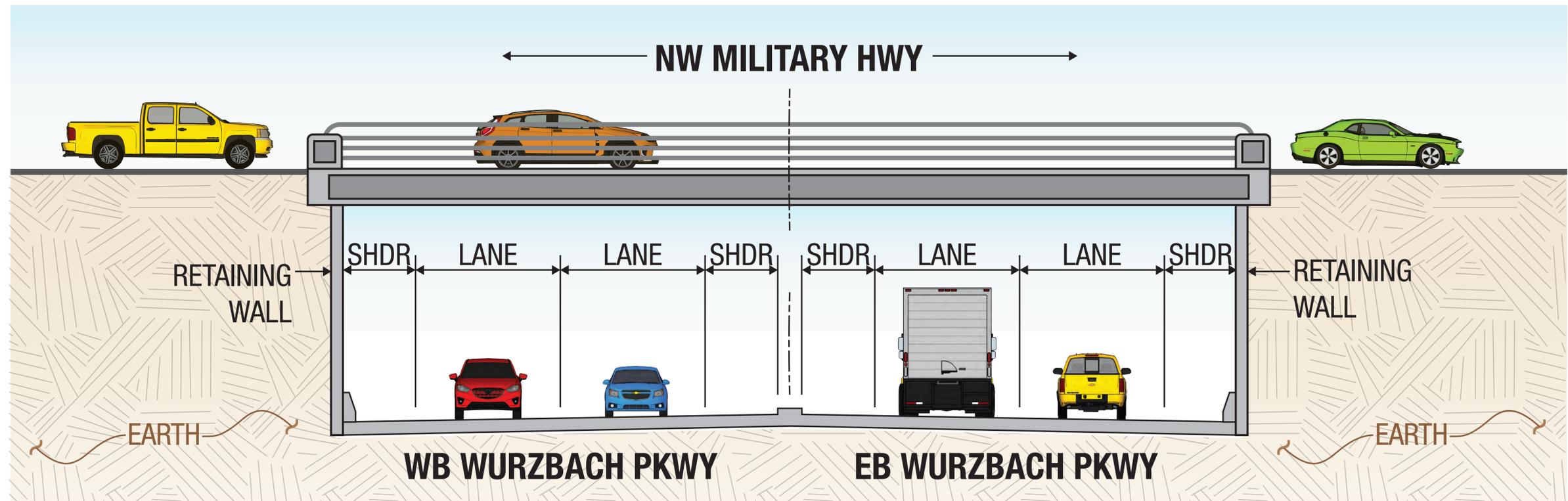
Comments must be submitted or postmarked by
September 15, 2017



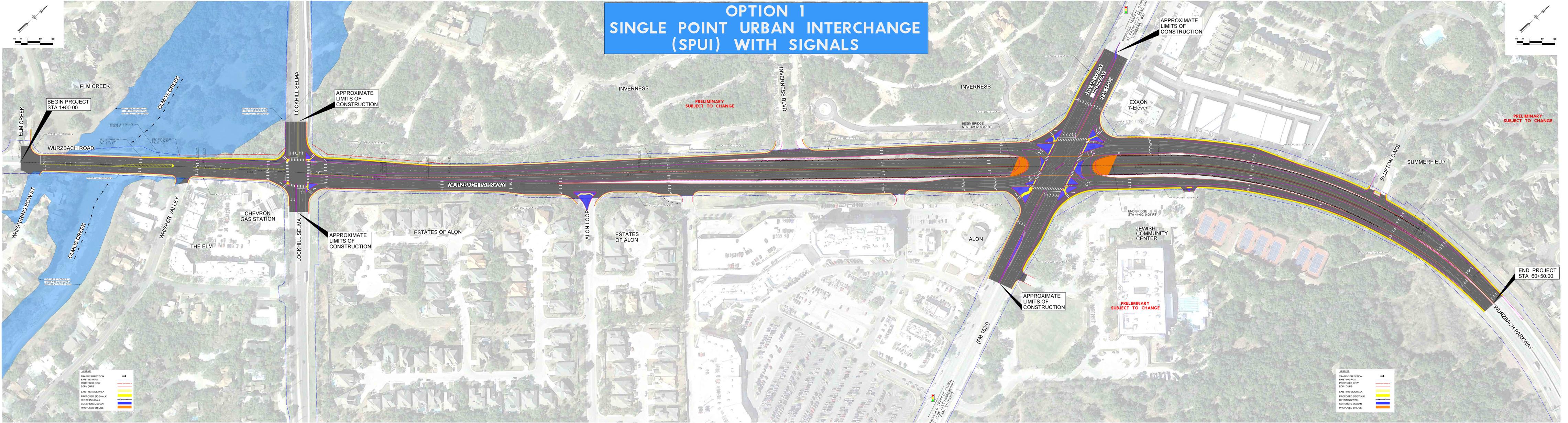
Thank You For Your Participation



Cross Sections at NW Military Hwy Bridge



OPTION 1 SINGLE POINT URBAN INTERCHANGE (SPUI) WITH SIGNALS



APPROXIMATE
LIMITS OF
CONSTRUCTION

PRELIMINARY
SUBJECT TO CHANGE

APPROXIMATE
LIMITS OF
CONSTRUCTION

APPROXIMATE
LIMITS OF
CONSTRUCTION

APPROXIMATE
LIMITS OF
CONSTRUCTION

END PROJECT
STA 60+50.00

BEGIN PROJECT
STA 1+00.00

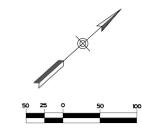
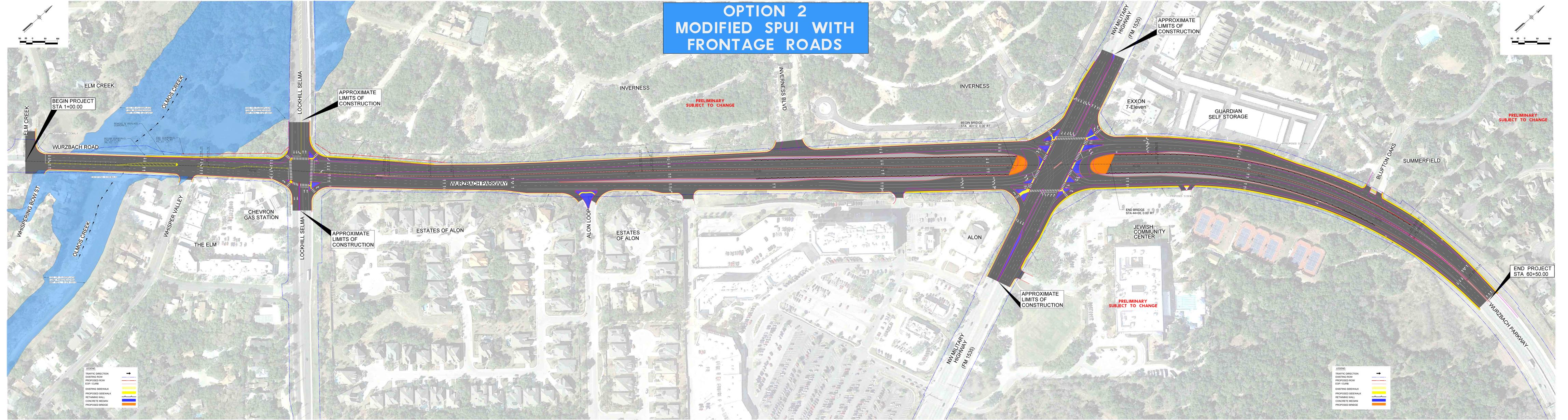
PRELIMINARY
SUBJECT TO CHANGE

PRELIMINARY
SUBJECT TO CHANGE

- LEGEND
- TRAFFIC DIRECTION
 - EXISTING ROW
 - PROPOSED ROW
 - EDP / CURB
 - EXISTING SIDEWALK
 - PROPOSED SIDEWALK
 - RETAINING WALL
 - CONCRETE MEDIAN
 - PROPOSED BRIDGE

- LEGEND
- TRAFFIC DIRECTION
 - EXISTING ROW
 - PROPOSED ROW
 - EDP / CURB
 - EXISTING SIDEWALK
 - PROPOSED SIDEWALK
 - RETAINING WALL
 - CONCRETE MEDIAN
 - PROPOSED BRIDGE

OPTION 2 MODIFIED SPU WITH FRONTAGE ROADS



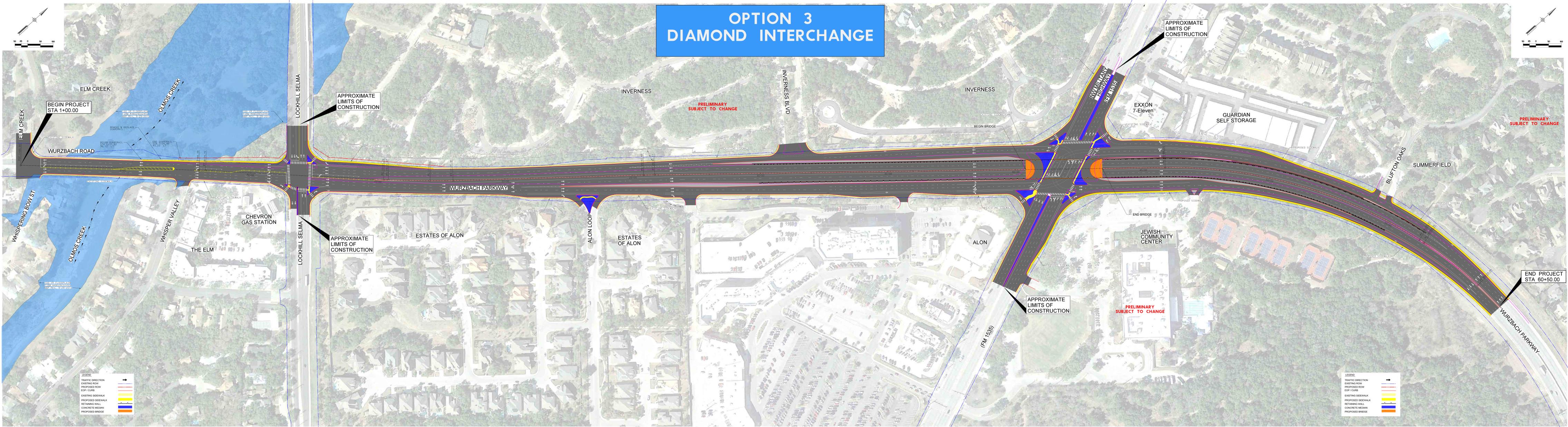
LEGEND

TRAFFIC DIRECTION	→
EXISTING ROW	—
PROPOSED ROW	—
EXISTING SIDEWALK	—
PROPOSED SIDEWALK	—
RETAINING WALL	—
CONCRETE MEDIAN	—
PROPOSED BRIDGE	—

LEGEND

TRAFFIC DIRECTION	→
EXISTING ROW	—
PROPOSED ROW	—
EXISTING SIDEWALK	—
PROPOSED SIDEWALK	—
RETAINING WALL	—
CONCRETE MEDIAN	—
PROPOSED BRIDGE	—

OPTION 3 DIAMOND INTERCHANGE



BEGIN PROJECT
STA 1+00.00

APPROXIMATE
LIMITS OF
CONSTRUCTION

PRELIMINARY
SUBJECT TO CHANGE

APPROXIMATE
LIMITS OF
CONSTRUCTION

APPROXIMATE
LIMITS OF
CONSTRUCTION

APPROXIMATE
LIMITS OF
CONSTRUCTION

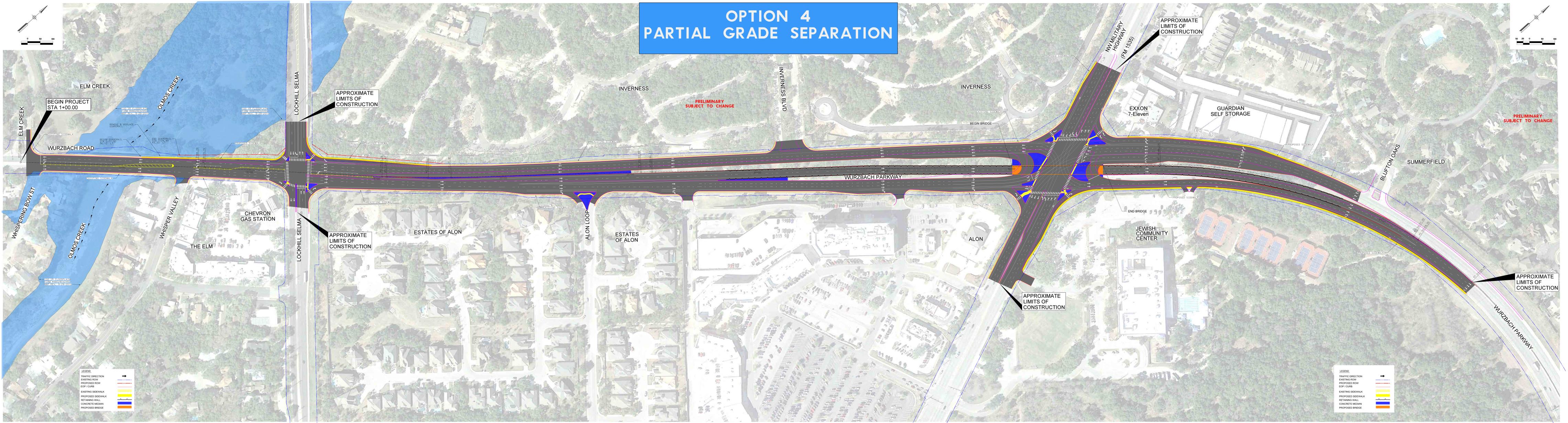
PRELIMINARY
SUBJECT TO CHANGE

END PROJECT
STA 60+50.00

- LEGEND
- TRAFFIC DIRECTION
 - EXISTING ROW
 - PROPOSED ROW
 - EXISTING SIDEWALK
 - PROPOSED SIDEWALK
 - RETAINING WALL
 - CONCRETE MEDIAN
 - PROPOSED BRIDGE

- LEGEND
- TRAFFIC DIRECTION
 - EXISTING ROW
 - PROPOSED ROW
 - EXISTING SIDEWALK
 - PROPOSED SIDEWALK
 - RETAINING WALL
 - CONCRETE MEDIAN
 - PROPOSED BRIDGE

OPTION 4 PARTIAL GRADE SEPARATION



BEGIN PROJECT
STA 1+00.00

APPROXIMATE
LIMITS OF
CONSTRUCTION

PRELIMINARY
SUBJECT TO CHANGE

APPROXIMATE
LIMITS OF
CONSTRUCTION

APPROXIMATE
LIMITS OF
CONSTRUCTION

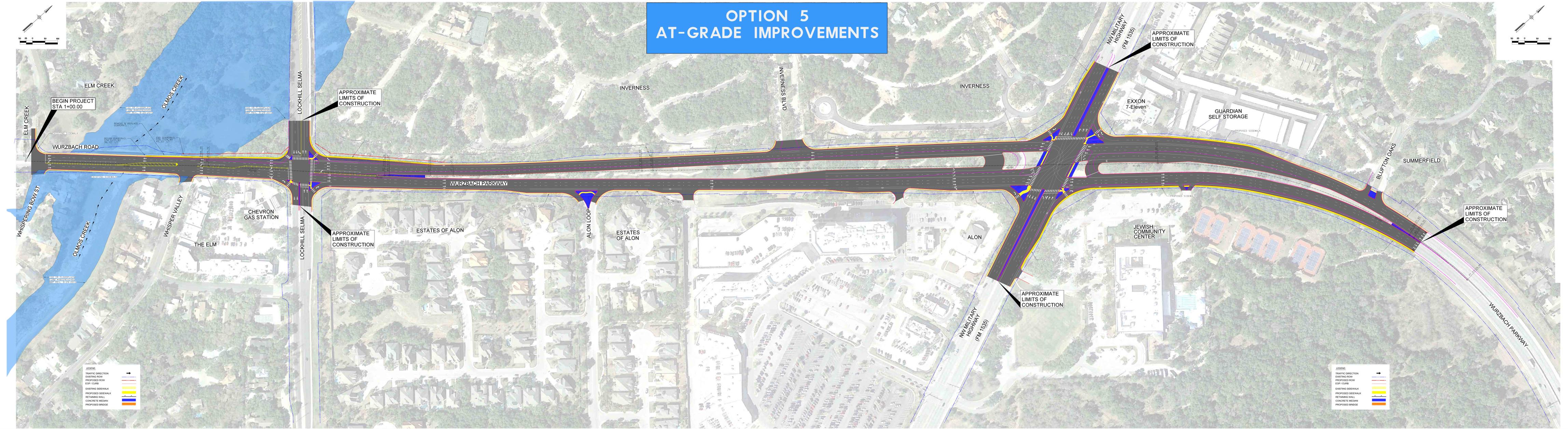
APPROXIMATE
LIMITS OF
CONSTRUCTION

APPROXIMATE
LIMITS OF
CONSTRUCTION

LEGEND
TRAFFIC DIRECTION
EXISTING ROW
PROPOSED ROW
EOP / CURB
EXISTING SIDEWALK
PROPOSED SIDEWALK
RETAINING WALL
CONCRETE MEDIAN
PROPOSED BRIDGE

LEGEND
TRAFFIC DIRECTION
EXISTING ROW
PROPOSED ROW
EOP / CURB
EXISTING SIDEWALK
PROPOSED SIDEWALK
RETAINING WALL
CONCRETE MEDIAN
PROPOSED BRIDGE

OPTION 5 AT-GRADE IMPROVEMENTS



BEGIN PROJECT
STA 1+00.00

APPROXIMATE
LIMITS OF
CONSTRUCTION

APPROXIMATE
LIMITS OF
CONSTRUCTION

APPROXIMATE
LIMITS OF
CONSTRUCTION

APPROXIMATE
LIMITS OF
CONSTRUCTION

- LEGEND
- TRAFFIC DIRECTION
 - EXISTING ROW
 - PROPOSED ROW
 - EDP / CURB
 - EXISTING SIDEWALK
 - PROPOSED SIDEWALK
 - RETAINING WALL
 - CONCRETE MEDIAN
 - PROPOSED BRIDGE

- LEGEND
- TRAFFIC DIRECTION
 - EXISTING ROW
 - PROPOSED ROW
 - EDP / CURB
 - EXISTING SIDEWALK
 - PROPOSED SIDEWALK
 - RETAINING WALL
 - CONCRETE MEDIAN
 - PROPOSED BRIDGE

ELM CREEK

OLMOS CREEK

LOCKHILL SELMA

INVERNESS

INVERNESS BLVD

INVERNESS

NW MILITARY
HIGHWAY
(FM 1535)

EXXON
7-Eleven

GUARDIAN
SELF STORAGE

SUMMERFIELD

BLUFTON OAKS

LOCKHILL SELMA

ESTATES OF ALON

ALON LOOP

ESTATES
OF ALON

NW MILITARY
HIGHWAY
(FM 1535)

JEWISH
COMMUNITY
CENTER

WURZBACH PARKWAY

WURZBACH PARKWAY

WURZBACH ROAD

WHISPERING BOY ST

WHISPER VALLEY

CHEVRON
GAS STATION

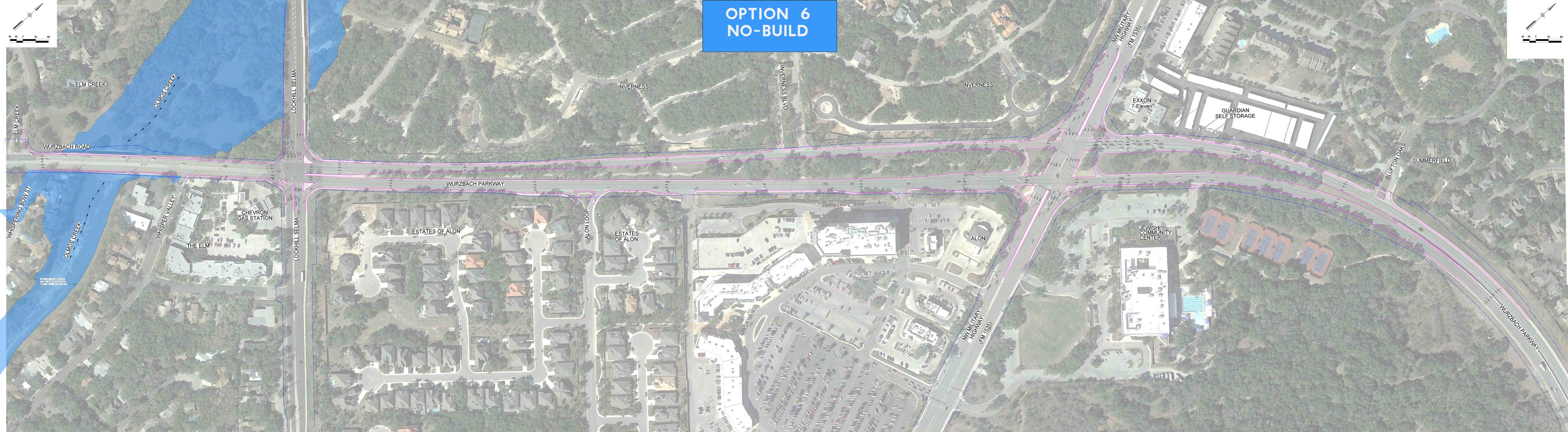
THE ELM

ELM CREEK

OLMOS CREEK

WURZBACH PARKWAY

**OPTION 6
NO-BUILD**



Level-of-Service (LOS) at a Signalized Intersection

A	B	C	D	E	F
<ul style="list-style-type: none">• No delay.• Arrive at the signal on green.• No waiting cars.• Not commonly seen except in very low volume locations or only during off-peak conditions.	<ul style="list-style-type: none">• Little or no delay.• May arrive on green.• May be a few cars waiting.• This LOS is seen in less populated or rural areas.	<ul style="list-style-type: none">• Some delay.• Some congestion.• Most cars make it through the signal when it turns green.• Desirable LOS in urban and suburban areas.	<ul style="list-style-type: none">• Noticeable delay.• May wait for more than one cycle.• Not all cars make it through when signal turns green• Considered a good LOS at intersections in metropolitan suburban and urban areas.	<ul style="list-style-type: none">• Congested conditions.• Will wait for more than one cycle.• Many cars do not make it through signal.• Congestion is becoming more than typically considered acceptable.	<ul style="list-style-type: none">• Very congested• May wait for two or more cycles.• Many cars do not make it through signal.• Indicates intersection is over capacity and cannot handle volumes.

Wurzbach Parkway @ NW Military: Performance Measures by Proposed Alternative

2017

Average Delay Experienced by Drivers (in seconds per vehicle)

Options/ Metric	Grade Separated				At-Grade	
	1	2	3	4	5	6
	SPUI With Signals	Modified SPUI	Diamond	Partial Grade- Separation	At-Grade Imp's	Existing
	AM Peak Hour					
Total Interchange Delay	25.6	37.1	46.8	55.3	54.4	182.1
WB Thru Delay	Free Flow	69.8	49.1	74.7	81.9	159.6
EB Thru Delay	Free Flow	36.5	55.7	Free Flow	61.9	187.2
NB NWM Delay	33.4	52.4	44.5	56.1	62.9	138.5
SB NWM Delay	10.0	34.2	44.3	52.8	49.0	59.8
	PM Peak Hour					
Total Interchange Delay	29.1	38.9	81.7	68.7	83.0	311.7
WB Thru Delay	Free Flow	110.0	47.1	117.8	90.6	157.8
EB Thru Delay	Free Flow	43.4	57.6	Free Flow	60.4	358.1
NB NWM Delay	28.5	49.3	50.9	80.0	78.0	477.1
SB NWM Delay	9.3	26.9	42.3	39.7	49.6	51.1

Level of Service for Each Option

Options/ Metric	Grade Separated				At-Grade	
	1	2	3	4	5	6
	SPUI	Modified SPUI	Diamond	Partial Grade- Separation	At-Grade Imp's	Existing
	AM Peak Hour					
Total Interchange Delay	C	D	D	E	D	F
WB Thru Delay	Free Flow	E	D	E	F	F
EB Thru Delay	Free Flow	D	E	Free Flow	E	F
NB NWM Delay	C	D	D	D	E	F
SB NWM Delay	B	C	D	D	D	E
	PM Peak Hour					
Total Interchange Delay	C	D	F	E	F	F+
WB Thru Delay	Free Flow	F	D	F	F	F
EB Thru Delay	Free Flow	D	E	Free Flow	E	F+
NB NWM Delay	C	D	D	E	E	F+
SB NWM Delay	A	C	D	D	D	D

Wurzbach Parkway @ NW Military: Performance Measures by Proposed Alternative

2030

Average Delay Experienced by Drivers (in seconds per vehicle)

Options/ Metric	Grade Separated				At-Grade	
	1	2	3	4	5	6
	SPUI With Signals	Modified SPUI	Diamond	Partial Grade- Separation	At-Grade Imp's	No Build
	AM Peak Hour					
Total Interchange Delay	30.5	48.8	97.5	116.7	106.2	353.1
WB Thru Delay	Free Flow	27.3	60.0	135.1	117.3	338.4
EB Thru Delay	Free Flow	42.5	56.7	Free Flow	83.1	360.1
NB NWM Delay	41.2	67.8	47.3	99.0	97.7	307.8
SB NWM Delay	13.5	40.6	58.1	61.6	94.0	174.6
	PM Peak Hour					
Total Interchange Delay	36.9	74.6	144.6	143.1	170.0	533.1
WB Thru Delay	Free Flow	48.2	47.1	38.4	68.8	157.8
EB Thru Delay	Free Flow	51.1	61.8	Free Flow	60.2	596.1
NB NWM Delay	42.7	74.6	68.1	190.7	241.8	767.7
SB NWM Delay	10.5	31.2	49.5	61.6	170.6	197.3

Level of Service for Each Option

Options/ Metric	Grade Separated				At-Grade	
	1	2	3	4	5	6
	SPUI	Modified SPUI	Diamond	Partial Grade- Separation	At-Grade Imp's	No Build
	AM Peak Hour					
Total Interchange Delay	C	D	F	F	F	F+
WB Thru Delay	Free Flow	C	E	F	F	F+
EB Thru Delay	Free Flow	D	E	Free Flow	F	F+
NB NWM Delay	D	E	D	F	F	F+
SB NWM Delay	B	D	E	E	F	F
	PM Peak Hour					
Total Interchange Delay	C	E	F	F	F	F+
WB Thru Delay	Free Flow	D	D	D	E	F
EB Thru Delay	Free Flow	D	E	Free Flow	E	F+
NB NWM Delay	D	E	E	F	F+	F+
SB NWM Delay	B	C	D	E	F	F

Wurzbach Parkway @ Lockhill-Selma: Performance Measures by Proposed Option @ NW Military

2017 Average Delay, LOS, and Queue Length for the WB Through Movement

NW Military Options/ Metric	Grade Separated				At-Grade	
	1	2	3	4	5	6
	SPUI With Signals	Modified SPUI	Diamond	Partial Grade- Separation	At-Grade Imp's	No Build
	AM Peak Hour					
Total Delay	38.0	40.6	37.9	46.9	34.8	42.4
LOS	D	D	D	D	C	D
WB Thru Queue Length (feet)	538			435		527
	PM Peak Hour					
Total Delay	50.2	53.1	49.2	51.7	53.5	97.3
LOS	D	D	D	D	D	F
WB Thru Queue Length (feet)	387			336		429

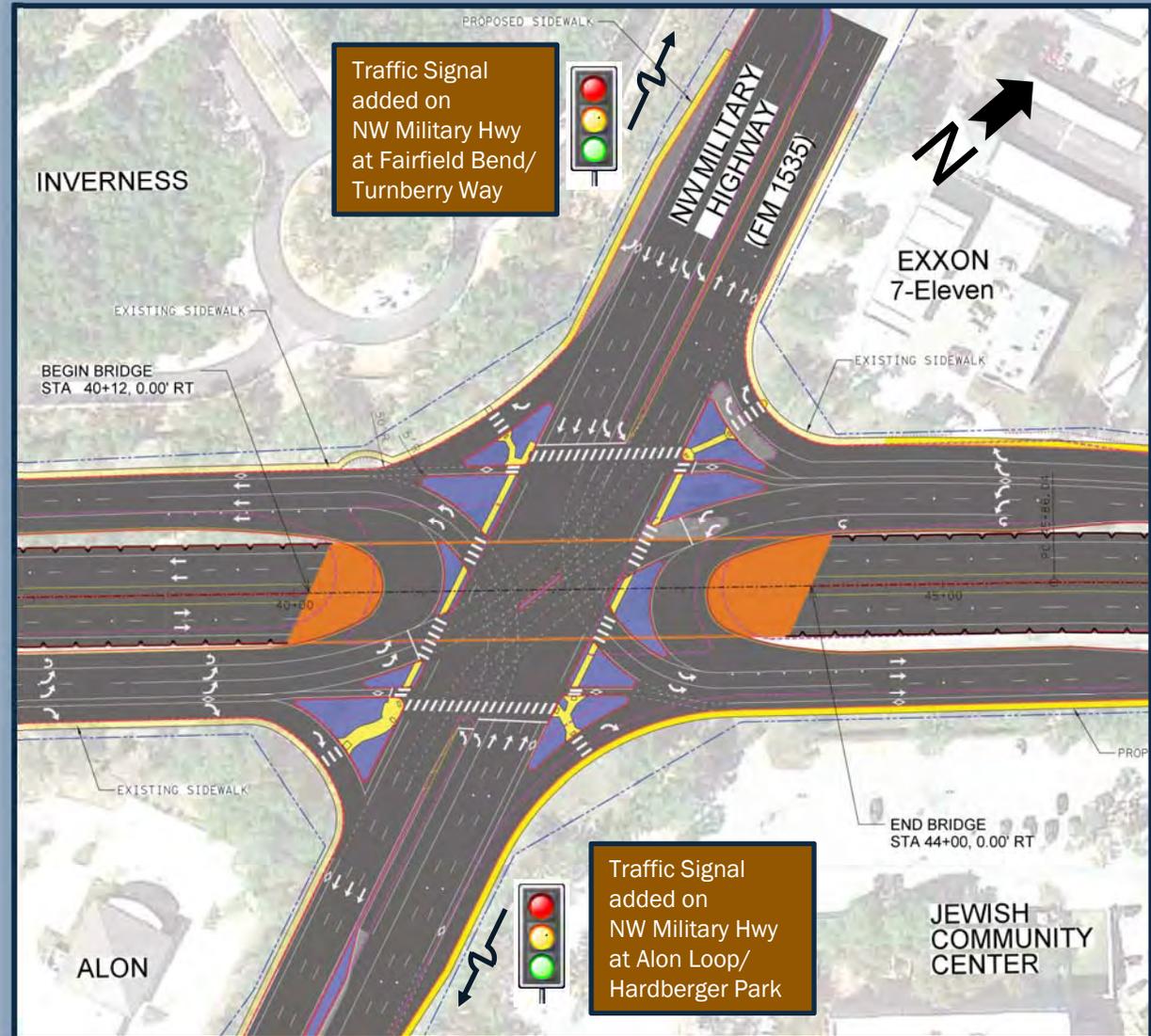
- The queue lengths were developed for the worst-case scenarios. For instance, the SPUI W/Signals option represents the worst-case for generating WB queue length of the 3 fully grade separated options, and the partial grade separation is expected to generate longer queue lengths compared with At-Grade Improvements Option.
- The westbound volumes are the peak direction during the AM peak hour.

2030 Average Delay, and LOS

NW Military Options/ Metric	Grade Separated				At-Grade	
	1	2	3	4	5	6
	SPUI With Signals	Modified SPUI	Diamond	Partial Grade- Separation	At-Grade Imp's	No Build
	AM Peak Hour					
Total Delay	57.6	56.6	56.8	57.4	56.7	145.5
LOS	E	E	E	E	E	F
	PM Peak Hour					
Total Delay	108.7	100.7	105.8	101.3	103.8	150.3
LOS	F	F	F	F	F	F

① Single Point Urban Interchange (SPUI) with Signals

- ❑ Allows for concurrent left turns on Wurzbach Parkway and on NW Military
- ❑ Large turning radii for left and right turns
- ❑ 3-phase signal operation
- ❑ Includes signals at Turnberry Way/ Fairfield Bend and Alon Loop/ Hardberger Park
- ❑ Cost: \$29 M



① Single Point Urban Interchange (SPUI) with Signals

PROS

More efficient than all other design options – 3 phase operation

Larger turning radii increases efficiency; similar to through lane capacity

CONS

Excludes through movements for Wurzbach Parkway at grade

Less intuitive for drivers

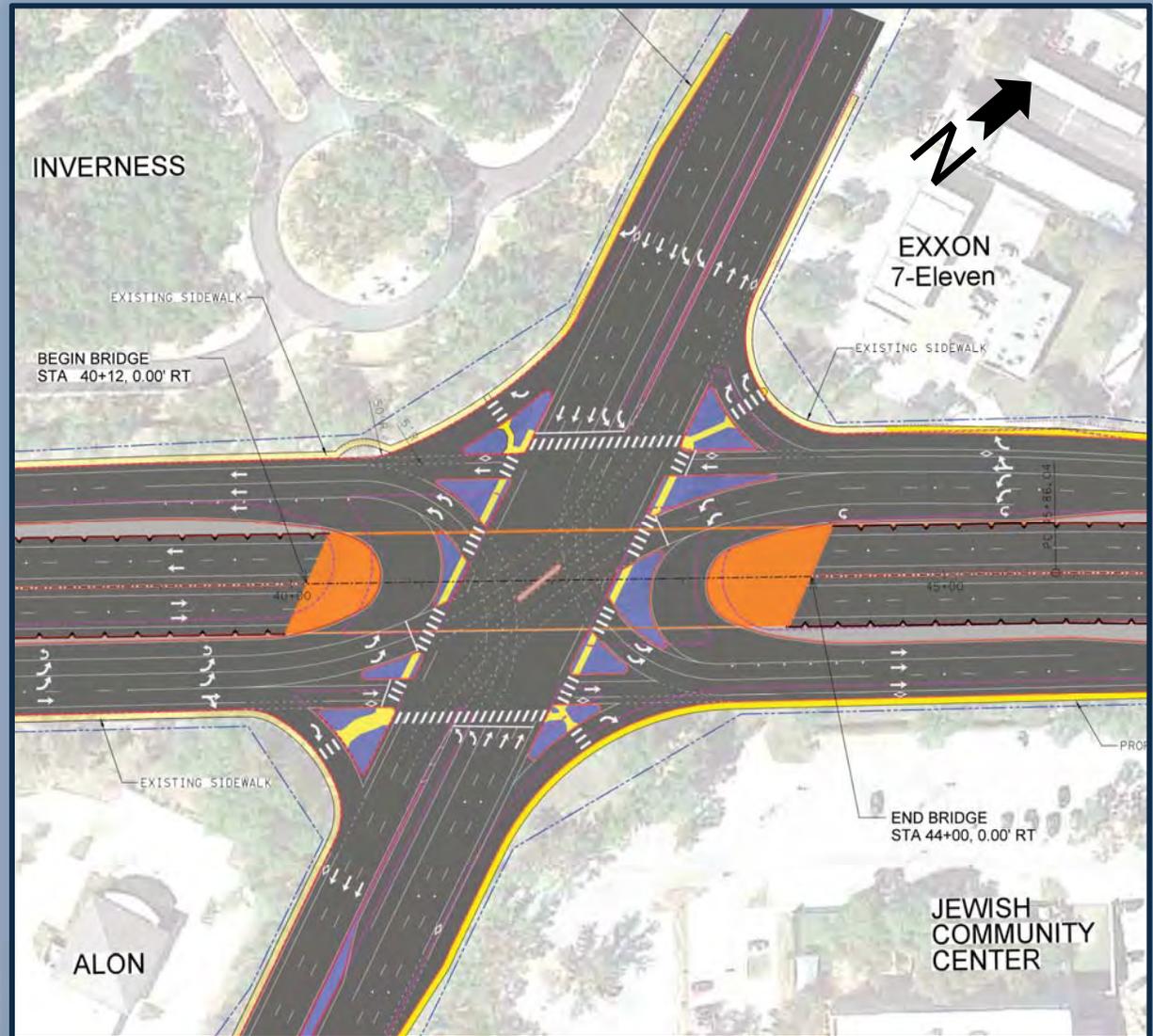
Tree removal

High construction costs

Construction impacts

② SPUI with Frontage Roads

- ❑ Like a SPUI but with Frontage Roads included
- ❑ 4-phase signal operation
- ❑ Does not include signals at Turnberry Way/Fairfield Bend and Alon Loop/Hardberger Park
- ❑ Cost: \$28.5M

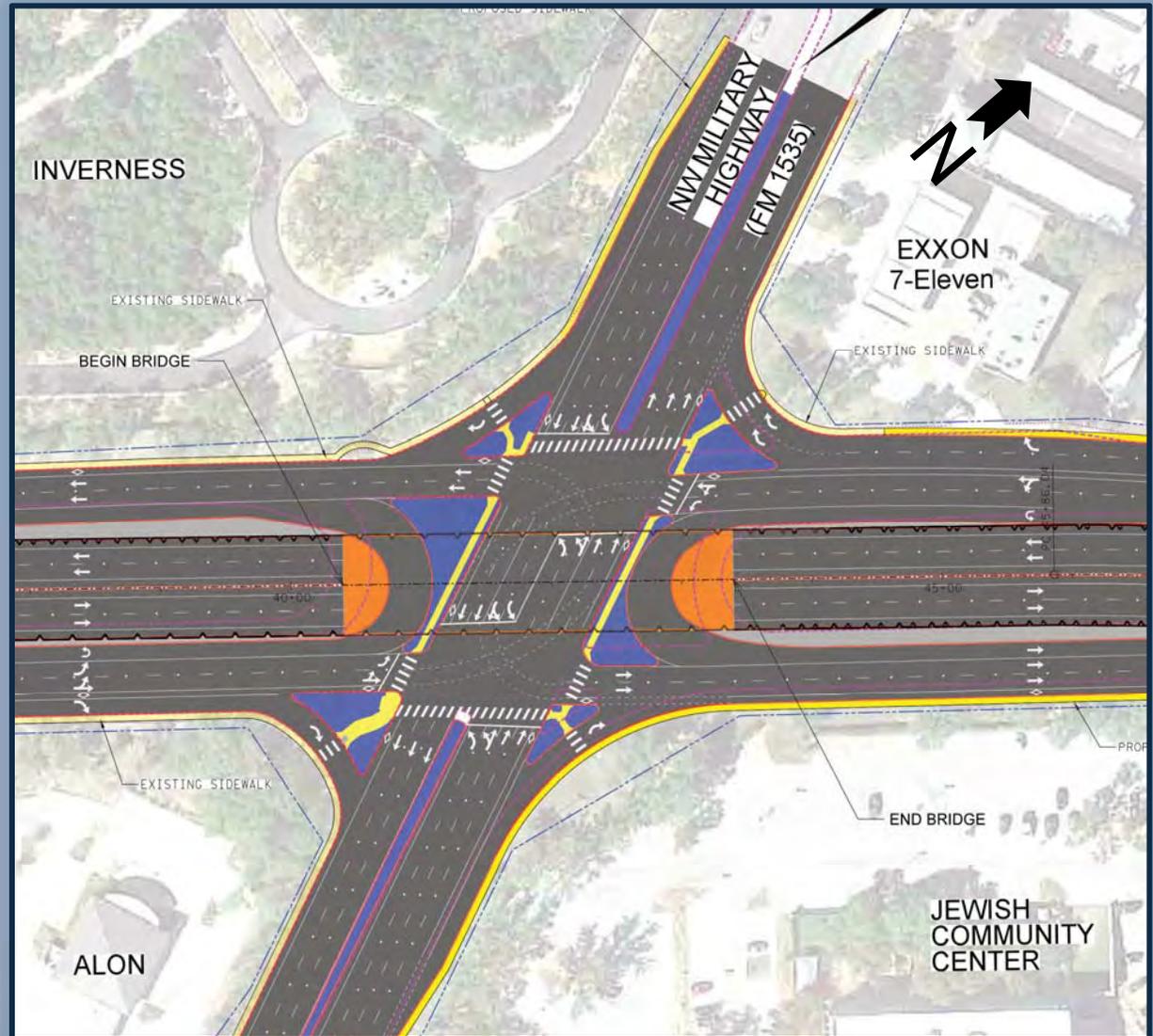


② SPUI with Frontage Roads

PROS	CONS
More efficient than all other options except the SPUI with Signals	Less intuitive for drivers
Maintains large turning radii and concurrent left turns which improves capacity and throughput	Tree removal
Maintains adjacent property access	High construction cost
	Construction impacts

3 Diamond Interchange

- ❑ Traditional interchange design
- ❑ Underpass for Wurzbach Parkway through traffic
- ❑ Frontage Roads for Wurzbach included
- ❑ 4-phase signal operation
- ❑ Cost: \$27M



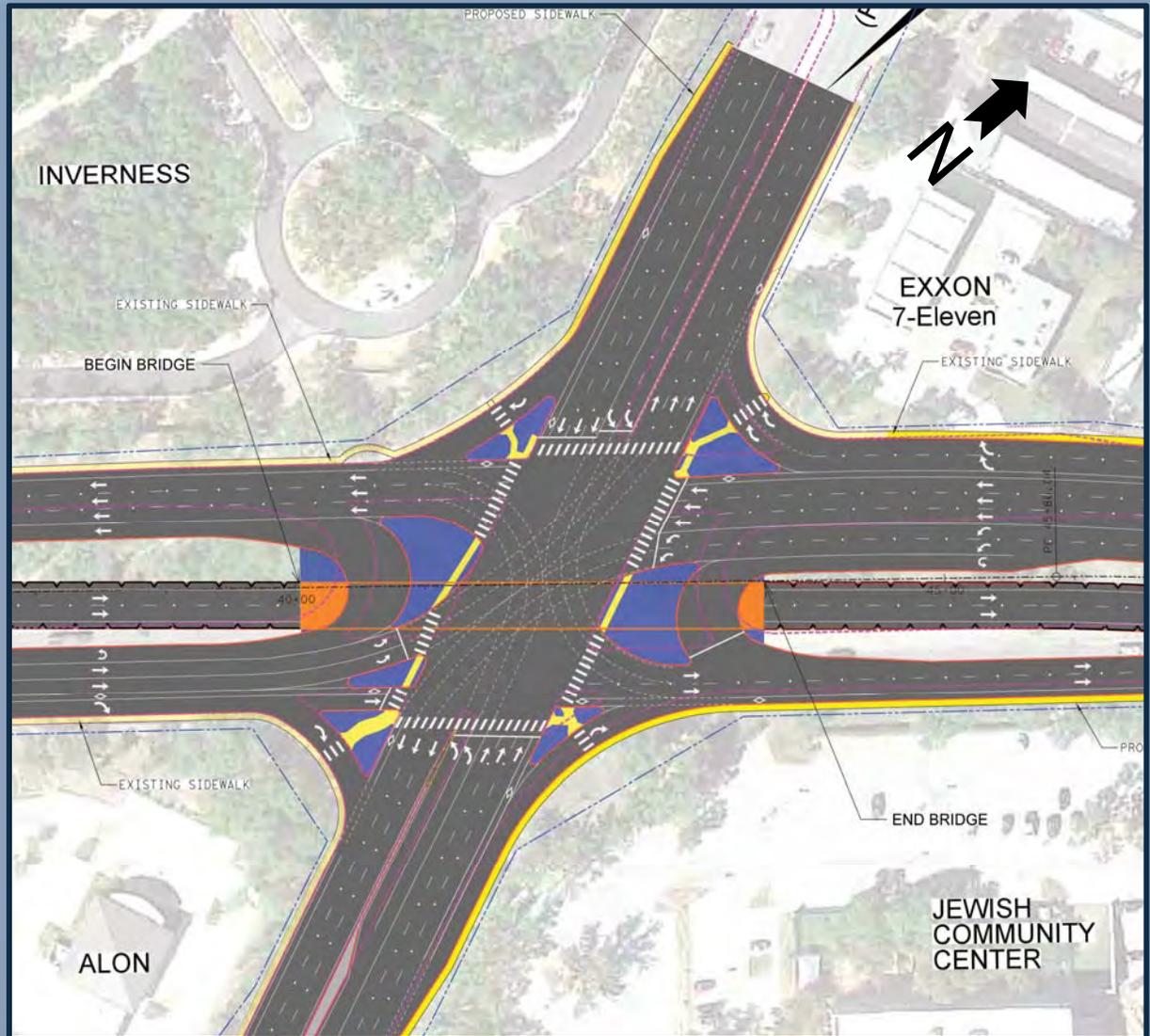
3

Diamond Interchange

PROS	CONS
Driver familiarity	Less effective than SPUI options for throughput and reducing delay
Maintains adjacent property access	Tree removal
	High construction costs
	Construction impacts

4 Partial Grade Separation

- ❑ Underpass for eastbound Wurzbach Parkway only
- ❑ Westbound Wurzbach Parkway traffic at-grade, controlled by traffic signal
- ❑ 4-phase signal operation
- ❑ Cost: \$21.5M



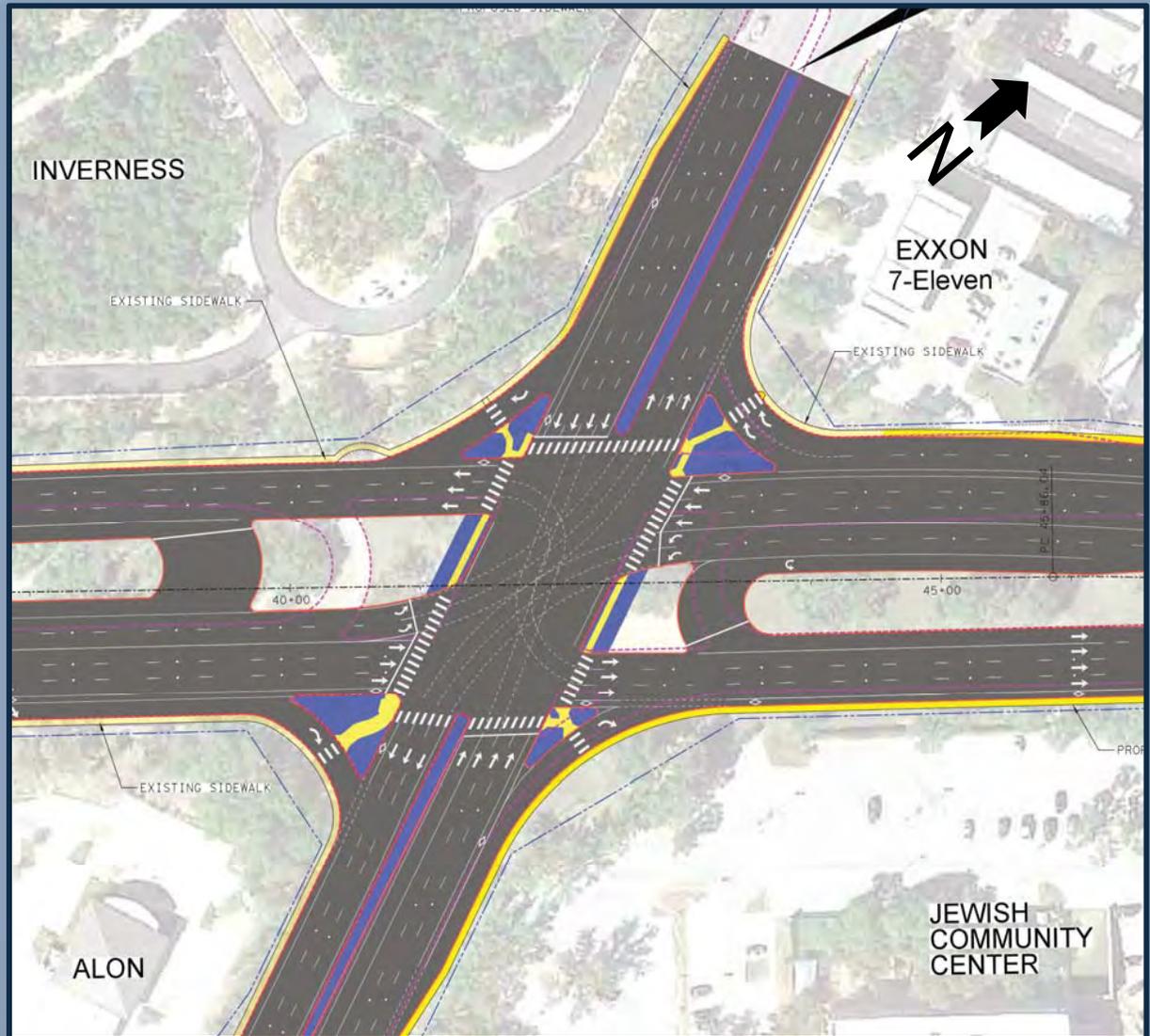
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Partial Grade Separation

PROS	CONS
Doesn't increase WB congestion downstream at Lockhill-Selma	Very inefficient due to imbalanced signal operation
Maintains adjacent property access	Tree removal
Lower construction cost than other grade separated options	Cost is high relative to capacity benefit
	Construction impacts

5 At-Grade Improvements

- ❑ No underpass; all movements at-grade
- ❑ Adds lanes on Wurzbach Parkway
- ❑ Adds lanes on NW Military Highway
- ❑ 4-phase signal operation
- ❑ Cost: \$7.5M



5

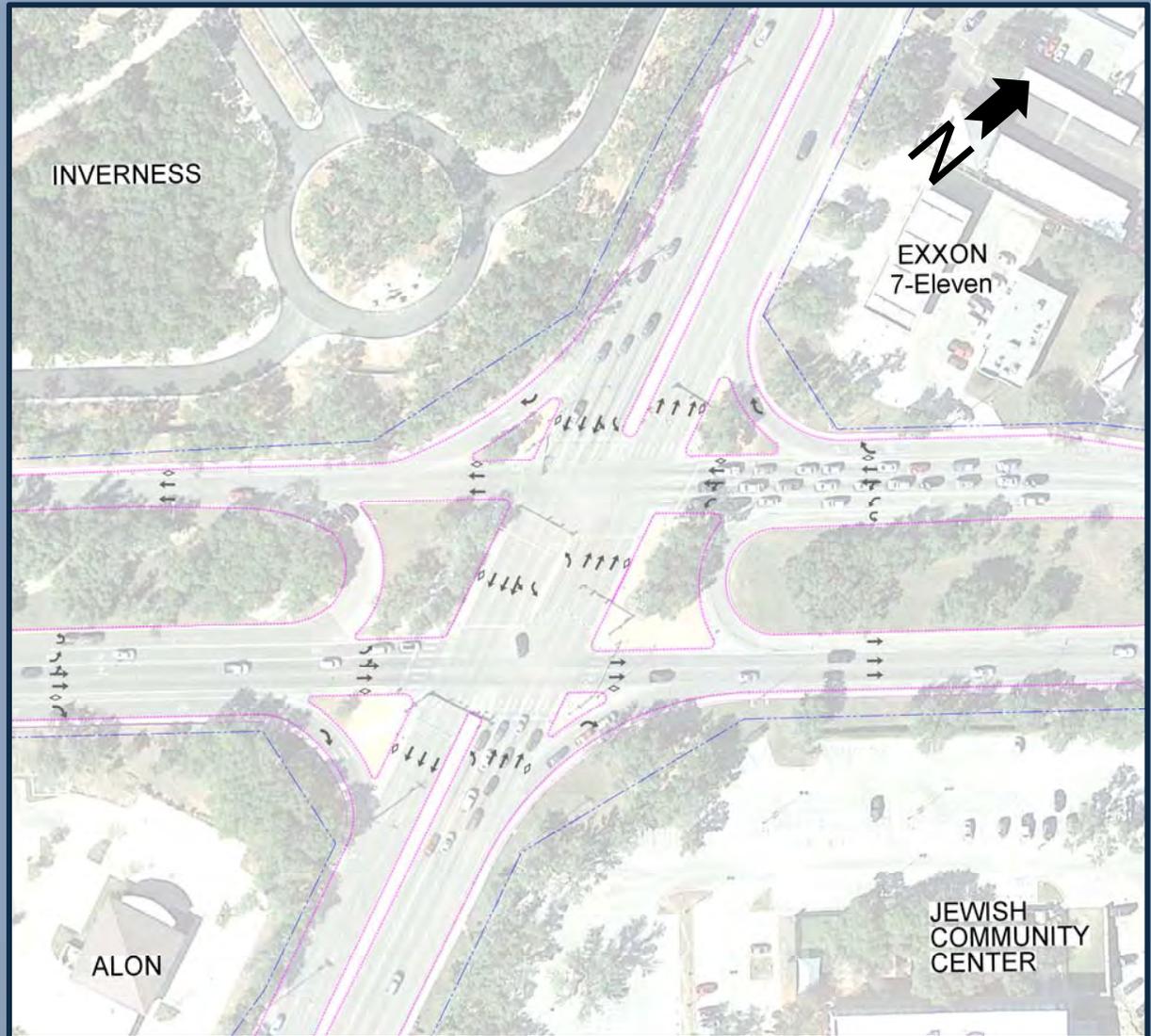
At-Grade Improvements

PROS	CONS
Low construction impacts	Most inefficient operation of all options
Retains some tree canopy	Does not allow for concurrent left turn movements
Maintains property access	Least effective for adding capacity and reducing delay and congestion
Lower construction costs	All movements controlled by signal



6 No-Build

- ❑ Keep current intersection configuration
- ❑ No physical improvements
- ❑ Cost: \$5,000



6

No-Build

PROS	CONS
Retains median and trees	Traffic congestion not improved
Little to no cost for signal timing	Does nothing to address future growth/increase in traffic
No construction impacts	
Access is not affected	



Provide Your Comments

- ❑ Leave a comment card
- ❑ Talk to the court reporter
- ❑ Email your comments to: wurzbach@wsp.com
- ❑ Mail your comments to:

WSP USA
Bank of America Plaza
300 Convent Street, Suite 1330
San Antonio, TX 78205



Comments must be submitted or postmarked by
September 15, 2017

APPENDIX F
DESCRIPTION OF PROJECT MODIFICATIONS
RESULTING FROM OPEN HOUSE



Project Modifications

Principal Arterial (PA) 1502 - 1502-03-003

Wurzbach Parkway from Lockhill-Selma Road to NW Military Highway

Wednesday, August 30, 2017 from 6:00 to 8:00 p.m.

Due to input received from the public during the second public open house regarding the six (6) alternatives presented, analysis of potential alternatives will be conducted with the results shared in a future open house event.