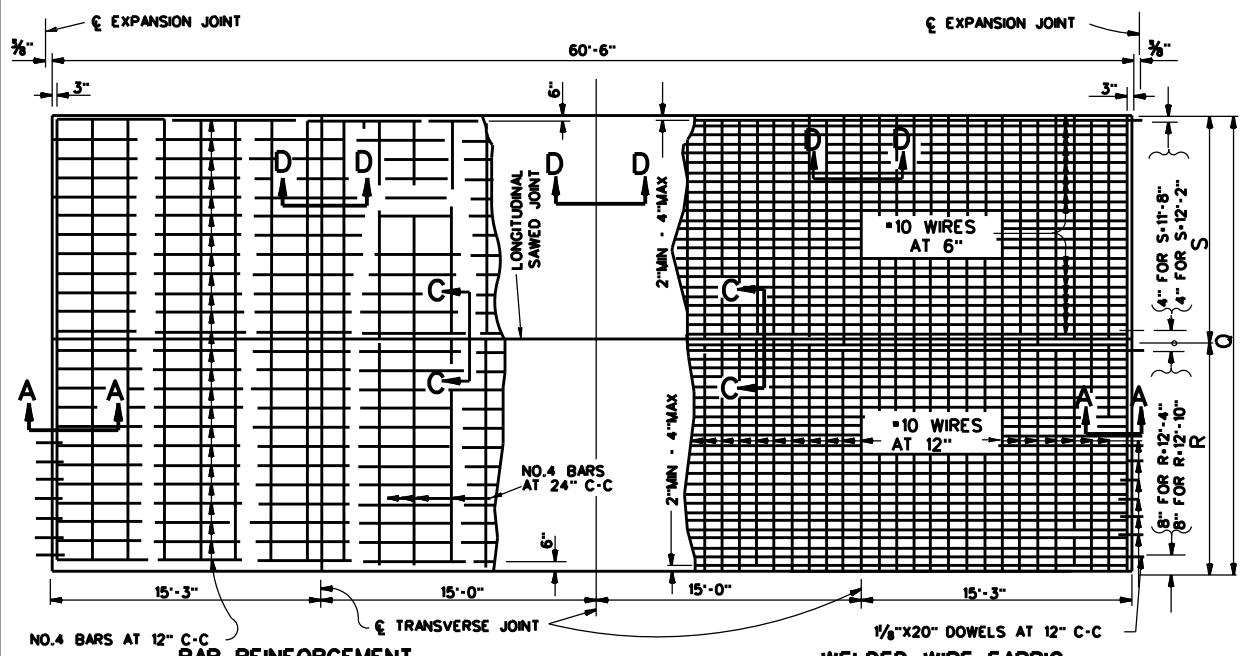


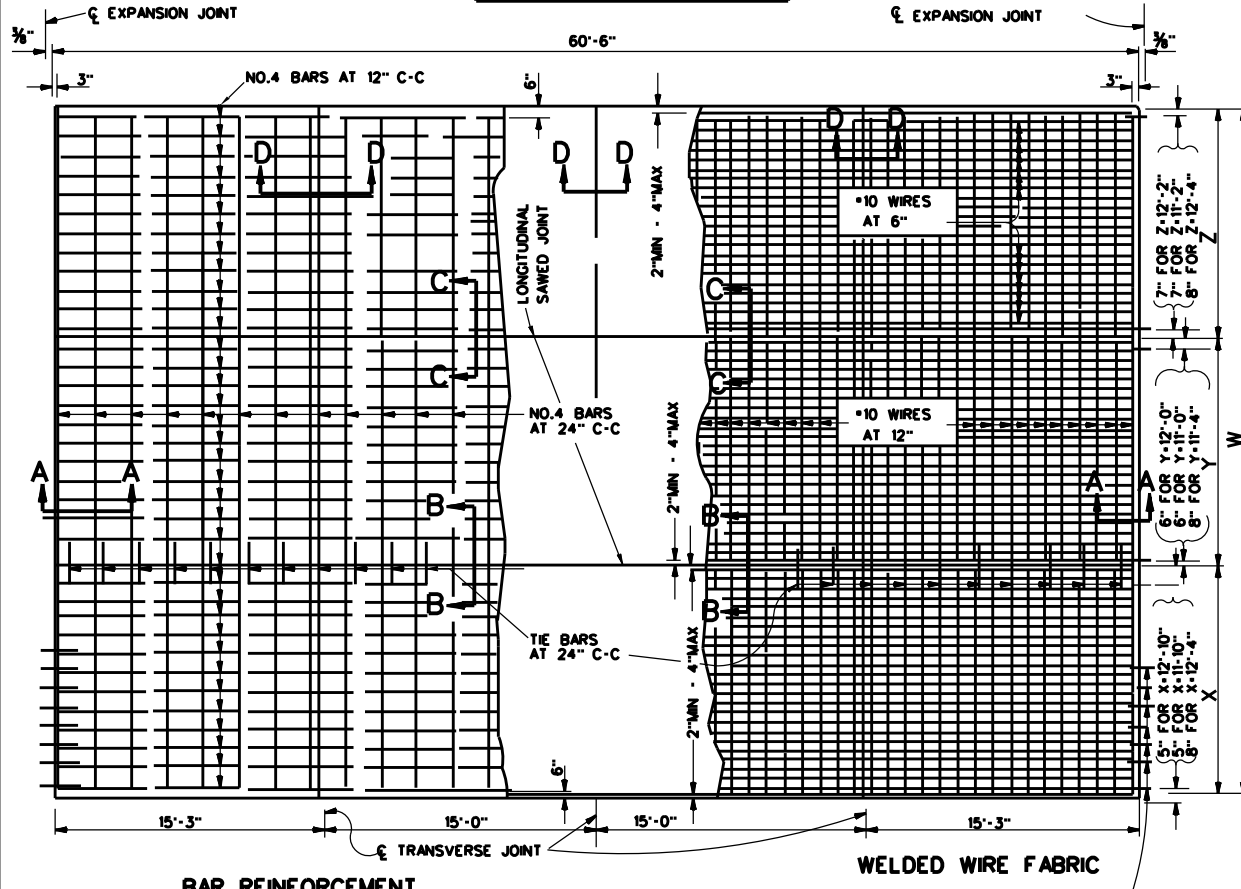
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TWO LANE PAVEMENT PLAN

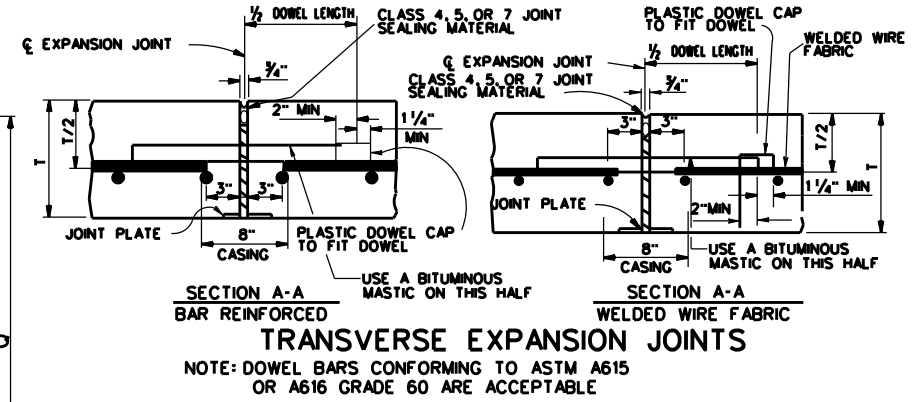
	WIDTH - Q		
	24'-0"	24'-6"	25'-0"
R	12'-4"	12'-4"	12'-10"
S	11'-8"	12'-2"	12'-2"



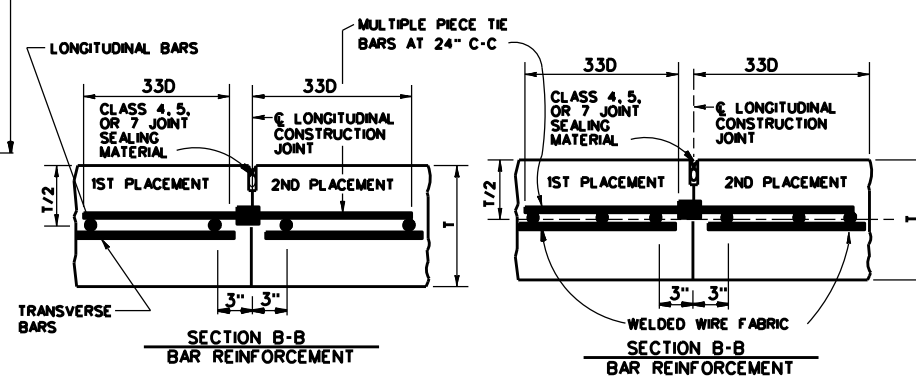
THREE LANE PAVEMENT PLAN

	WIDTH - W		
	37'-0"	36'-0"	34'-0"
X	12'-10"	12'-4"	11'-10"
Y	12'-0"	11'-4"	11'-0"
Z	12'-2"	12'-4"	11'-2"

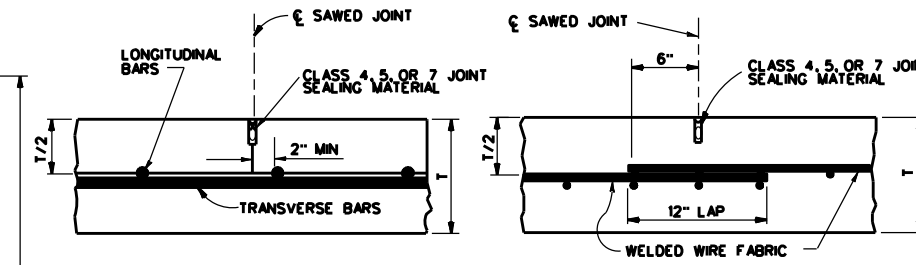
D = DIAMETER
R = RADIUS
T = THICKNESS



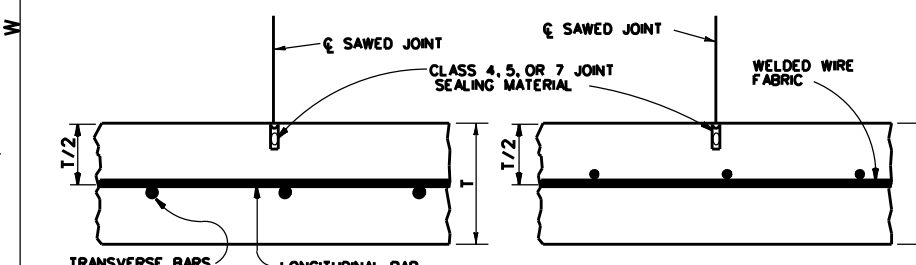
TRANSVERSE EXPANSION JOINTS
NOTE: DOWEL BARS CONFORMING TO ASTM A615 OR A616 GRADE 60 ARE ACCEPTABLE



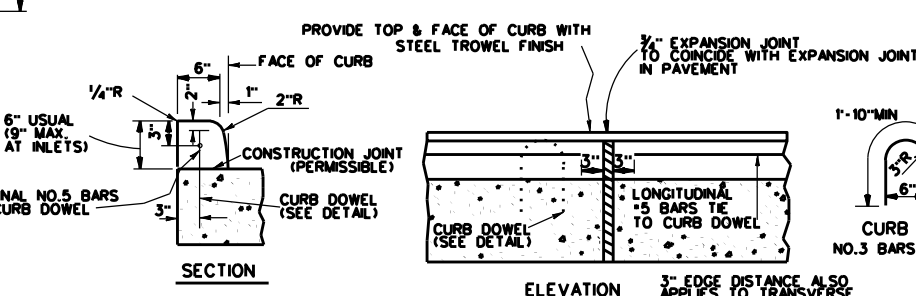
LONGITUDINAL CONSTRUCTION JOINTS



LONGITUDINAL SAWED JOINTS



TRANSVERSE SAWED JOINTS



TYPICAL 6" CURB (DETAIL)

GENERAL NOTES

- MULTIPLE PIECE TIE BARS ARE REQUIRED AT LONGITUDINAL CONSTRUCTION JOINTS. USE MULTIPLE PIECE TIE BAR ASSEMBLIES WITH STOP TYPE COUPLINGS AND WITH THREADS ON THE BARS. ENSURE THE MULTIPLE PIECE TIE BAR ASSEMBLIES DEVELOP A MINIMUM ULTIMATE TENSILE STRENGTH EQUAL TO 1.25 TIMES THE YIELD STRENGTH OF THE TRANSVERSE BARS BEING JOINED. USE PERFORMED REINFORCING BARS FOR THE BARS. TIE BAR ASSEMBLIES MADE FROM STEELS OTHER THAN ASTM GRADE 60 AND WITH DEFORMATIONS OTHER THAN ASTM STANDARD MAY BE USED IF IT CAN BE PROVEN TO THE ENGINEER THAT THEY ARE IN EVERY RESPECT THE EQUAL OF THE ASSEMBLIES SPECIFIED. LABORATORY TESTING OF THE PROPOSED ASSEMBLIES, AT THE CONTRACTOR'S EXPENSE, MAY BE REQUIRED.
- FORM CONSTRUCTION JOINTS WITH METAL OR WOOD FORMS EQUAL IN DEPTH TO THE NOMINAL DEPTH OF THE PAVEMENT OR BY OTHER MEANS APPROVED PRIOR TO THEIR USE.
- SAW LONGITUDINAL AND TRANSVERSE JOINTS AS SOON AS SAWING CAN BE ACCOMPLISHED WITHOUT DAMAGE TO THE PAVEMENT AND BEFORE 24 HOURS AFTER PLACING THE CONCRETE. THE EXACT TIME WILL BE APPROVED BY THE ENGINEER. PERFORMED JOINT WITH ASPHALT STRIP IS NOT ACCEPTABLE.
- LONGITUDINAL JOINTS ARE SHOWN OFFSET FOUR INCHES FROM THE THEORETICAL LANE LINE AND MAY BE OFFSET TO EITHER SIDE IF THE WIDTH OF THE WIRE FABRIC IS PROPERLY ADJUSTED.
- ONE OF THE LONGITUDINAL JOINTS OF PAVEMENT SLABS WIDER THAN TWO LANES MAY BE A CONSTRUCTION JOINT. FOR PAVEMENT SLABS WIDER THAN 15 FT. PROVIDE A LOGITUDINAL SAWED JOINT UNLESS OTHERWISE DIRECTED.
- FORM THE JOINT SEAL SPACE AT TRANSVERSE EXPANSION JOINTS BY USING A STRAIGHT FORM PLACED BEHIND THE LONGITUDINAL FLOAT. LOOSEN THE FORM AS SOON AS THE CONCRETE WILL RETAIN ITS SHAPE AND EDGE WITH AN APPROVED EDGING TOOL. TOOL BOTH EDGES OF LONGITUDINAL CONSTRUCTION JOINTS TO A 1/8 IN. RADIUS AT THE PAVEMENT SURFACE.
- DO NOT DISCHARGE CONCRETE FROM THE MIXER DIRECTLY ON TOP OF OR ON THE SIDES OF THE EXPANSION JOINT ASSEMBLIES.
- LAP TRANSVERSE EDGES OF SHEETS OF WELDED WIRE FABRIC 12 INCHES EXCEPT AT TRANSVERSE EXPANSION JOINTS. LAP LONGITUDINAL EDGES 6 INCHES EXCEPT AT LONGITUDINAL CONSTRUCTION JOINTS.
- DOWEL BARS MAY BE COATED WITH STAINLESS STEEL, MONEL METAL, OR IN ACCORDANCE WITH THE ITEM "REINFORCING STEEL" SECTION ON EPOXY COATING WITH A WELDED DOWEL ASSEMBLY SUPPORT, AS APPROVED. ENSURE THE CASING CONFORMS TO THE REQUIREMENTS OF ONE OF THE GRADES OF ASTM A67-70 OR A176-71 AND IS NOT LESS THAN 0.010 INCH THICK. PROVIDE A CASING AT LEAST 8 INCHES LONG AND THAT COVERS THE MIDDLE 8 INCHES OF THE DOWEL.
- SECURE DOWELS PARALLEL TO THE PAVEMENT SURFACE AND PERPENDICULAR TO THE JOINT WITH THE AID OF APPROVED WELDED WIRE BASKET ARRANGEMENTS. ENSURE WELDED WIRE BASKET ARRANGEMENTS DO NOT CROSS THE EXPANSION JOINT. UNIFORMLY COAT DOWELS WITH A BITUMINOUS MASTIC ON THE END WITH THE DOWEL CAP.
- DO NOT BEND TIE BARS AND DOWEL BARS TO PREVENT DISPLACEMENT OF WIRE FABRIC BY CONCRETE PLACEMENT. TIE THE FABRIC PANEL TOGETHER AND TIE THE INITIAL FABRIC PANELS OF EACH SLAB TO THE DOWEL BASKET OR AS DIRECTED.
- TOOL PAVEMENT EDGES TO A RADIUS OF 1/8 IN. WITH AN APPROVED EDGING TOOL.
- DETAILS FOR PAVEMENT WIDTH, PAVEMENT THICKNESS, AND CROWN-SLOPE ARE ELSEWHERE SHOWN ON THE PLANS.
- THE CONTRACTOR HAS THE OPTION OF USING WELDED WIRE FABRIC OR BAR REINFORCEMENT. LOCATE THE LONGITUDINAL STEEL AT THE CENTER OF THE SLAB. TAKE NECESSARY PRECAUTIONS TO INSURE THAT THE FINAL POSITION OF STEEL IS WITHIN 1/2 IN. OF THE SLAB CENTER. ENSURE THE LONGITUDINAL AND TRANSVERSE STEEL SPACING DOES NOT VARY MORE THAN ONE-TWELFTH OF SPACING SHOWN.
- LONGITUDINAL STEEL MAY BE SPLICED WITH 33 TIMES BAR DIAMETER LAPS.
- FOR LANE WIDTHS NOT SHOWN OR FOR VARIABLE PANEL LENGTHS AND WIDTHS, SPACE REINFORCING STEEL AND DOWELS AS DIRECTED.
- USE APPROVED BAR MAT CHAIRS. DO NOT EXCEED CHAIR SPACING OF 30 IN. C-C (TRANSVERSE) AND 48 IN. C-C (LONGITUDINAL). GALVANIZING THE CHAIRS IS NOT REQUIRED.
- OBTAIN BOARDS FOR EXPANSION JOINT FILLER FROM REDWOOD TIMBER.
- PROVIDE AND CONSTRUCT THE JOINT PLATE AS APPROVED.
- WHEN CURB IS PLACED SEPARATELY FROM THE CONCRETE PAVEMENT, PROVIDE THE REINFORCING STEEL AS SHOWN IN THE CURB DETAIL. THE CURB REINFORCING STEEL MAY BE OMITTED WHEN THE CURB IS PLACED MONOLITHICALLY.

(GENERAL NOTES CONTINUED ON SHEET 2 OF 2)

Houston District Standard

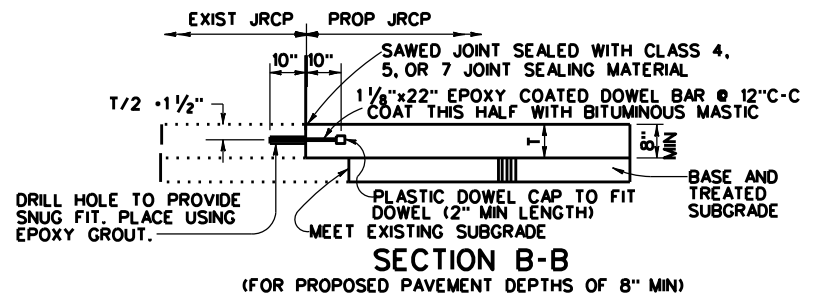
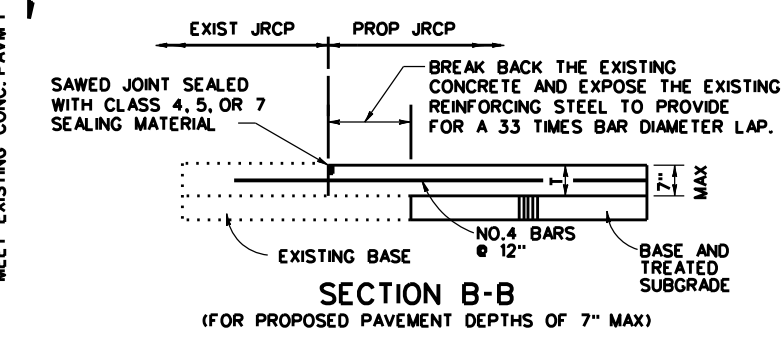
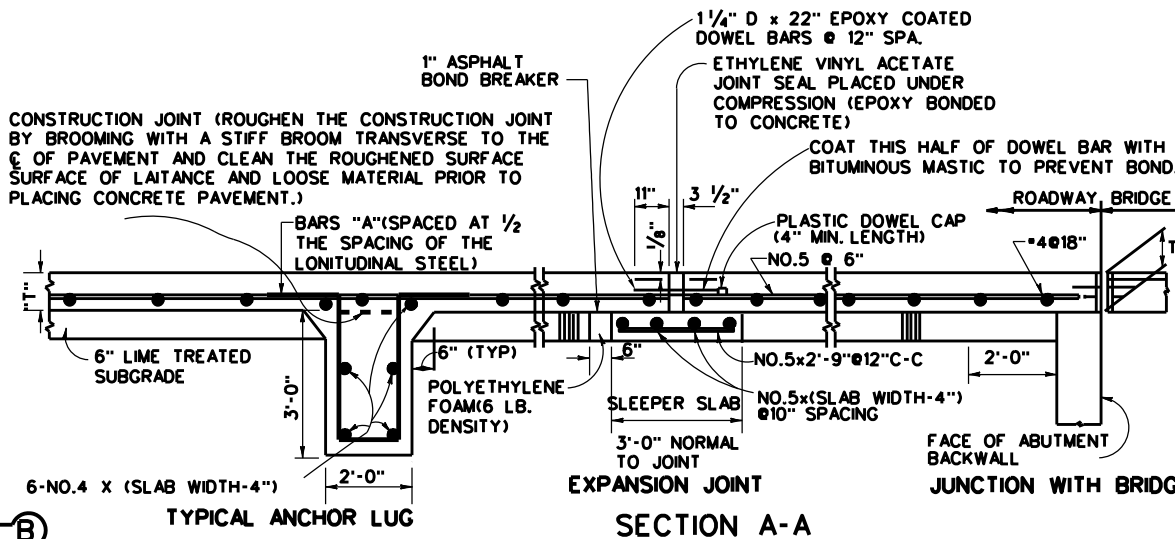
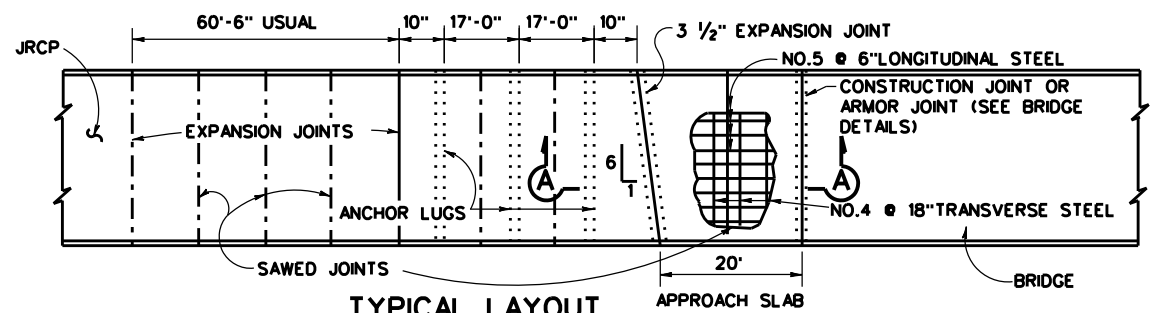
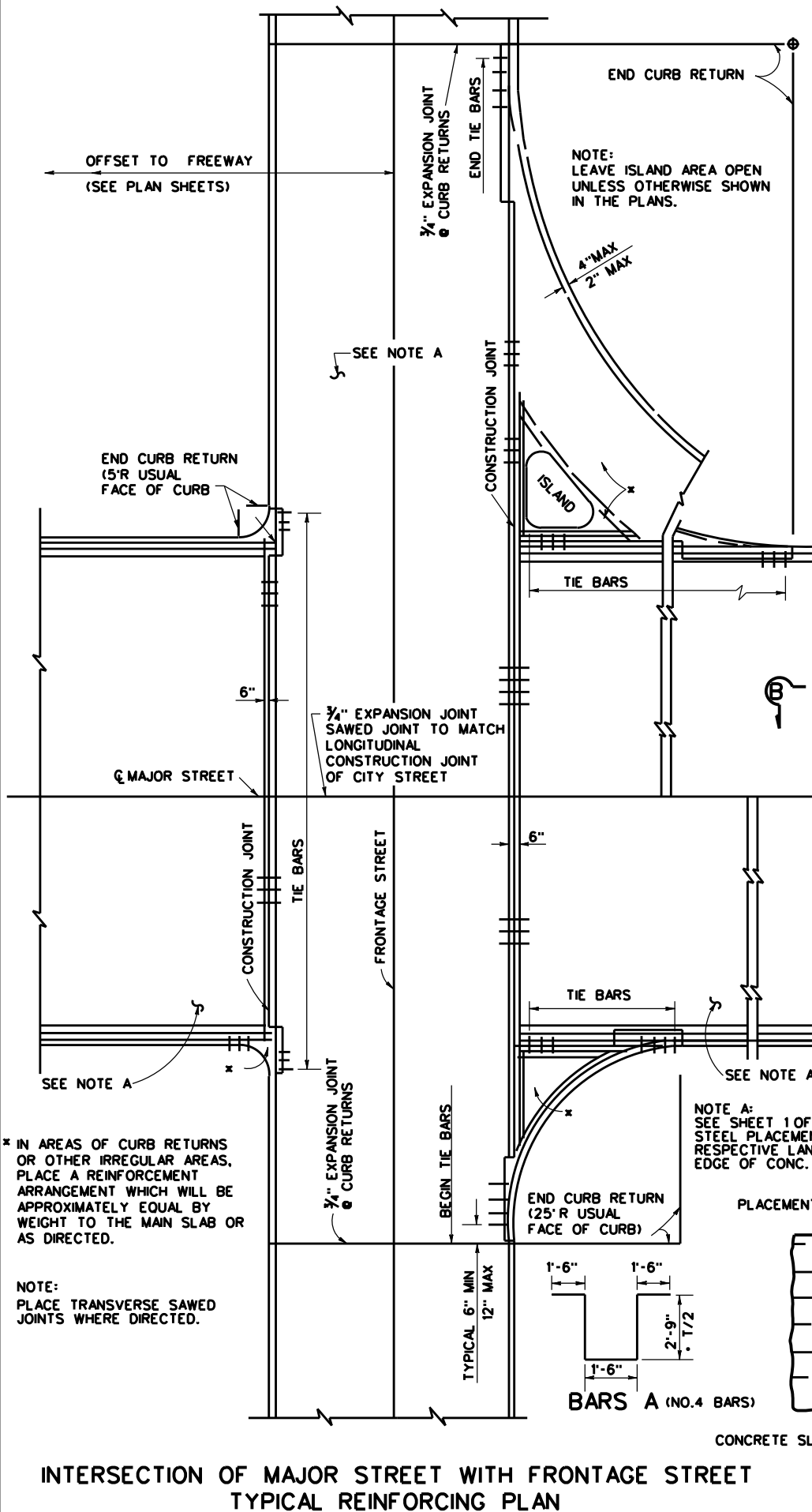
JOINTED REINFORCED CONCRETE PAVEMENT DETAILS
(FOR PAVEMENT THICKNESS 10 INCHES OR LESS)

JRCP-25 (HOU)

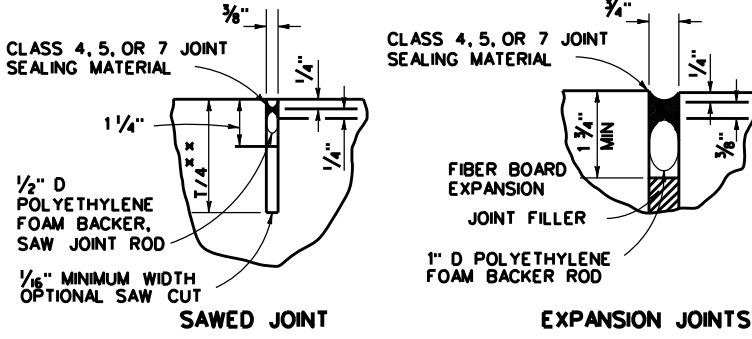
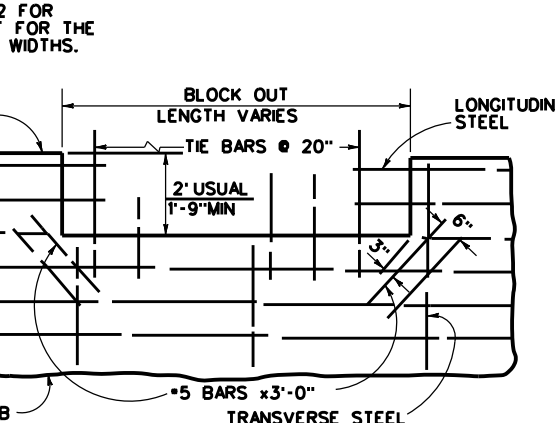
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REVISIONS				
02-25-2024 Spec Updates	DIST	COUNTY		SHEET NO.

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REPLACE ANY BENT LONGITUDINAL REINFORCING, IF THERE IS NOT SUFFICIENT EXPOSED REINFORCING TO PROVIDE A MINIMUM OF A 33 TIMES BAR DIAMETER LAP. REMOVE THE EXISTING PAVEMENT AND SUFFICIENTLY EXPOSE THE EXISTING REINFORCING TO PROVIDE A 33 TIMES BAR DIAMETER LAP. REPLACE ANY SHEAR BARS THAT ARE DISTURBED, BY DRILLING AND GROUTING AS REQUIRED BY NOTE *29. PERFORM THIS CORRECTIVE ACTION AT NO EXPENSE TO THE DEPARTMENT.



** IF SILICEOUS RIVER GRAVEL IS USED AS THE COARSE AGGREGATE, THIS DEPTH IS T/3.

- GENERAL NOTES (CONTINUED FROM SHEET 1 OF 2)
- CONSTRUCT ANCHOR LUGS, EXPANSION JOINTS, AND SLEEPER SLABS AS DETAILED IN SECTION A-A. THESE WILL BE PAID FOR IN ACCORDANCE WITH ITEM, "CONCRETE PAVEMENT TERMINALS."
 - REINFORCING STEEL FOR TERMINAL ANCHOR SYSTEMS MAY BE GRADE 40 OR GRADE 60.
 - PLACE CONCRETE FOR ANCHOR LUGS AS SOON AS POSSIBLE AFTER COMPLETING EXCAVATION, TO PRESERVE THE INHERENT SOIL CHARACTERISTICS. EXCAVATING FOR AND PLACING CONCRETE FOR ANCHOR SYSTEM MAY BE IN PREFORMED SECTIONS CORRESPONDING TO THE WIDTH OF PAVING PLACEMENT.
 - APPLY A STEEL TROWEL FINISH TO SLEEPER SLABS AND COAT WITH AN ASPHALT BOND BREAKER.
 - THE DETAILS FOR ANCHORS, LUGS, EXPANSION JOINTS, AND SLEEPER SLABS ARE NOT APPLICABLE UNLESS SHOWN ELSEWHERE IN THE PLANS.
 - APPROACH SLAB WILL BE PAID FOR IN ACCORDANCE WITH THE ITEM "CONCRETE STRUCTURES."
 - WITHIN 5 MINUTES OF SAWING, COMPLETELY REMOVE THE RESULTING SLURRY FROM THE JOINT BY FLUSHING WITH HIGH PRESSURE WATER. THEN ALLOW THE JOINT TO DRY FOR A MINIMUM OF 48 HOURS BEFORE SANDBLASTING THE JOINT.
 - DO NOT SHEAR CUT DOWEL BARS.
 - SIZE ADDITIONAL SHEAR BARS AS LONGITUDIAL BARS AND SPACE THEM MIDWAY BETWEEN ALTERNATE LONGITUDINAL BARS ALONG THE TRANSVERSE CONSTRUCTION JOINT FORMED AT THE LEAVE-OUT.
 - IF THE CONCRETE DESIGN REQUIRES GREATER THAN 5.5 SACKS OF CEMENTITIOUS MATERIAL PER CUBIC YARD, WRITTEN APPROVAL BY THE AREA ENGINEER WILL BE REQUIRED. ENSURE CONCRETE PAVEMENT MIXES PLACED FROM APRIL 1 TO OCTOBER 31 CONTAIN A MINIMUM OF 25 PERCENT BY WEIGHT OF CLASS "F" FLY ASH.
 - IN LOCATIONS WHERE THE PLANS CALL FOR FAST TRACK CONCRETE PAVEMENT IN LIEU OF JRCP (LAID ON COMPACTED OR STABILIZED SUBGRADE), USE DETAILS IN THIS STANDARD IN CONJUNCTION WITH THE APPROPRIATE FAST TRACK CONCRETE SPECIFICATION. IF THE JRCP IS LAID UPON A BASE STRUCTURE, ADD 3" TO THE FAST TRACK PAVEMENT THICKNESS TO COMPENSATE FOR THE BASE.

Texas Department of Transportation
Houston District Standard

JOINTED REINFORCED CONCRETE PAVEMENT DETAILS

EXPANSION JOINT DESIGN
(FOR PAVEMENT THICKNESS 10 INCHES OR LESS)
JRCP-25 (HOU)

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